

# Recommendations Summary

*DRAFT* July 2023

**REIMAGINE  
JACKSON STREET**  
WHAT'S YOUR VISION? 

**Prepared for:**

Escambia County

Brownsville CRA

City of Pensacola

Westside CRA

By:

**wsp**

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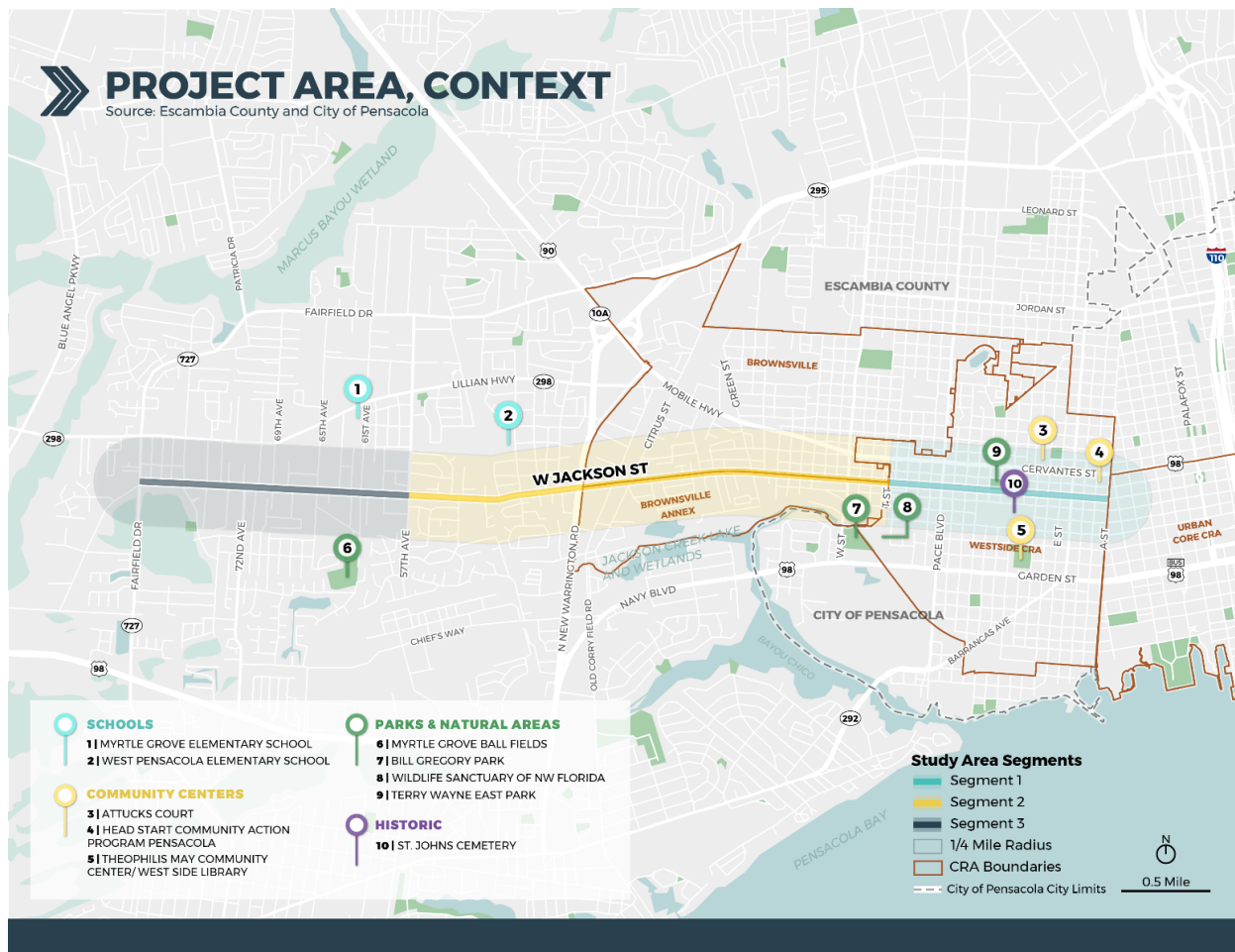
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# EXECUTIVE SUMMARY

The City of Pensacola and Escambia County and their Community Redevelopment Agencies (CRAs) engaged in a visioning and planning process to improve Jackson Street from A Street to Fairfield Drive.

