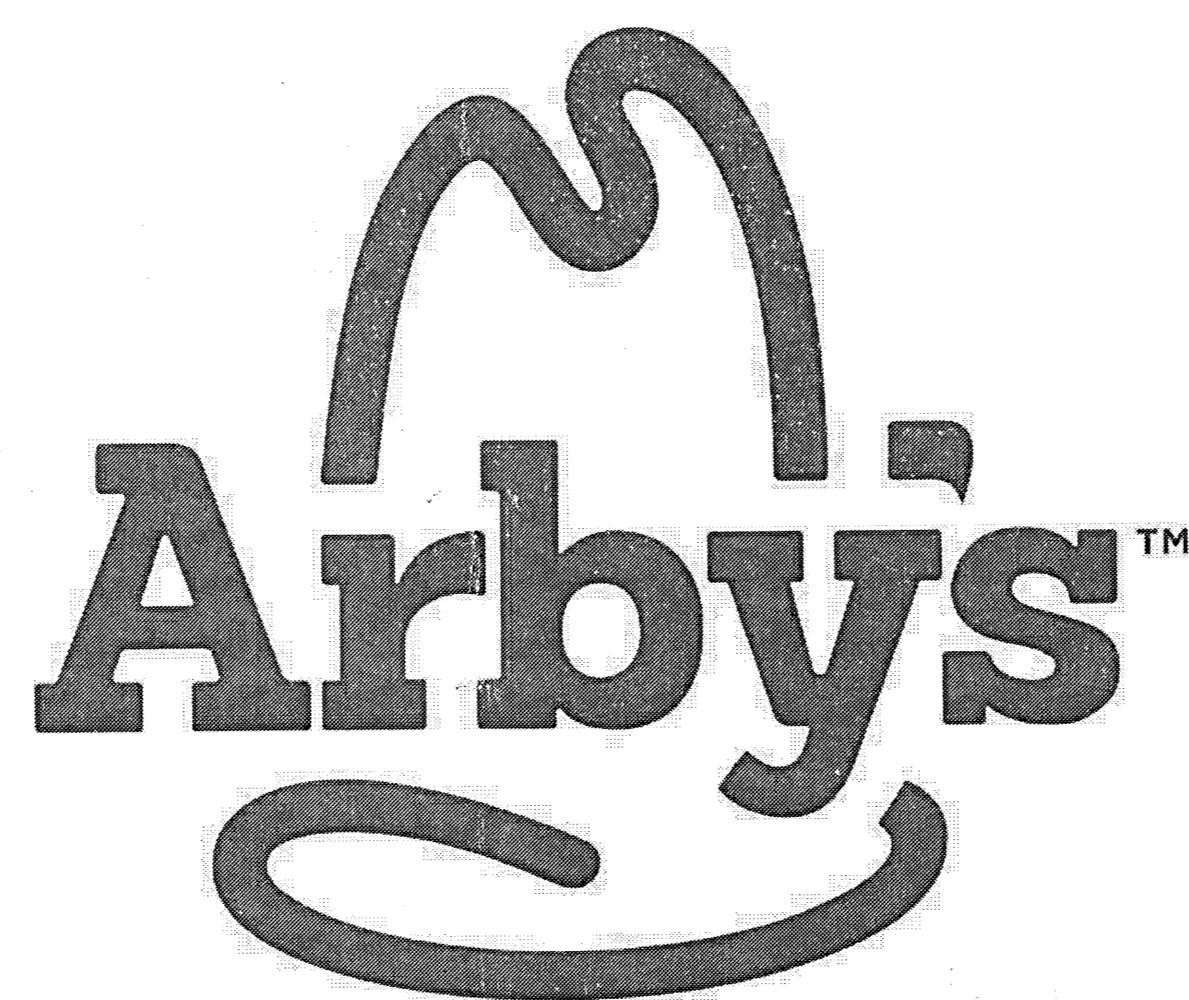
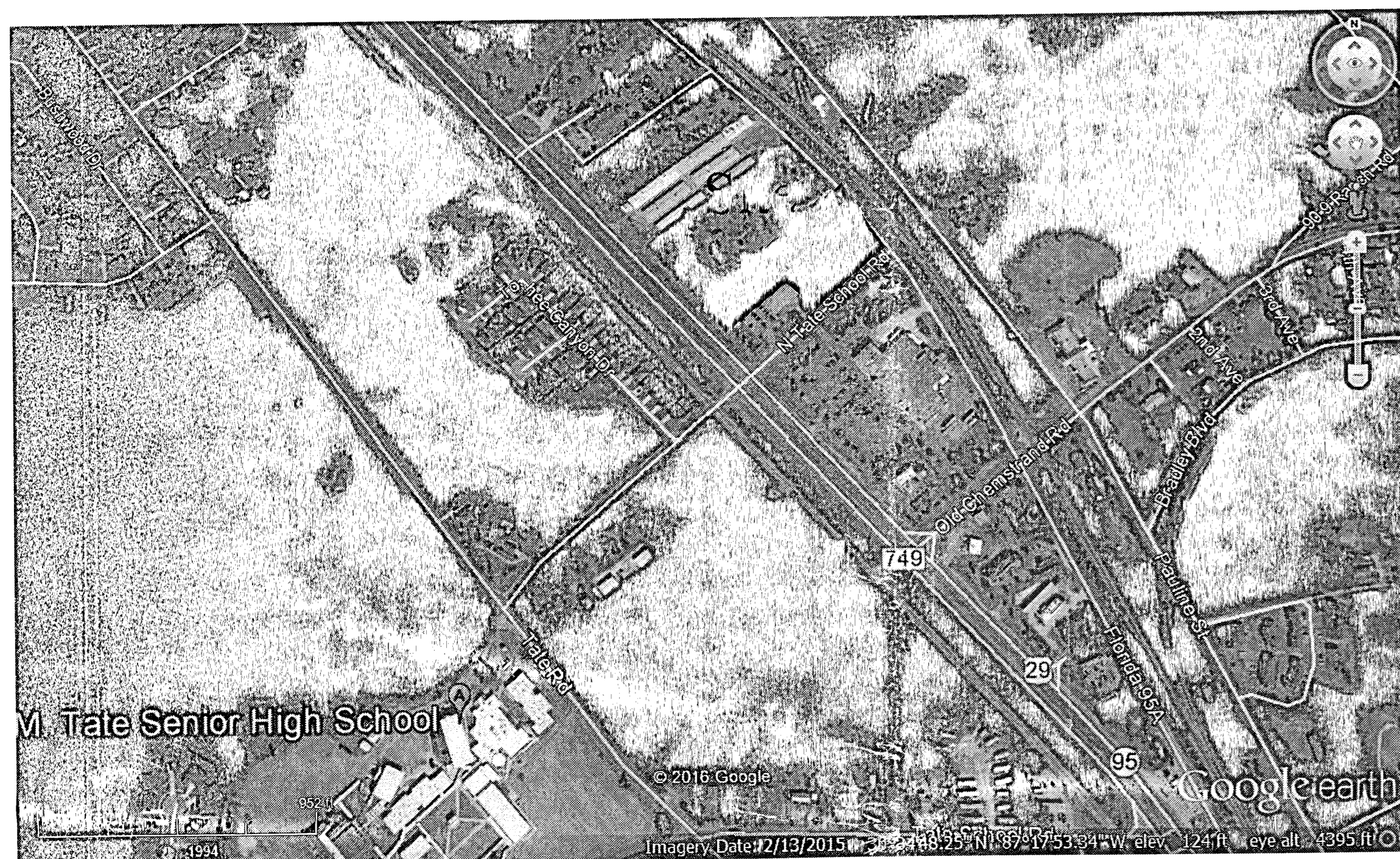


## Vicinity Map



## Arby's Cantonment 1494 S Highway 29 Cantonment

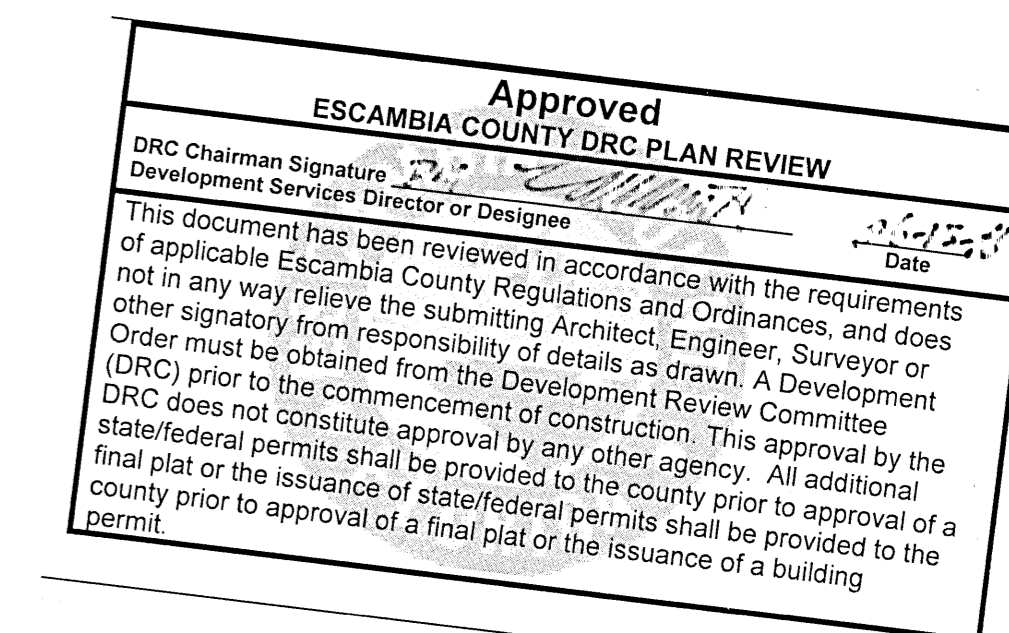
Prepared for:

**Beavers, Inc.**

50 Beal Parkway SW, Suite 9  
Fort Walton Beach, FL 32547

Phone (850)664-2500

Contact: Kenny Beavers



## Sheet Index

- |    |                           |
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| 1  | Site Plan                 |
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| 5  | Details                   |
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| 7  | Details                   |
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| 9  | FDOT Details              |
| 10 | FDOT Details              |

### DESCRIPTION OF RECORD:

COMMENCING AT A 4" SQUARE CONCRETE MONUMENT WITH A DISK MARKED AS THE S.W. CORNER OF SECTION 14, T-1-N, R-31-W, ESCAMBIA COUNTY, FLORIDA; THENCE N38°59'57"W, ALONG THE WESTERLY LINE OF SAID SECTION 14, FOR 906.57'; THENCE N50°36'10"E AND PARALLEL WITH THE SOUTH LINE OF SAID SECTION FOR 1251.80' AND THE INTERSECTION OF THE EASTERLY RIGHT-OF-WAY OF U.S. HIGHWAY 29 (200' R/W) AND THE NORTHERLY RIGHT-OF-WAY LINE OF TATE SCHOOL ROAD (100' R/W); THENCE N46°06'00"E, ALONG SAID NORTHERLY RIGHT-OF-WAY LINE FOR 300.00' TO AN IRON ROD AND CAP FOR THE POINT OF BEGINNING; THENCE CONTINUE N46°06'00"E, ALONG SAME COURSE FOR 333.12' TO AN IRON ROD AND CAP MARKED #3578 ON THE WESTERLY RIGHT-OF-WAY LINE OF COUNTY ROAD #95A (66' R/W) SAID POINT BEING ON A CURVE CONCAVE TO THE SOUTHWEST AND HAVING A RADIUS OF 3,531.75' A CHORD BEARING OF N37°45'19"W, WITH A DISTANCE OF 224.89'; THENCE NORTHWESTERLY ALONG THE ARC OF SAID CURVE FOR 224.92' TO AN IRON ROD AND CAP MARKED #3774; THENCE S50°36'10"W AND PARALLEL WITH THE SOUTH LINE OF SAID SECTION FOR 656.69' TO AN IRON ROD AND CAP MARKED #3578 ON THE EASTERLY RIGHT-OF-WAY OF THE AFORESAID U.S. HIGHWAY #29; THENCE S43°35'03"E, ALONG SAID EASTERLY RIGHT-OF-WAY LINE FOR 125.15' TO AN IRON ROD AND CAP MARKED #3578; THENCE N46°07'27"E, FOR 300.00' TO AN IRON ROD AND CAP MARKED #3578; THENCE S43°34'53"E, FOR 150.00' TO THE POB.

Prepared by:

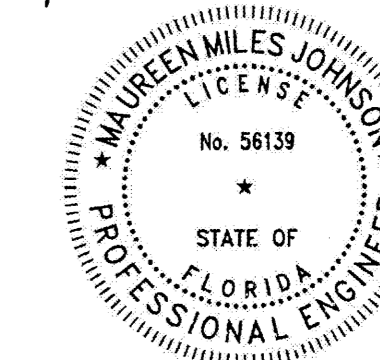
**Maureen M. Johnson, PE, PA**

4423 Windlake Drive ~ Niceville, FL 32578

Phone 850.279.6670

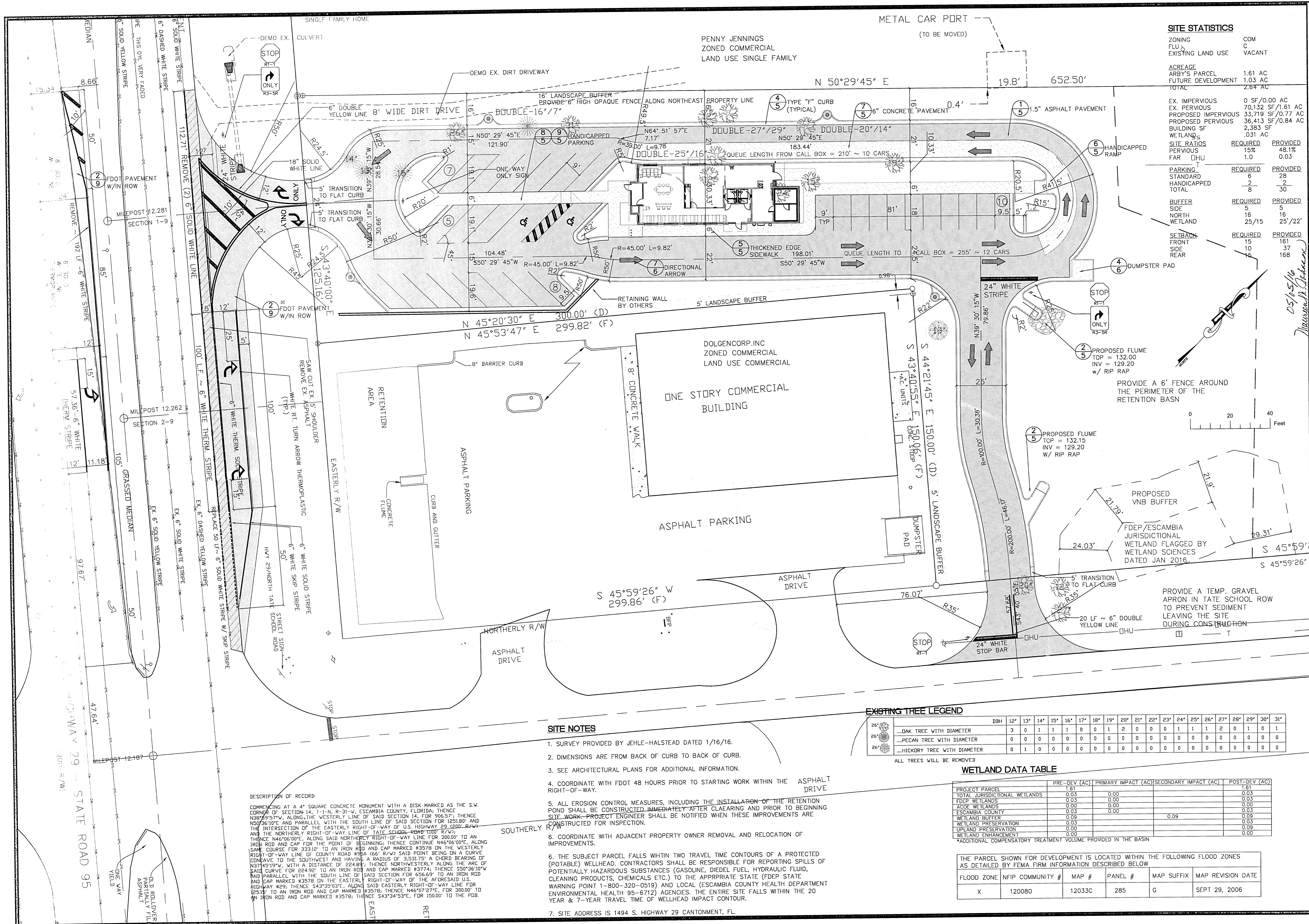
FL PE #56139

05/05/16  
*Maureen M. Johnson*



NOT RELEASED FOR  
CONSTRUCTION

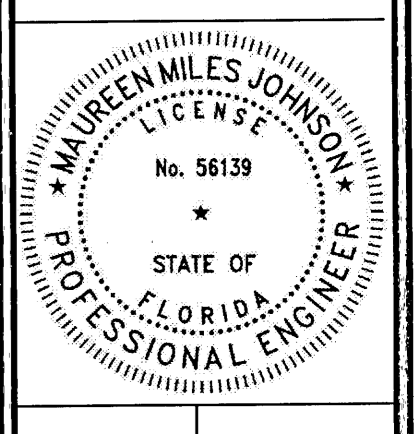
05/05/2016



**SITE STATISTICS**

FLU & EXISTING LAND USE	COM C VACANT
ACREAGE	
ARBY'S PARCEL	1.61 AC
FUTURE DEVELOPMENT	1.03 AC
TOTAL	2.64 AC
EX. IMPERVIOUS	0 SF/0.00 AC
EX. PERVIOUS	70,132 SF/1.61 AC
PROPOSED IMPERVIOUS	33,719 SF/0.77 AC
PROPOSED PERVIOUS	36,413 SF/0.84 AC
BUILDING SF	2,383 SF
WETLANDS	.031 AC
SITE RATIOS	
PERVIOUS	15%
FAR	1.0
OHU	0.03
REQUIRED	PROVIDED
PARKING STANDARD	28
HANDICAPPED	2
TOTAL	30
REQUIRED	PROVIDED
BUFFER SIDE	5
NORTH	16
WETLAND	25/15
25'/22'	
REQUIRED	PROVIDED
SETBACK FRONT	15
SIDE	10
REAR	37
	168

**Maureen M. Johnson, PE, PA**  
 4423 Windlake Drive ~ Niceville, FL 32578  
 Phone 850.279.6670/850.699.0294  
 FL PE #56139



**Arby's - Cantonment**  
**Site Plan**

**NOT RELEASED FOR CONSTRUCTION**

SCALE: 1"=20'  
 DATE: 05/05/16  
 SHEET: 1 OF 10

**SITE NOTES**

1. SURVEY PROVIDED BY JEHL-HALSTEAD DATED 1/16/16.
2. DIMENSIONS ARE FROM BACK OF CURB TO BACK OF CURB.
3. SEE ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
4. COORDINATE WITH FDOT 48 HOURS PRIOR TO STARTING WORK WITHIN THE RIGHT-OF-WAY.
5. ALL EROSION CONTROL MEASURES, INCLUDING THE INSTALLATION OF THE RETENTION POND SHALL BE CONSTRUCTED IMMEDIATELY AFTER CLEARING AND PRIOR TO BEGINNING SITE WORK. PROJECT ENGINEER SHALL BE NOTIFIED WHEN THESE IMPROVEMENTS ARE CONSTRUCTED FOR INSPECTION.
6. COORDINATE WITH ADJACENT PROPERTY OWNER REMOVAL AND RELOCATION OF IMPROVEMENTS.
7. THE SUBJECT PARCEL FALLS WITHIN TWO TRAVEL TIME CONTOURS OF A PROTECTED (POTABLE) WELLHEAD. CONTRACTORS SHALL BE RESPONSIBLE FOR REPORTING SPILLS OF POTENTIALLY HAZARDOUS SUBSTANCES (GASOLINE, DIESEL FUEL, HYDRAULIC FLUID, CLEANING PRODUCTS, CHEMICALS ETC.) TO THE APPROPRIATE STATE (FDEP STATE WARNING POINT 1-800-320-0519) AND LOCAL (ESCAMBIA COUNTY HEALTH DEPARTMENT ENVIRONMENTAL HEALTH 95-6712) AGENCIES. THE ENTIRE SITE FALLS WITHIN THE 20 YEAR & 7-YEAR TRAVEL TIME OF WELLHEAD IMPACT CONTOUR.
8. SITE ADDRESS IS 1494 S. HIGHWAY 29 CANTONMENT, FL.

**EXISTING TREE LEGEND**

	DBH	12"	13"	14"	15"	16"	17"	18"	19"	20"	21"	22"	23"	24"	25"	26"	27"	28"	29"	30"	31"
DAK TREE WITH DIAMETER	3	0	1	1	1	0	0	1	2	0	0	0	1	1	1	2	0	1	0	1	
PECAN TREE WITH DIAMETER	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
HICKORY TREE WITH DIAMETER	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