The study area encompasses W Jackson Street from N A Street to W Fairfield Drive and a 0.25-mile buffer. W Jackson Street is a local collector roadway extending approximately 6.2 miles from A Street to T Street (2 miles) in the City of Pensacola and T Street to W Fairfield Drive (4.2 miles) in Escambia County.

**Figure 1: Map of the project area, context**



The study area was divided into three segments.

- **Segment 1** is on the eastern side of the study area, from N A Street to N T Street, entirely within the City of Pensacola.
- **Segment 2** is at the center of the study area, from N T Street to N 57<sup>th</sup> Avenue, entirely within Escambia County.
- **Segment 3** is on the western side of the study area, from N 57<sup>th</sup> Avenue to W Fairfield Drive, entirely within Escambia County.

Through engagement, the community named the master planning process REIMAGINE JACKSON STREET and identified the following goals and priorities.

## Project Goal

The project aims to develop an implementable master plan for the corridor, ensure all voices are heard during the corridor planning process, and develop an inclusive vision based on community inputs. The proposed improvements will complement the existing communities and address safety, mobility, and sustainable long-term economic development along the corridor.

## Vision Statement

In 10 years, Jackson Street will be an **east-west gateway** between the City of Pensacola and Escambia County that operates as a slower street connecting non-vehicle modes. It will have **an internationally diverse community fabric** with enhanced transportation and upgraded utility infrastructure. It will have **vibrant community nodes**, including local businesses, community gathering places, and recreational spaces. **People of all ages and abilities** can safely and conveniently walk, bike, or ride on this street.

- *Jackson Street stakeholders*

## Top Priorities Identified

The project team conducted a robust public outreach and engagement effort to inform the community of the project, solicit input and gather feedback on community preferences. Activities included walk audits with City, County, and CRA staff, community engagement through in-person, virtual workshops, pop-up events within the community, and a digital survey with 300 responses.

Community appreciated community assets are-

- **Historic homes and neighborhood**
- **Large tree canopy**
- **Diverse community**

Community priorities for improvement are-

- #1 Safe, Accessible Mobility Opportunities**
- #2 Celebrate, Preserve Community Identity**
- #3 Increase Access to Parks and Open Spaces**

# Recommendation #1. Provide Safe, Accessible Mobility

## REDUCE THE POSTED SPEED, AND INSTALL TRAFFIC CALMING THAT CREATE UNSAFE CONDITIONS FOR THOSE WALKING, BIKING, OR USING TRANSIT.

Most people do not feel safe walking, biking, or accessing transit along Jackson Street, and most residents recognize that speed is an issue. Also, five-year crashes indicated that the western sections with a higher posted speed limit are more unsafe. Therefore, we recommend:

1. **Reduce the posted speed limit** on Segment 1 (A to Pace St) to 25 MPH; Segment 2 and 3 (Pace and Fairfield) to 30 MPH;
2. **Reduce travel lanes width** to 10' wide, curb return 25'; and
3. **Install traffic calming improvements:**
  - a. Curb extensions at mid-block and each block- Segment 1
  - b. Mini roundabouts
    - i. Segment 3: N 49<sup>th</sup> Avenue, N 62<sup>nd</sup> Avenue, N 65<sup>th</sup> Avenue, N 72<sup>nd</sup> Avenue
  - c. Replace the signal with a four-way STOP sign at 57<sup>th</sup> Avenue.
  - d. Realign the intersection on the east side of A Street for better visibility for people driving, biking, and walking across A Street between the east and west sides of Jackson Street.