ALL TREES WILL BE REMOVED

**WETLAND DATA TABLE**

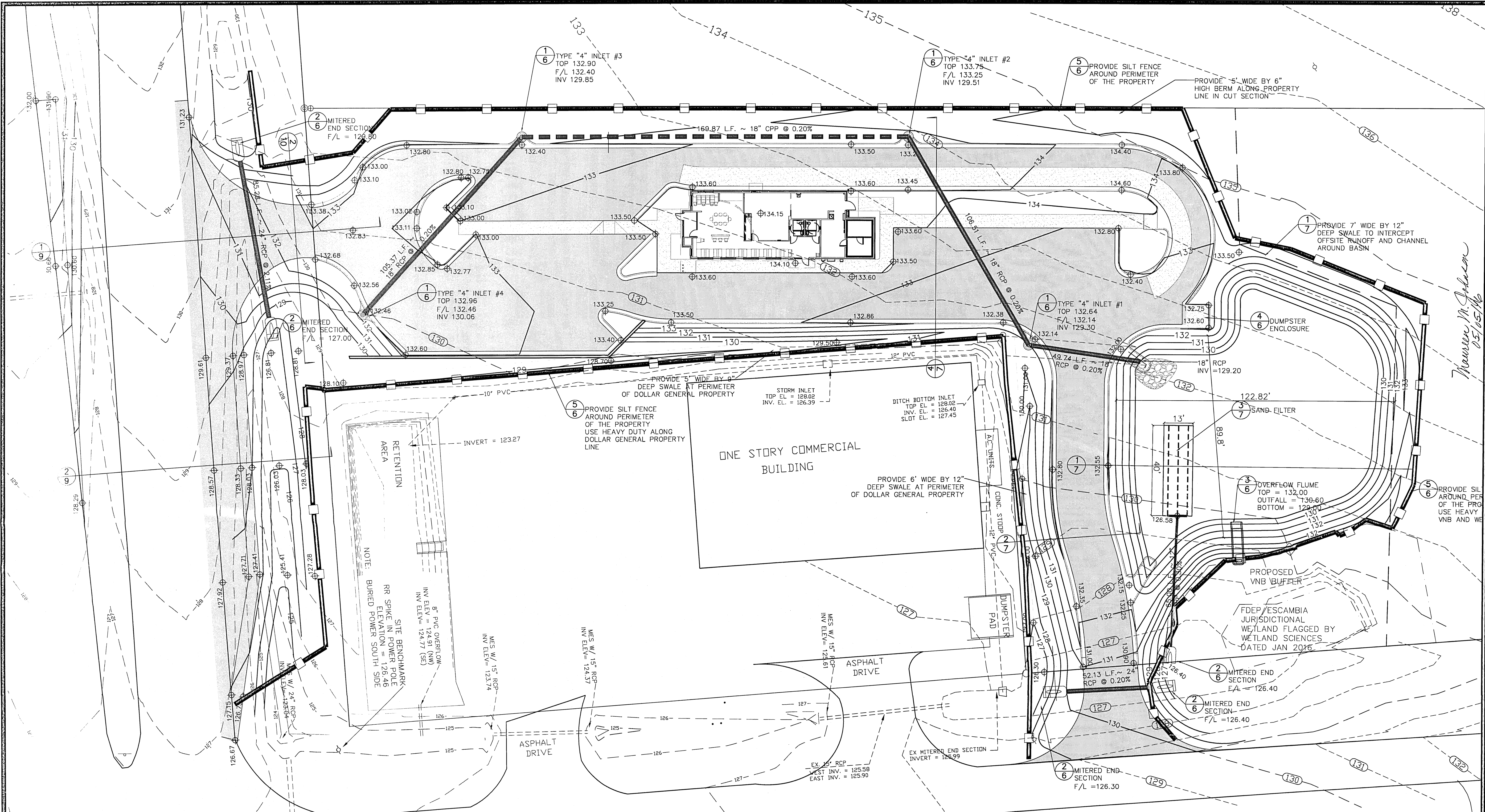
	PRE-DEV (AC)	PRIMARY IMPACT (AC)	SECONDARY IMPACT (AC)	POST-DEV (AC)
PROJECT PARCEL	1.61			1.61
TOTAL JURISDICTIONAL WETLANDS	0.03	0.00		0.03
FDEP WETLANDS	0.03	0.00		0.03
ACOE WETLANDS	0.00	0.00		0.00
ESCAMBIA COUNTY WETLAND BUFFER	0.03	0.00	0.09	0.03
WETLAND PRESERVATION	0.03			0.03
UPLAND PRESERVATION	0.00			0.09
WETLAND ENHANCEMENT	0.00			0.00

\*ADDITIONAL COMPENSATORY TREATMENT VOLUME PROVIDED IN THE BASIN

THE PARCEL SHOWN FOR DEVELOPMENT IS LOCATED WITHIN THE FOLLOWING FLOOD ZONES AS DETAILED BY FEMA FIRM INFORMATION DESCRIBED BELOW

FLOOD ZONE	NFIP COMMUNITY #	MAP #	PANEL #	MAP SUFFIX	MAP REVISION DATE
X	120080	12033C	285	G	SEPT 29, 2006

**DESCRIPTION OF RECORD:**  
 COMMENCING AT A 4" SQUARE CONCRETE MONUMENT WITH A DISK MARKED AS THE S.W. CORNER OF SECTION 14, T-1-N, R-31-W, ESCAMBIA COUNTY, FLORIDA; THENCE N88°59'37"W, ALONG THE WESTERLY LINE OF SAID SECTION 14, FOR 906.57'; THENCE N50°36'10"E AND PARALLEL WITH THE SOUTH LINE OF SAID SECTION FOR 1251.80' AND THE INTERSECTION OF THE EASTERLY RIGHT-OF-WAY OF U.S. HIGHWAY 29 (300' R/W) AND THE NORTHERLY RIGHT-OF-WAY LINE OF TATE SCHOOL ROAD (100' R/W); THENCE N46°06'00"E, ALONG SAID NORTHERLY RIGHT-OF-WAY LINE FOR 300.00' TO AN IRON ROD AND CAP FOR THE POINT OF BEGINNING; THENCE CONTINUE N46°06'00"E, ALONG SAME COURSE FOR 332.12' TO AN IRON ROD AND CAP MARKED #3578 ON THE WESTERLY RIGHT-OF-WAY LINE OF COUNTY ROAD #95A (66' R/W) SAID POINT BEING ON A CURVE CONCAVE TO THE SOUTHWEST AND HAVING A RADIUS OF 3,531.75'; A CHORD BEARING OF N37°48'19"W, WITH A DISTANCE OF 224.88'; THENCE NORTHWESTERLY ALONG THE ARC OF SAID CURVE FOR 224.88' TO AN IRON ROD AND CAP MARKED #3774; THENCE S50°36'10"W AND PARALLEL WITH THE SOUTH LINE OF SAID SECTION FOR 656.69' TO AN IRON ROD AND CAP MARKED #3578 ON THE EASTERLY RIGHT-OF-WAY OF THE AFORESAID U.S. HIGHWAY 29; THENCE S43°39'12"E, ALONG SAID EASTERLY RIGHT-OF-WAY LINE FOR 1251.15' TO AN IRON ROD AND CAP MARKED #3578; THENCE N46°07'27"E, FOR 300.00' TO AN IRON ROD AND CAP MARKED #3578; THENCE S43°34'53"E, FOR 150.00' TO THE PUB. EAST

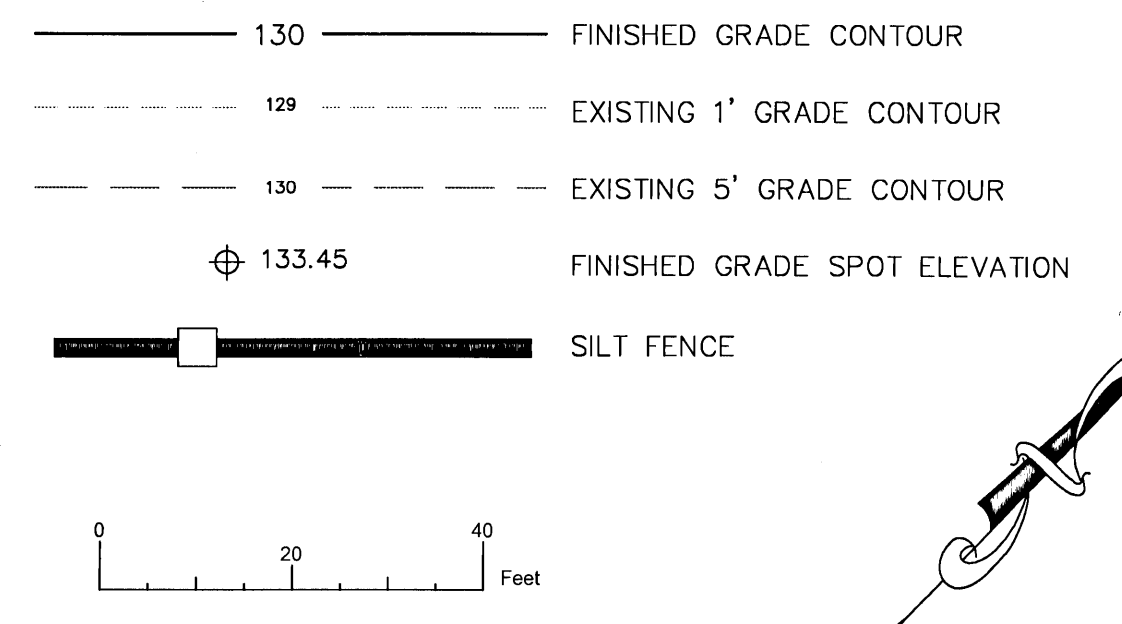


**GRADING NOTES**

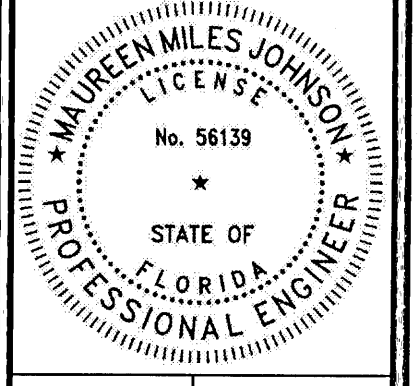
- TOPOGRAPHIC SURVEY PROVIDED BY JEHL-HALSTEAD DATE 1/16/16.
- BENCHMARK LOCATED AT THE SOUTH PROPERTY CORNER WITH DOLLAR GENERAL, RR SPIKE IN POWER POLE EL 126.46. ADDITIONAL BENCHMARK LOCATED AT THE NORTHWEST CORNER OF THE INTERSECTION WITH COUNTY ROAD 95-A & TATE SCHOOL RD. NGS 162, EL 139.37
- FINISHED GRADE SPOTS ARE AT FLOWLINE OF CURB.
- THE PROJECT ENGINEER SHALL PROVIDE TO ESCAMBIA COUNTY "AS-BUILT" RECORD DRAWINGS FOR VERIFICATION AND APPROVAL BY ESCAMBIA COUNTY ONE WEEK PRIOR TO REQUESTING A FINAL INSPECTION AND CERTIFICATE OF OCCUPANCY, OR PROVIDE "AS-BUILT" CERTIFICATION THAT THE PROJECT CONSTRUCTION ADHERES TO THE PERMITTED PLANS AND SPECIFICATION. THE "AS-BUILT" CERTIFICATION OR THE "AS-BUILT" RECORD DRAWINGS MUST BE SIGNED, SEALED, AND DATED BY A REGISTERED FLORIDA PROFESSIONAL ENGINEER.
- ALL ASPECTS OF THE STORMWATER/DRAINAGE COMPONENTS AND/OR TRANSPORTATION COMPONENTS SHALL BE COMPLETED PRIOR TO ISSUANCE OF A FINAL CERTIFICATE OF OCCUPANCY.
- NO DEVIATIONS OR REVISIONS FROM THESE PLANS BY THE CONTRACTOR SHALL BE ALLOWED WITHOUT PRIOR APPROVAL FROM BOTH THE DESIGN ENGINEER AND ESCAMBIA COUNTY. ANY DEVIATIONS MAY RESULT IN DELAYS IN OBTAINING A CERTIFICATE OF OCCUPANCY.
- THE CONTRACTOR SHALL INSTALL PRIOR TO CONSTRUCTION AND MAINTAIN DURING CONSTRUCTION ALL SEDIMENT CONTROL MEASURES AS REQUIRED TO RETAIN ALL SEDIMENTS ON THE SITE. IMPROPER SEDIMENT CONTROL MEASURES MAY RESULT IN CODE ENFORCEMENT VIOLATION. FILTER FABRIC SHALL BE PROVIDED OVER THE INLETS UNTIL PERMANENT STABILIZATION/FINAL PAVEMENT IS INSTALLED.
- RETENTION/DETENTION AREAS SHALL BE SUBSTANTIALLY COMPLETE PRIOR TO ANY CONSTRUCTION ACTIVITIES THAT MAY INCREASE STORMWATER RUNOFF RATES. THE CONTRACTOR SHALL CONTROL STORMWATER DURING ALL PHASES OF CONSTRUCTION AND THE ADEQUATE MEASURES TO PREVENT THE EXCAVATED POND FROM BLINDING DUE TO SEDIMENTS.
- ALL DISTURBED AREAS WHICH ARE NOT PAVED SHALL BE STABILIZED WITH SEEDING, FERTILIZER AND MULCH, HYDROSEED AND/OR SOD.
- ALL NEW BUILDING ROOF DRAINS, DOWN SPOUTS, OR GUTTERS SHALL BE ROUTED TO CARRY ALL STORMWATER TO RETENTION/DETENTION AREAS.
- DEVELOPER/CONTRACTOR SHALL RESHAPE PER PLAN SPECIFICATIONS, CLEAN OUT ACCUMULATED SILT, AND STABILIZE RETENTION/DETENTION PONDS AT THE END OF CONSTRUCTION WHEN ALL DISTURBED AREAS HAVE BEEN STABILIZED AND PRIOR TO REQUEST FOR INSPECTION.

- CONTRACTOR SHALL MAINTAIN RECORD DRAWINGS DURING CONSTRUCTION WHICH SHOW "AS-BUILT" CONDITIONS OF ALL WORK INCLUDING PIPING, DRAINAGE STRUCTURES, TOPO OF PONDS, OUTLET STRUCTURES, DIMENSIONS, ELEVATIONS, GRADING, ETC. RECORD DRAWINGS SHALL BE PROVIDED TO THE ENGINEER OF RECORD PRIOR TO REQUESTING FINAL INSPECTION.
- THE OWNER OF HIS AGENT SHALL BE ARRANGE/SCHEDULE WITH THE COUNTY A FINAL INSPECTION OF THE DEVELOPMENT UPON COMPLETION AND ANY INTERMEDIATE INSPECTIONS (850) 595-3472. AS BUILT CERTIFICATION IS REQUIRED PRIOR TO REQUEST FOR FINAL INSPECTION/APPROVAL.
- NOTIFY SUNSHINE UTILITIES 48 HOURS IN ADVANCE PRIOR TO DIGGING (800)432-4770.
- ANY DAMAGE TO EXISTING ROADS DURING CONSTRUCTION WILL BE REPAIRED BY THE DEVELOPER PRIOR TO FINAL AS-BUILT SIGN OFF FROM THE COUNTY.
- THE CONTRACTOR SHALL NOTIFY FDOT 48 HOURS IN ADVANCE PRIOR TO INITIATING ANY WORK IN THE STATES RIGHT-OF-WAY.
- R/W SHOULDER SHALL BE STABILIZED IN ACCORDANCE WITH FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION LATEST EDITION.
- CLEARING LIMITS ARE REPRESENTED BY FINISHED GRADE CONTOURS. RETAIN EXISTING VEGETATION ADJACENT TO PROPERTY LINES.
- MAUREEN M. JOHNSON, PE WILL BE RESPONSIBLE FOR MONITORING CONSTRUCTION, SUBMITTING NOTICE OF COMMENCEMENT, AND AS-BUILT CERTIFICATIONS TO THE NFWMD. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS PRIOR TO STARTING CONSTRUCTION. ALL SILT FENCE SHALL BE INSTALLED PRIOR TO CLEARING AND BASINS SHALL BE ROUGH GRADED AND MAINTAINED PRIOR TO ANY GRADING. SEE STORMWATER MANAGEMENT PLAN FOR FURTHER CONSTRUCTION SEQUENCING. THE BOTTOM OF THE BASINS MUST BE SCRAPPED AND FULLY STABILIZED PRIOR TO AS-BUILT CERTIFICATION.
- SEE GEOTECH REPORT PROVIDED BY NOVA DATED DECEMBER 17, 2015.
- CONTRACTOR SHALL PROVIDE AS-BUILT SURVEY OF INLETS, STORM STRUCTURES, FILTER, AND RETENTION BASIN.
- PRIOR TO CONSTRUCTION A SEPARATE BUILDING INSPECTION DEPARTMENT PERMIT(S) SHALL BE OBTAINED FOR ALL RETAINING WALLS OVER 2'.
- THESE PLANS DO NOT INCLUDE ANY DRAINAGE, ACTUAL OR IMPLIED, FOR THE RETAINING WALLS.

**GRADING LEGEND**



**Maureen M. Johnson, PE, PA**  
 4423 Windlake Drive ~ Niceville, FL 32578  
 Phone 850.279.6670/850.699.0294  
 FL PE #56139



**Arby's - Cantonment**  
**Grading Plan**

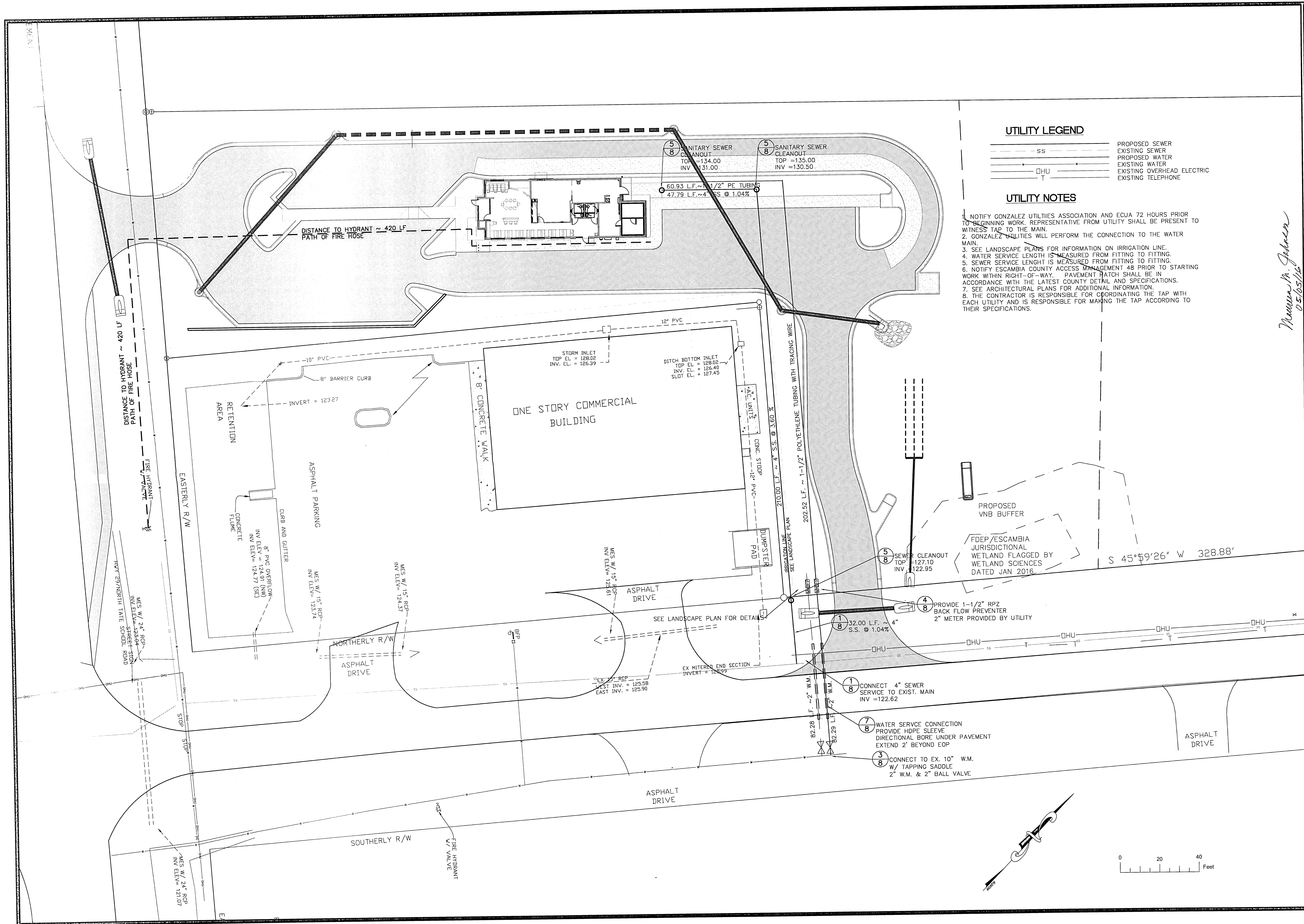
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SCALE: 1"=20'

DATE: 05/05/2016

SHEET: 2 OF 10

2



**UTILITY LEGEND**

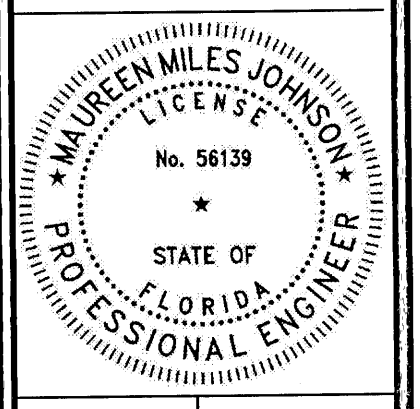
- SS --- PROPOSED SEWER
- S --- EXISTING SEWER
- W --- PROPOSED WATER
- W --- EXISTING WATER
- OHU --- EXISTING OVERHEAD ELECTRIC
- T --- EXISTING TELEPHONE

**UTILITY NOTES**

1. NOTIFY GONZALEZ UTILITIES ASSOCIATION AND ECUA 72 HOURS PRIOR TO BEGINNING WORK. REPRESENTATIVE FROM UTILITY SHALL BE PRESENT TO WITNESS TAP TO THE MAIN.
2. GONZALEZ UTILITIES WILL PERFORM THE CONNECTION TO THE WATER MAIN.
3. SEE LANDSCAPE PLANS FOR INFORMATION ON IRRIGATION LINE.
4. WATER SERVICE LENGTH IS MEASURED FROM FITTING TO FITTING.
5. SEWER SERVICE LENGTH IS MEASURED FROM FITTING TO FITTING.
6. NOTIFY ESCAMBIA COUNTY ACCESS MANAGEMENT 48 PRIOR TO STARTING WORK WITHIN RIGHT-OF-WAY. PAVEMENT PATCH SHALL BE IN ACCORDANCE WITH THE LATEST COUNTY DETAIL AND SPECIFICATIONS.
7. SEE ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.
8. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING THE TAP WITH EACH UTILITY AND IS RESPONSIBLE FOR MAKING THE TAP ACCORDING TO THEIR SPECIFICATIONS.