### » F Street intersection



*Figure 2: Rendering of Proposed Improvements*



**4. Sidewalk improvements, safe crossings**

- a. Continuous sidewalk from A Street to Fairfield Drive
  - i. Segment 1: 6' minimum
  - ii. Segment 2 & 3: 12' multimodal path with separated bike path
- b. Marked North-South Crossings
  - i. Segment 1&2: crossing at every intersection
  - ii. Segment 3: north
- c. Raised crosswalk with RRFB
  - a) Segment 1: east side of N A Street and N N Street;
  - b) Segment 2: N Green Street.
- d. Raised Intersections
  - i. Segment 2: Ross Street
- e. Raised crossing with rapid flashing beacon (RRFB)
  - i. A Street. It will serve the dual purpose of acting as a gateway entrance while slowing vehicles. Additional stormwater design and connections would be required to reduce localized ponding.
- f. High visibility crossing with rapid flashing beacon (RRFB)
  - i. Segment 1: N E Street, N G Street, N J Street, N K Street, N L Street, N M Street;
  - ii. Segment 2: N Q Street, N R Street, N T Street, Grandview Street, Goodwins Street, N Kirk Street, Fitzgerald St, Foye Street, Trevilian Street, Wiggins Avenue, Citrus Street;

**Grandview Street intersection**



**Figure 3: Rendering of Proposed Improvements**

- i. Segment 2: N Q Street, N R Street, N T Street, Grandview Street, Goodwins Street, N Kirk Street, Fitzgerald St, Foye Street, Trevilian Street, Wiggins Avenue, Citrus Street;
- ii. Segment 3: Gulf Street, N 48<sup>th</sup> Ave, Frances Drive, Edgewood Dr, Forest Park Dr, Woodsman Dr, N 59<sup>th</sup> Ave, N 63<sup>rd</sup> Ave, N 68<sup>th</sup> Ave, N 70<sup>th</sup> Ave, N 71<sup>st</sup> Ave, N 74<sup>th</sup> Ave, N 77<sup>th</sup> Ave, N 78<sup>th</sup> Avenue.

**5. Increase safety and mobility for those walking and biking at FDOT-owned intersections.**

- a. Roundabouts at Pace Boulevard. Long-term improvements at New Warrington Road and Fairfield Drive, owned by the Florida Department of Transportation (FDOT)
- b. Realign intersections at New Warrington Road with refuge island and channelization and Fairfield Drive with right-in and right-out.

**6. Bike infrastructure, on-street parking**

- a. Segment 1: On-street parking is maintained.
- b. Segment 2&3: No on-street parking, 12' multimodal path with 6' marked and separated biking area.

**7. Transit improvements**

- c. Install transit shelters and provide ADA access at all bus stops. Prioritize safe high-visibility crossing infrastructure across transit stops (as identified in 4. Sidewalk improvements, safe crossings).
- d. In the short term, invest in a circulator or on-demand transit serving Segment 1 and downtown Pensacola. Extend the service to other segments in the future.

**PRIORITIZE ONE-DIG SOLUTIONS WITH INVESTMENT IN NEW AND UPGRADE EXISTING UTILITY INFRASTRUCTURE**

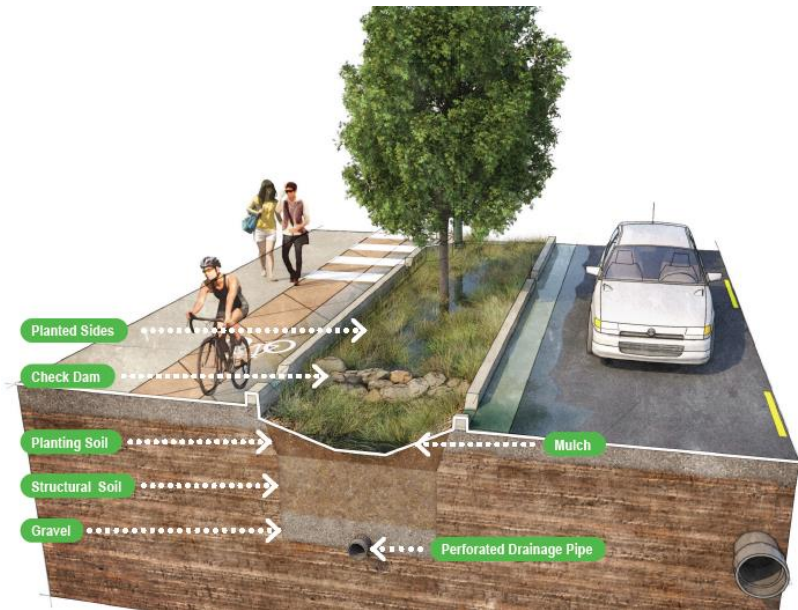
Integrate utility infrastructure improvements during engineering to leverage planned or funded utility upgrades, particularly during construction.