*Maureen M. Johnson*  
05165116

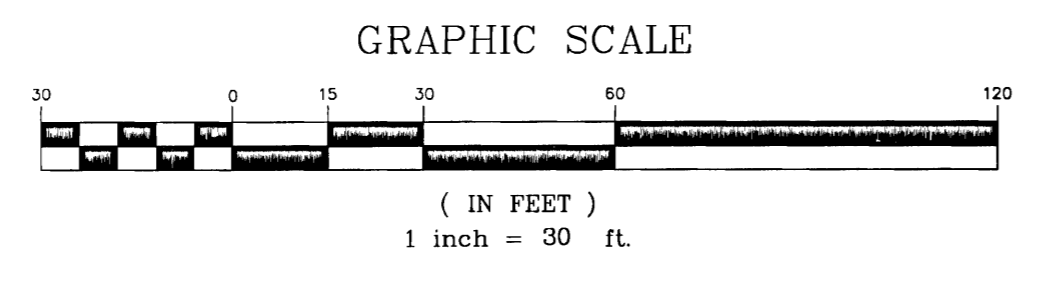
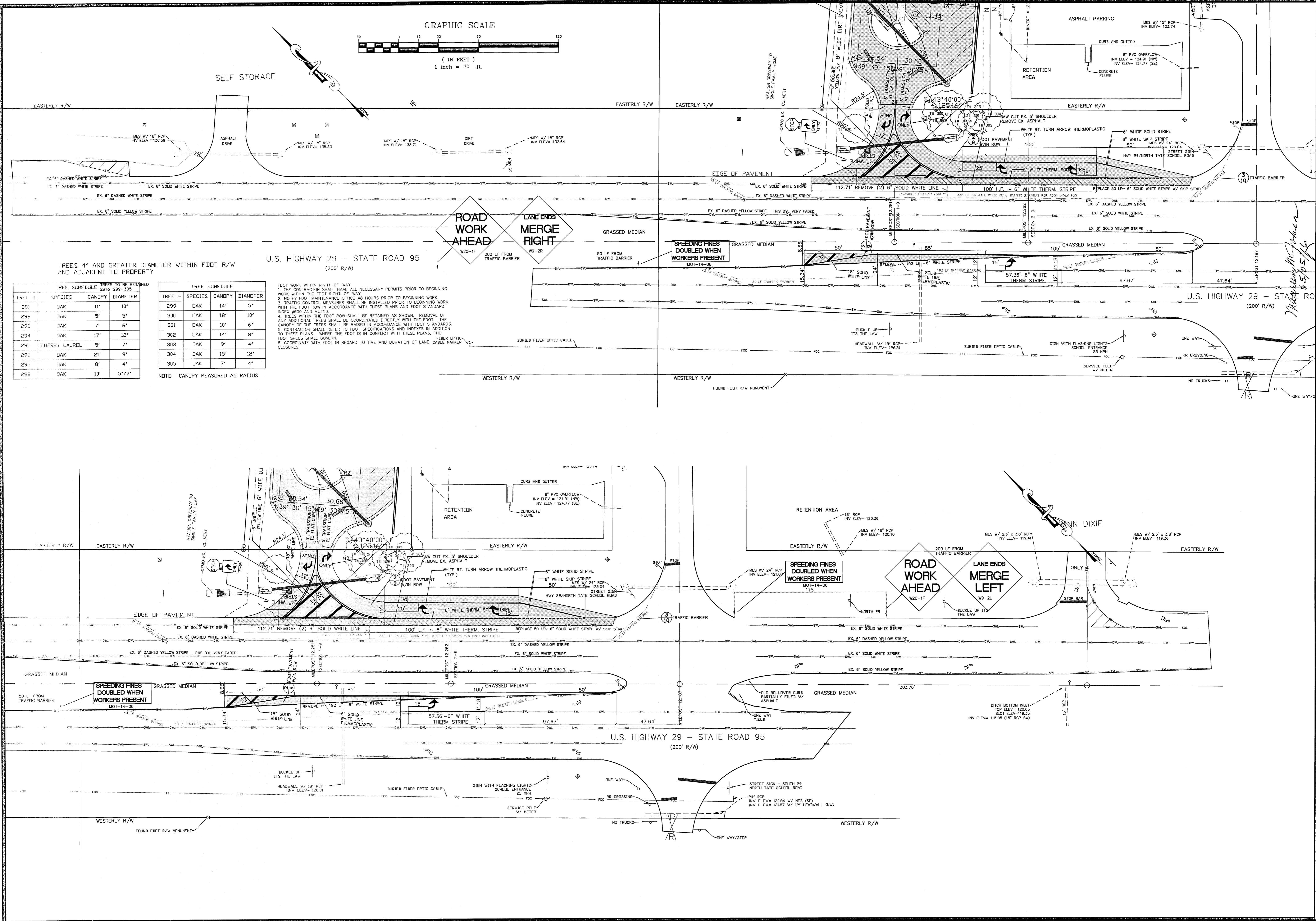
**Maureen M. Johnson, PE, PA**  
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Phone 850.279.6670/850.699.0294  
FL PE #56139



**Arby's - Cantonment**  
**Utility Plan**

**NOT RELEASED FOR CONSTRUCTION**

SCALE  
1"=20'  
DATE  
05/05/2016  
SHEET  
3 OF 10  
**3**



TREES TO BE RETAINED  
291A-299-305

TREE #	SPECIES	CANDPY	DIAMETER
291	DAK	11'	10"
292	DAK	5'	5"
293	DAK	7'	6"
294	DAK	17'	12"
295	CHERRY LAUREL	5'	7"
296	DAK	21'	9"
297	DAK	8'	4"
298	DAK	10'	5'/7"

TREE SCHEDULE

TREE #	SPECIES	CANDPY	DIAMETER
299	DAK	14'	5"
300	DAK	18'	10"
301	DAK	10'	6"
302	DAK	14'	8"
303	DAK	9'	4"
304	DAK	15'	12"
305	DAK	7'	4"

NOTE: CANDPY MEASURED AS RADIUS

FOOT WORK WITHIN RIGHT-OF-WAY  
1. THE CONTRACTOR SHALL HAVE ALL NECESSARY PERMITS PRIOR TO BEGINNING WORK WITHIN THE FDOT RIGHT-OF-WAY.  
2. NOTIFY FDOT MAINTENANCE OFFICE 48 HOURS PRIOR TO BEGINNING WORK.  
3. TRAFFIC CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING WORK WITH THE FDOT ROW IN ACCORDANCE WITH THESE PLANS AND FDOT STANDARD INDEX #600 AND MUTCD.  
4. TREES WITHIN THE FDOT ROW SHALL BE RETAINED AS SHOWN. REMOVAL OF ANY ADDITIONAL TREES SHALL BE COORDINATED DIRECTLY WITH THE FDOT. THE CANOPY OF THE TREES SHALL BE RAISED IN ACCORDANCE WITH FDOT STANDARDS.  
5. CONTRACTOR SHALL REFER TO FDOT SPECIFICATIONS AND INDEXES IN ADDITION TO THESE PLANS. WHERE THE FDOT IS IN CONFLICT WITH THESE PLANS, THE FDOT SPECS SHALL GOVERN.  
6. COORDINATE WITH FDOT IN REGARD TO TIME AND DURATION OF LANE CABLE MARKER CLOSURES.

**Maureen M. Johnson, PE, PA**  
4423 Windlake Drive ~ Niceville, FL 32578  
Phone 850.279.6670/850.699.0294  
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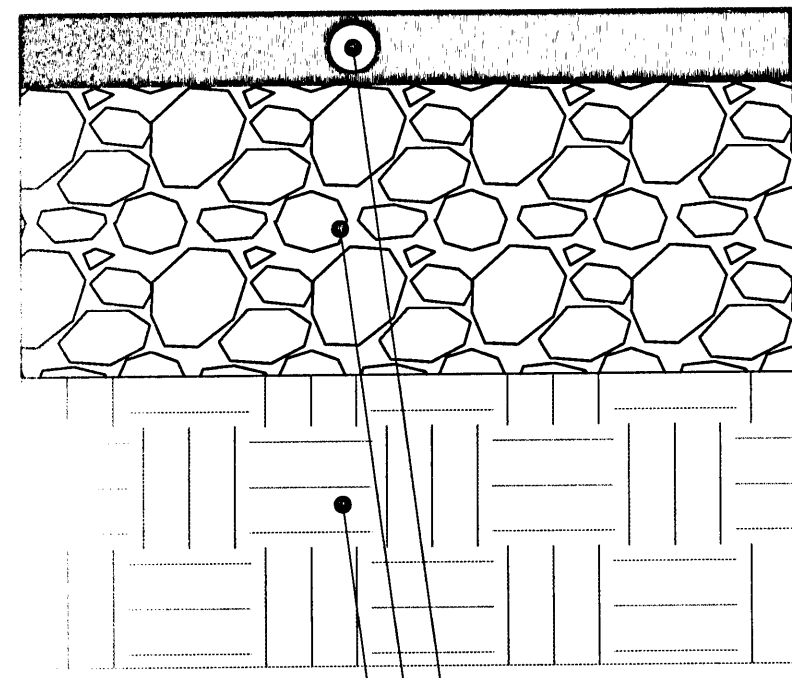
**Arby's - Cantonment**  
FDOT Connection Plan

NOT RELEASED FOR CONSTRUCTION

REVISIONS

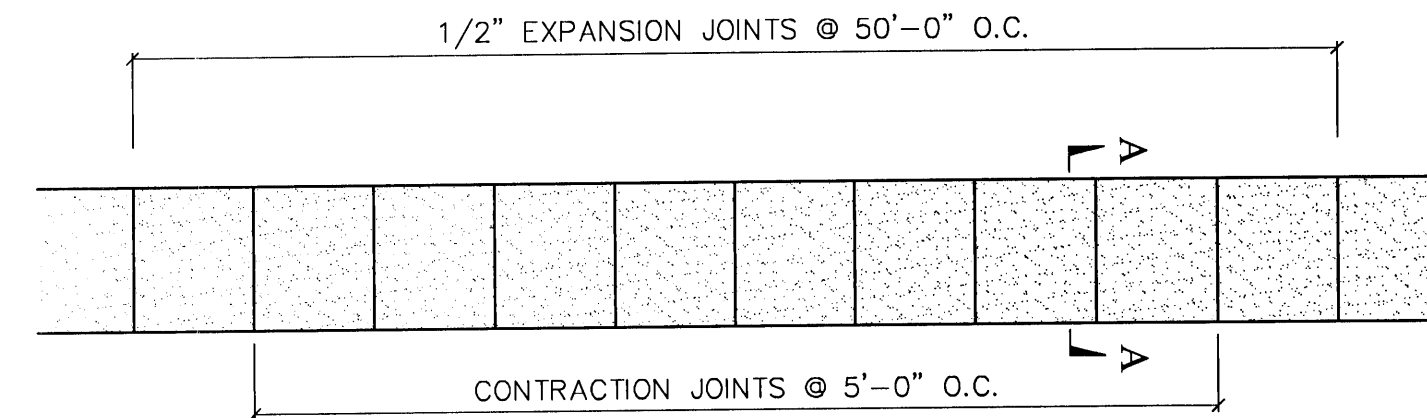
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DATE	05/05/2016
SHEET	4 OF 10

4

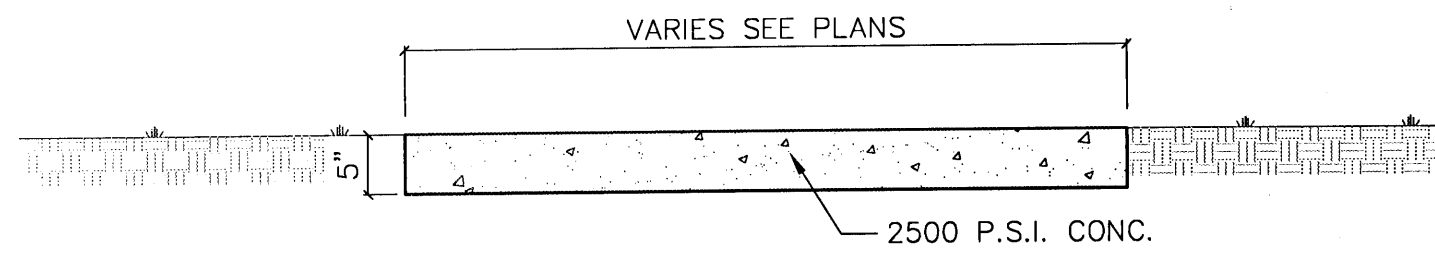


- 1-1/2" LAYER - FDOT SUPERPAVE-SP FINE
- OR-
- 2-1/2" LAYER - FDOT SUPERPAVE -SP FINE SECTION 334 WITHIN CUL-DE-SACS & INTERSECTIONS
- 6" LAYER - LIMEROCK BASE COURSE MIN LBR 100
- FILL AND TOP 12" OF SUBGRADE MIN LBR 40