- 1. **Septic to Sewer conversion** along segment 2&3 are required. Considerable infrastructure investment is needed for the conversion, with potential cost sharing between property owners, the County, and the ECUA.
- 2. **Water main replacement.** Several sections of the corridor have cast iron pipes and must be replaced. Also, consider that all water service connection will be renewed with meters placed in casing within the right-of-way.
- 3. **Stormwater infrastructure.**
  - e. Segments 1 & 2 are at higher elevations than the surrounding streets. So, stormwater should be captured to reduce runoff to adjacent streets and water bodies. Also, water quality at outfalls should be addressed.
  - f. Segment 3 and its surrounding areas are relatively low compared to the others. So, an areawide stormwater basin/sub-basin masterplan study is required to assess retention and capacity needs.
- 4. **Power utility infrastructure.** With a renewed focus on resiliency, the City wants to see undergrounding of all utilities where practical and feasible.



## INVEST IN INFRASTRUCTURE THAT PROVIDES LONG-TERM CO-BENEFITS AND ENHANCES THE NATURAL ENVIRONMENT

Integrating green infrastructure like swales, rain gardens, and grey stormwater infrastructure has co-benefits like reduced lifecycle costs, increased water quality, and opportunities for native plants and trees to thrive within a constrained right-of-way. The City and County are using pilot projects to assess the feasibility and the associated cost of maintaining these grey-green infrastructures. The proposed roadway improvements have opportunities to integrate green infrastructure: Segment 1- Urban rain garden within curb extensions; Segment 2&3 - Swale as illustrated.



**Figure 4: Illustrated green infrastructure within swales in Segment 2&3.**

### 61<sup>st</sup> Street intersection



**Figure 5: Rendering of Proposed Improvements**



## PLANNING LEVEL COST ESTIMATES

Preliminary cost estimates were developed using the FDOT average cost with a 20% contingency and 3% 5-year cost escalation.

	<i>Design &amp; Construction Cost</i>	<i>Engineering Design Cost</i>
<i>Segment 1</i>	\$6,300,000	\$938,000
<i>Segment 2</i>	\$12,500,000	\$1,868,000
<i>Segment 3</i>	\$17,300,000	\$2,576,000
<i>Fairfield Drive (FDOT)</i>	\$280,772.43	\$42,000
<i>New Warrington Road (FDOT)</i>	\$110,800	\$19,120
<i>Pace Boulevard (FDOT)</i>	\$1,009,000	\$150,000

**Table 1: Opinion of Design and Construction Cost**



**Figure 6: Community Engagement was Central to the Master Planning Process**

## POTENTIAL GRANT FUNDING OPPORTUNITIES

Identifying funding for 100% engineering design and construction is necessary to implement the Reimagine Jackson Street conceptual plan. Some Federal and State grant funding sources applicable are listed below.

Eligible Federal Grants	Engineering Design	Match Required?
RAISE, U.S. Dept of Transportation (USDOT)	Yes	Yes
PROTECT, U.S. Dept of Transportation (USDOT)	Yes	Yes
Surface Transportation Block Grant, USDOT	Yes	Yes
Safe Streets for All (SS4A), USDOT	Yes	Yes
Healthy Streets Program, USDOT	<i>New program TBD</i>	Yes
Safeguarding Tomorrow Revolving Loan Fund Program, Federal Emergency Management Agency (FEMA)	Yes	<i>New program TBD</i>
Building Resilient Infrastructure and Communities (BRIC), FEMA	Yes	Yes
Reconnecting Communities Pilot (RCP) Program, USDOT	Yes	Yes
Neighborhood Access and Equity Grant Program, USDOT	Yes	Yes
Metropolitan Transportation Planning Program, USDOT	Yes	Yes
Urbanized Area Formula Grants, USDOT	Yes	Yes

**Table 2: Potential Grant Opportunities to Fund Jackson Street**

## Recommendation #2. Celebrate Jackson Street's 'International' Identity, Preserve Existing, Diverse Communities

### 1. Preserve historic assets along the corridor.

- a. **Incentivize restoration of historical buildings** in segment 1 that may have served as commercial or retail spaces, as grandfathered into the current zoning code.
- b. **Continue city & county CRA existing programs and initiatives.** Initiating coordinated marketing, including an online “dashboard” like other cities, should be considered to increase corridor participation.
- c. **Preserve the existing tree canopy** in segment 3. Also, replace or plant new canopy trees appropriately and ensure they thrive on the streetscape with structured soil.

### 2. Celebrate the cultural history of the community.

- a. **Create opportunities for cultural acknowledgments.** Identify opportunities such as sculptures at the gateway entrance at A street within the larger roundabouts at Pace, New Warrington Road.
- b. **Support art in community spaces.** Work with local non-profits and developers to integrate art within community spaces like community centers, street corners, parks and others.

### 3. Establish additional affordable housing investments to preserve the existing community and reduce displacement. Also, there is far more unmet demand for affordable housing than market-rate housing. Within Westside CRA and the Brownsville CRA are thirteen larger empty lots for potential multi-family affordable housing. These vacant single-family lots could lend themselves to a Pensacola Habitat for Humanity model.

- a) **Identify and promote affordable housing funding programs** that apply to low-density multi-family rental, including, but not limited to, Florida Housing Corporation's Funding and Financing Programs (a conduit for both Federal and State programs).
- b) **Identify and engage Florida-active affordable multi-family housing developers.**
- c) **Establish partnerships with faith-based, not-for-profit development** to provide community outreach and identify potential residents.
- d) **Incentivize affordable housing developments.** Consider zoning updates, reduce parking requirements, and designate affordable housing as-of-right use with short approval processes.

# Recommendation #3. Increase Access to Parks and Open Spaces

1. **Increase access to natural resources and assets and activate community gathering spaces.**
  - a) **Regional East-West bicycle connection** along Jackson Street between the City of Pensacola Downtown and the Bayou Marcus.
  - b) **Regional North-South connections** between Oakcrest elementary school and planned multimodal infrastructure along US Hwy 98 and Bill Gregory Park.
  - c) **Local connections to Jackson Creek, a local hidden gem.** Conduct a feasibility analysis for a multi-use walking/biking path along Citrus Street across the railroad to Jackson Creek, alternatively, along Ross Street to Idlewood Drive.

Multimodal transportation infrastructure and transit connections to these destinations are noted in #1 Provide Safe Accessible Mobility