**1**  
**5** ASPHALTIC CONCRETE PAVEMENT SECTION  
N.T.S.

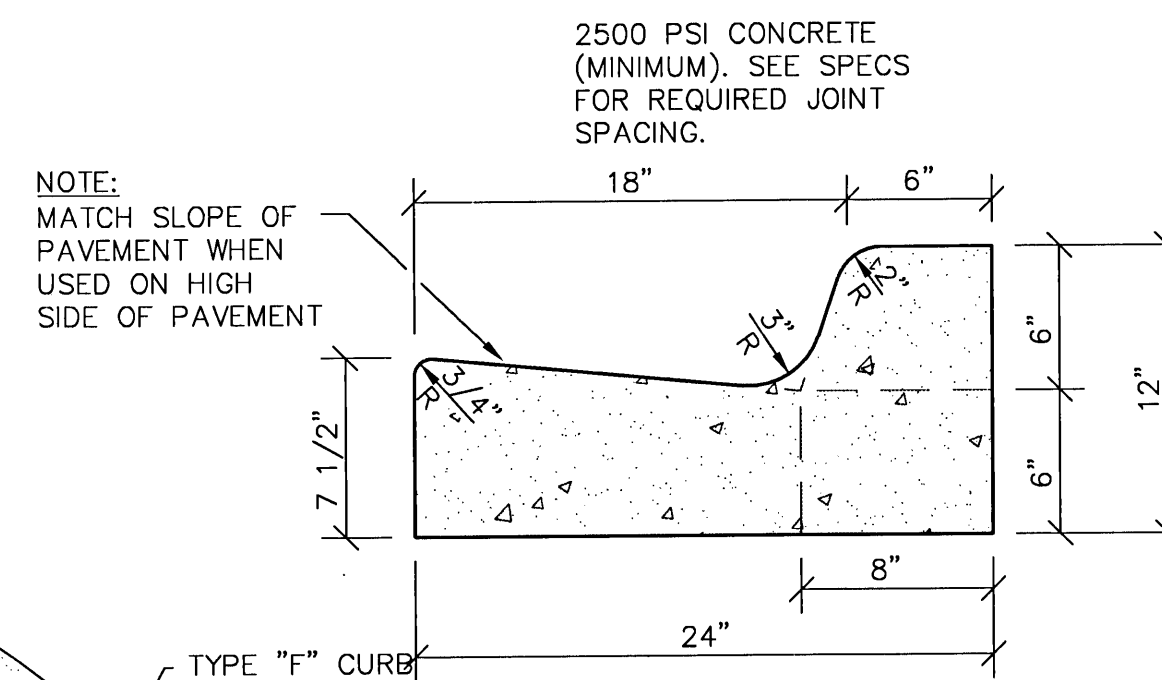


PLAN  
1/8" = 1'-0"



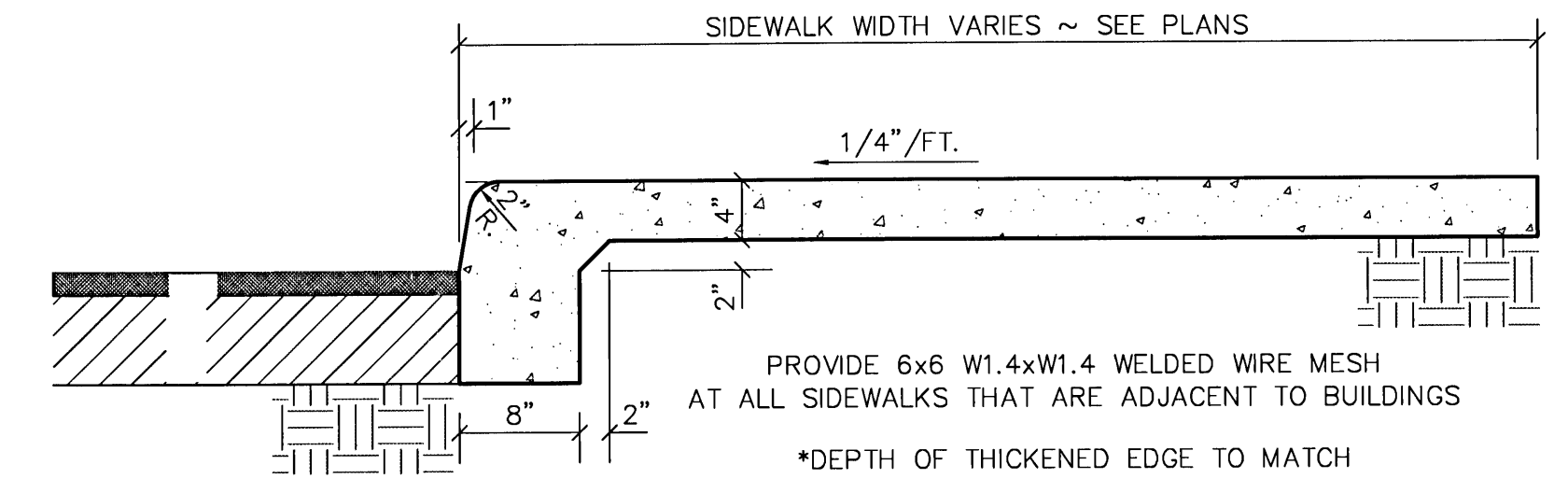
SECTION A-A  
3/4" = 1'-0"

**3**  
**5** CONCRETE SIDEWALK DETAIL  
SCALE: AS SHOWN

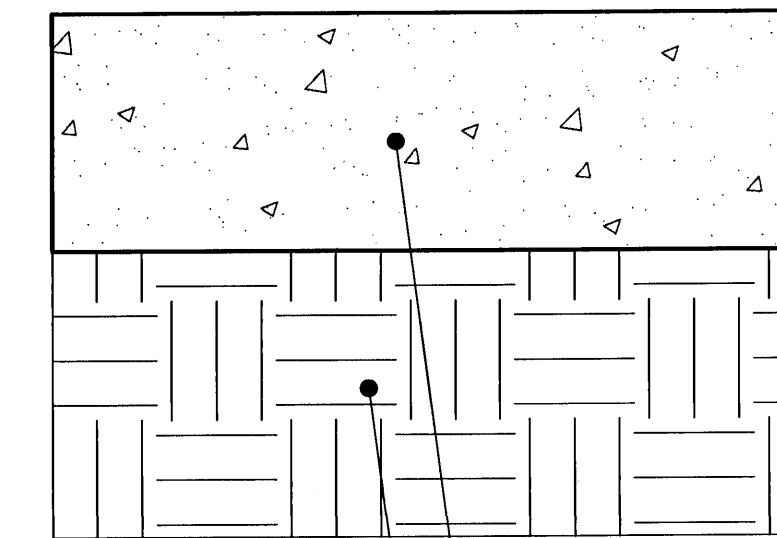


NOTE:  
MATCH SLOPE OF PAVEMENT WHEN USED ON HIGH SIDE OF PAVEMENT

**4**  
**5** TYPE "F" CURB AND GUTTER  
SCALE: 1-1/2" = 1'-0"

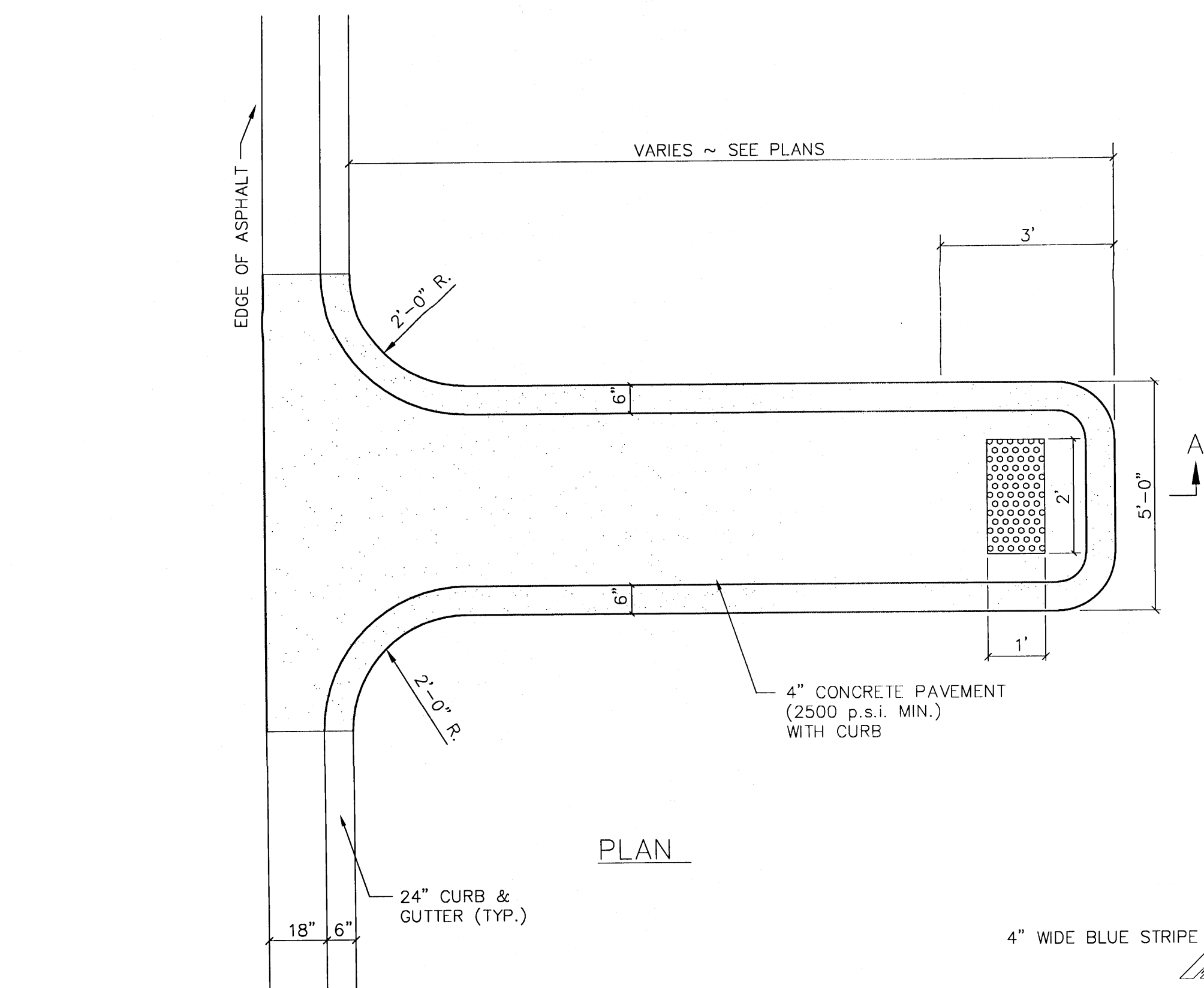


**5**  
**5** THICKENED EDGE CONCRETE SIDEWALK SECTION  
N.T.S.

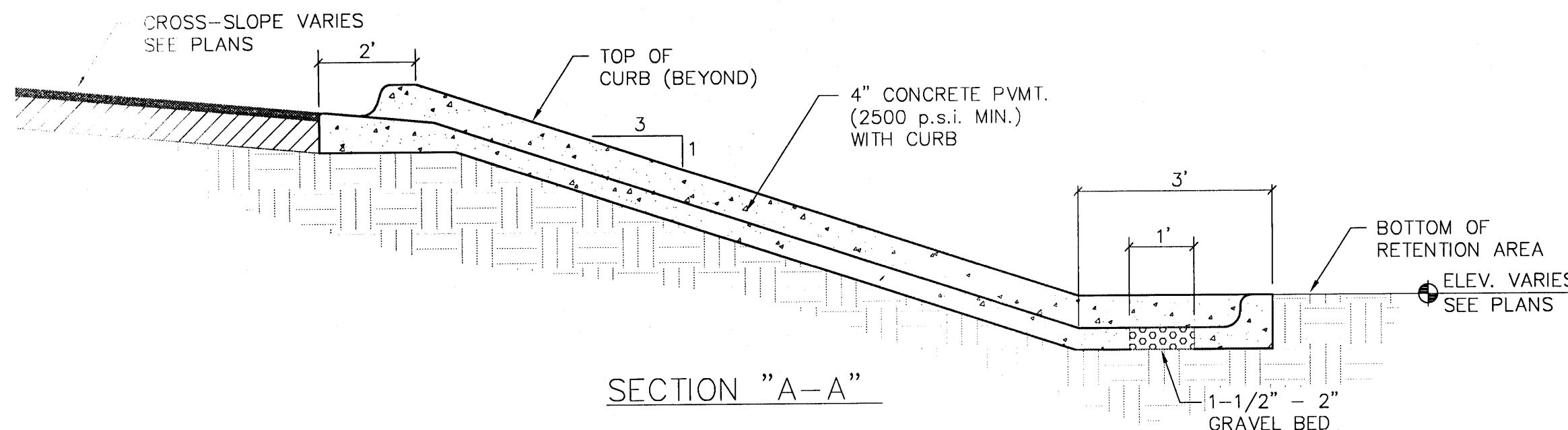


- 6" CONCRETE PAVEMENT (3,500 PSI) WITH FIBERMESH REINFORCEMENT @ 1-1/2 LBS. FIBER/C.Y.
- FILL AND TOP 12" OF SUBGRADE (COMPACTED TO 95% MAX. DENSITY AASHTO T 180)

**7**  
**5** CONCRETE PAVEMENT SECTION  
N.T.S.

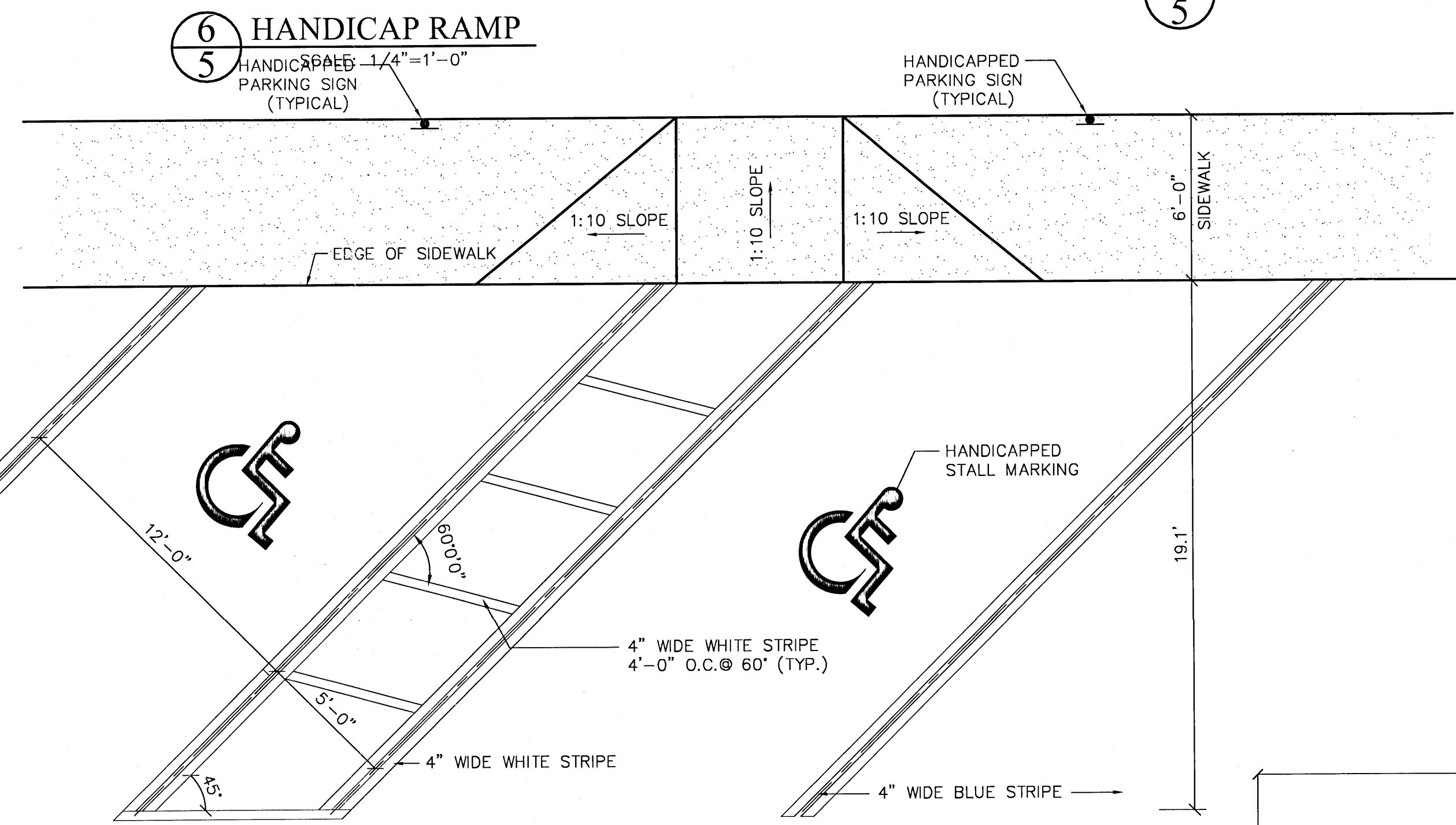


PLAN

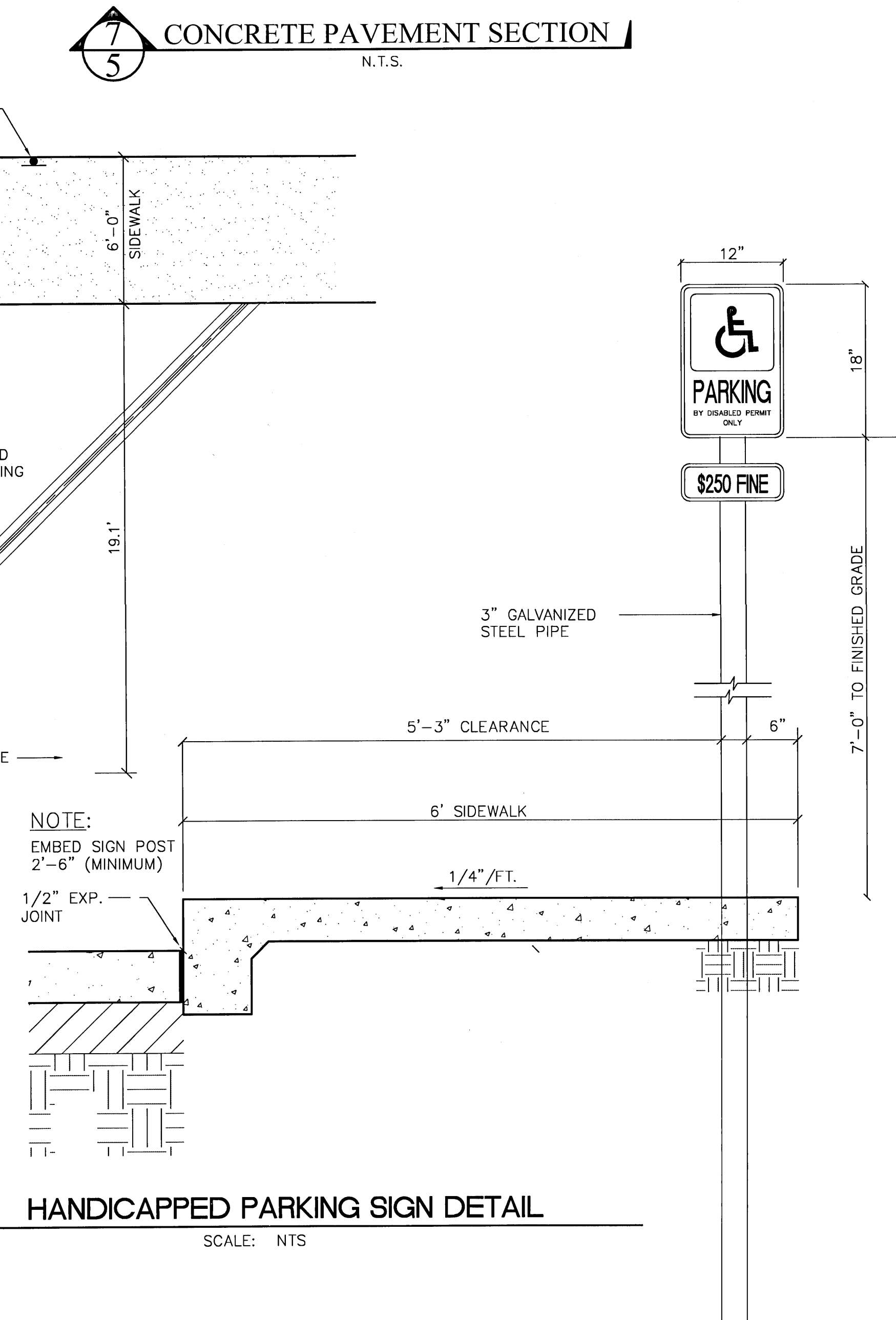


SECTION "A-A"

**2**  
**5** CONCRETE FLUME DETAIL  
SCALE: 1/2" = 1'-0"



**6**  
**5** HANDICAP RAMP  
SCALE: 1/4" = 1'-0"

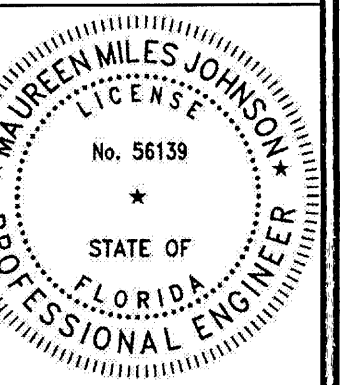


NOTE:  
EMBED SIGN POST 2'-6" (MINIMUM)  
1/2" EXP. JOINT

**9**  
**5** HANDICAPPED PARKING SIGN DETAIL  
SCALE: N.T.S.

*Maureen M. Johnson*  
05/05/16

**Maureen M. Johnson, PE, PA**  
4423 Windlake Drive ~ Niceville, FL 32578  
Phone 850.279.6670/850.699.0294  
FL PE #56139