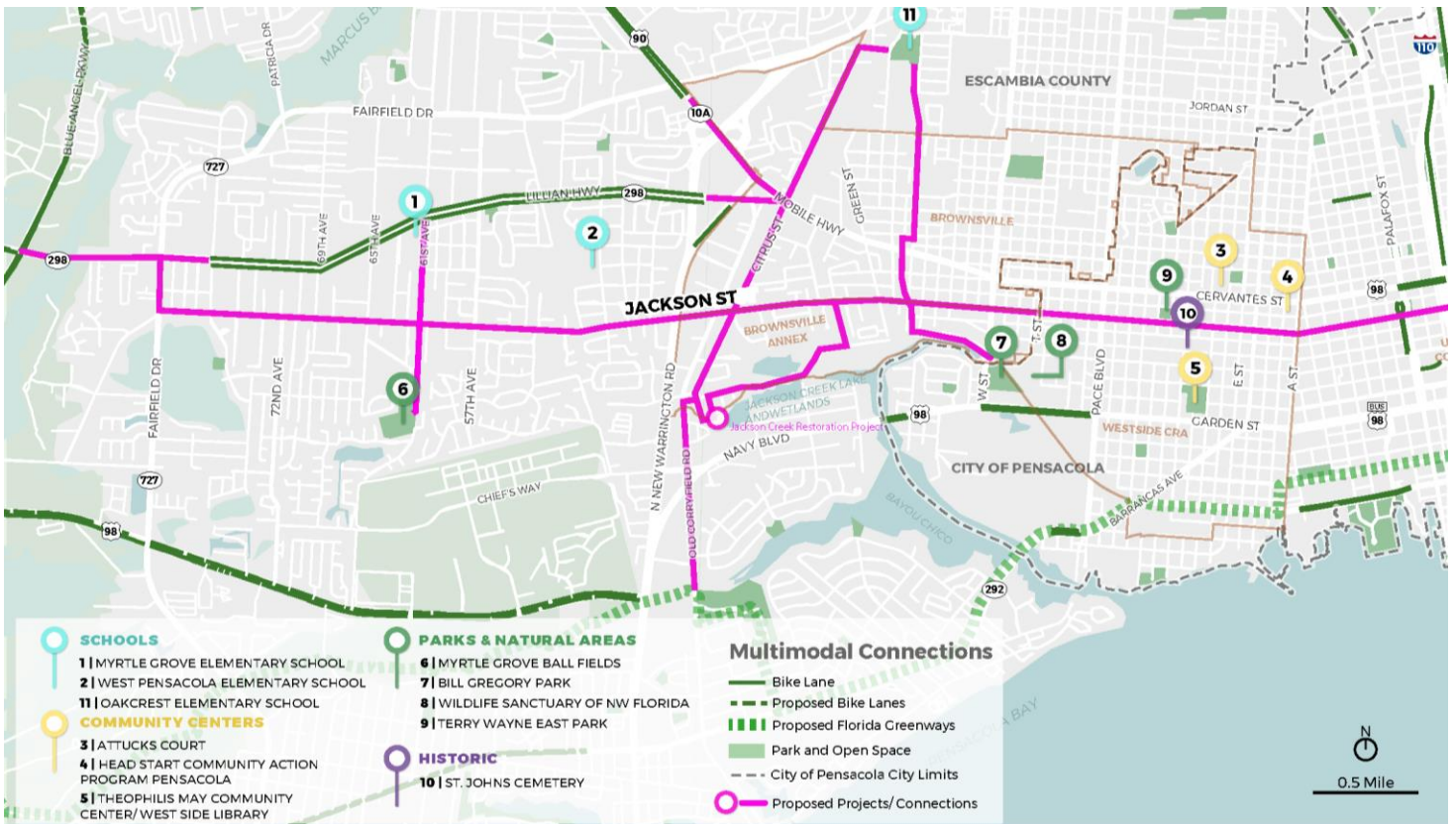


Figure 7: Recommended Regional Multimodal Connections



# Next Steps

## Towards Reimagine Jackson Street Implementation

Short-term recommendations and long-term strategies were identified through the master planning process. Recommended immediate next steps are:

- 1) Identify and apply for the appropriate grants for engineering design per segment or combine segments for large grants, identify match funding and necessary partnerships.

	Engineering Design Cost	Stack Eligible Grant Funding to pursue	Match Funding	Partnerships
<b>Segment 1</b> (City-owned ROW)	\$938,000			
<b>Segment 2</b> (County-owned ROW)	\$1,868,000			
<b>Segment 3</b> (County-owned ROW)	\$2,576,000			

- 2) Coordinate with the Florida Department of Transportation (FDOT) and Transportation Planning Organization

	Engineering Cost	Engineering & Construction Cost
<b>Fairfield Drive intersection</b> (FDOT-owned ROW)	\$42,000	\$280,772.43
<b>New Warrington Road intersection</b> (FDOT-owned ROW)	\$19,120	\$110,800
<b>Pace Boulevard intersection</b> (FDOT-owned ROW)	\$150,000	\$1,009,000

- 3) Identify opportunities and partnerships to preserve historic assets and celebrate community identity.