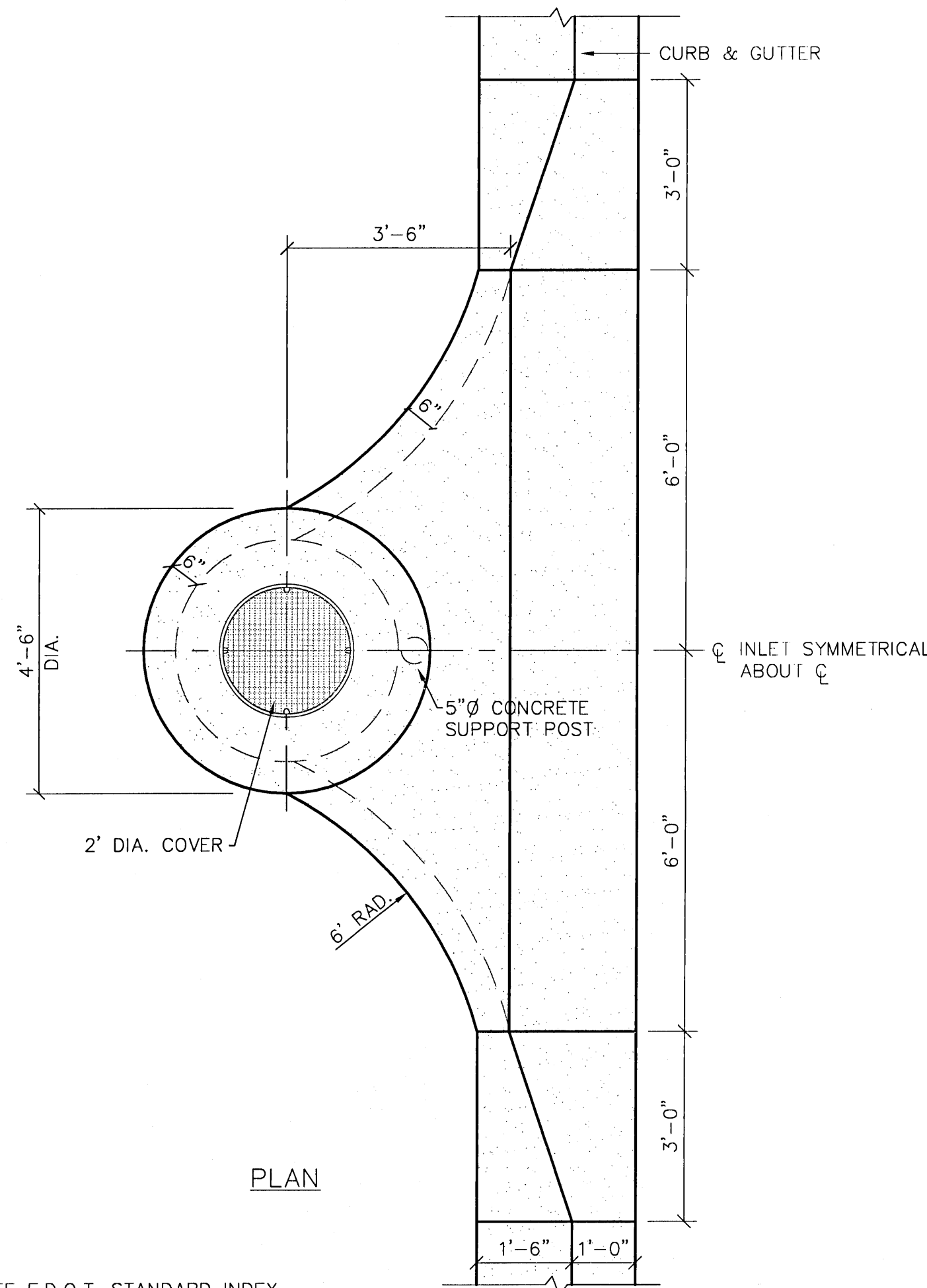
Arby's - Cantonment

Details

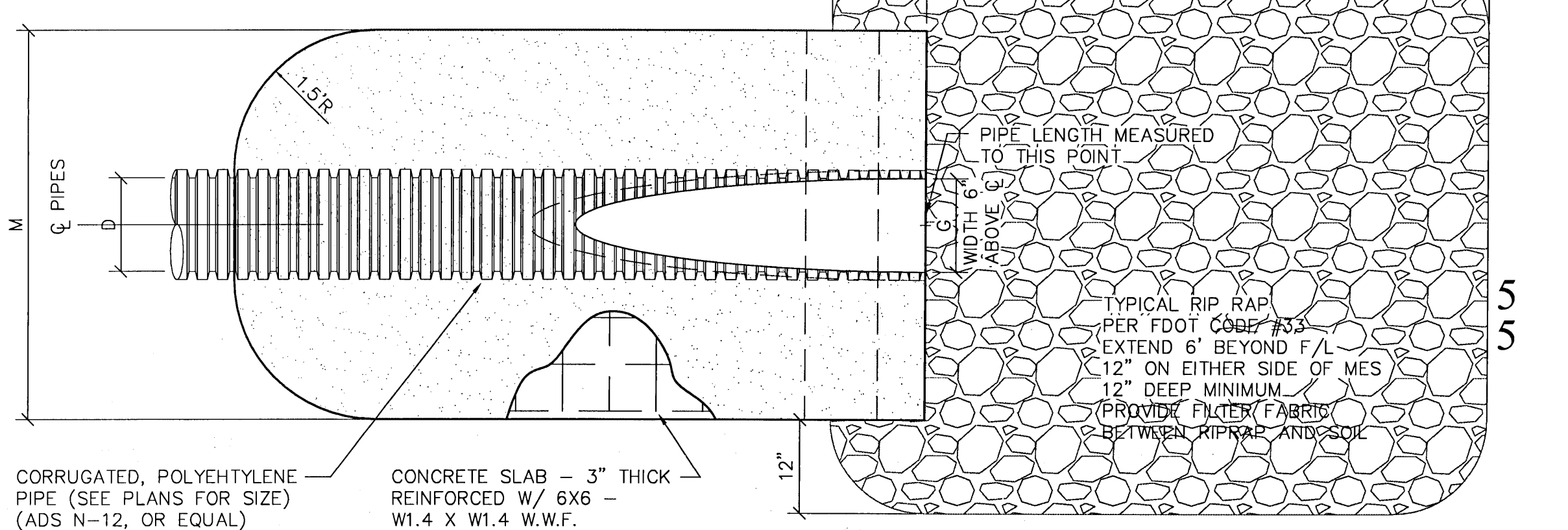
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FOR CONSTRUCTION

SCALE AS SHOWN  
DATE 05/05/2016  
SHEET 5 OF 10

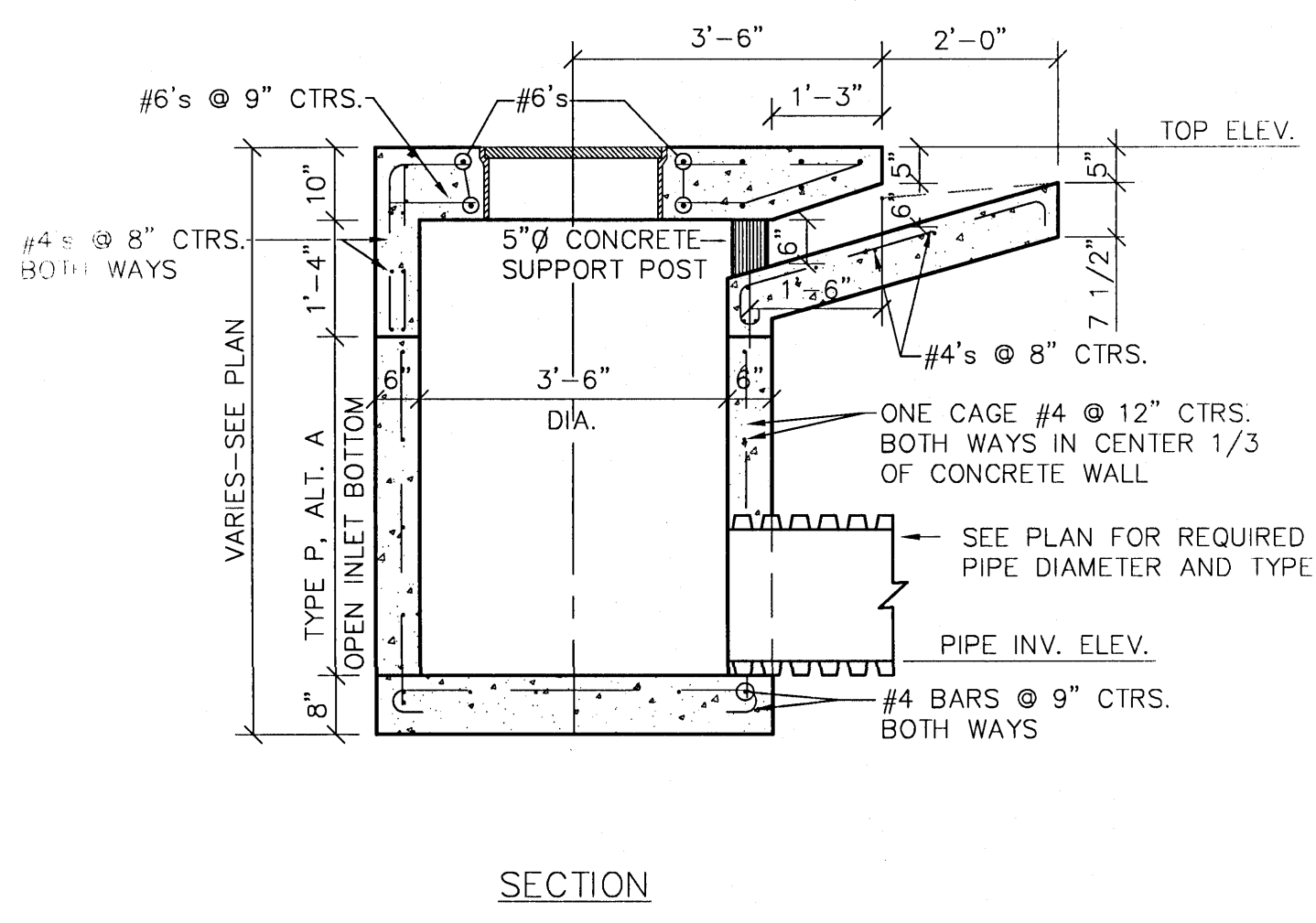
**5**



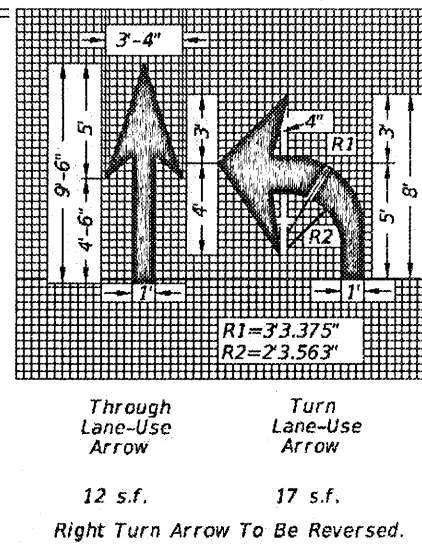
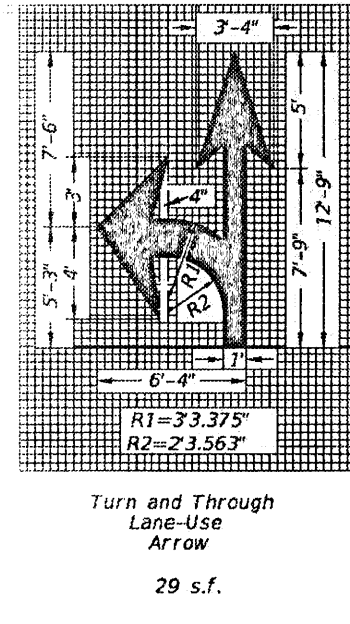
DIMENSIONS							
D	A	B	C	G	M	N	E
15"	2.27'	4.09'	6.36'	1.22'	4.63'	1.19'	4.03'
18"	2.36'	5.12'	7.48'	1.41'	4.92'	1.21'	5.03'
24"	2.53'	7.18'	9.71'	1.73'	5.50'	1.25'	7.03'



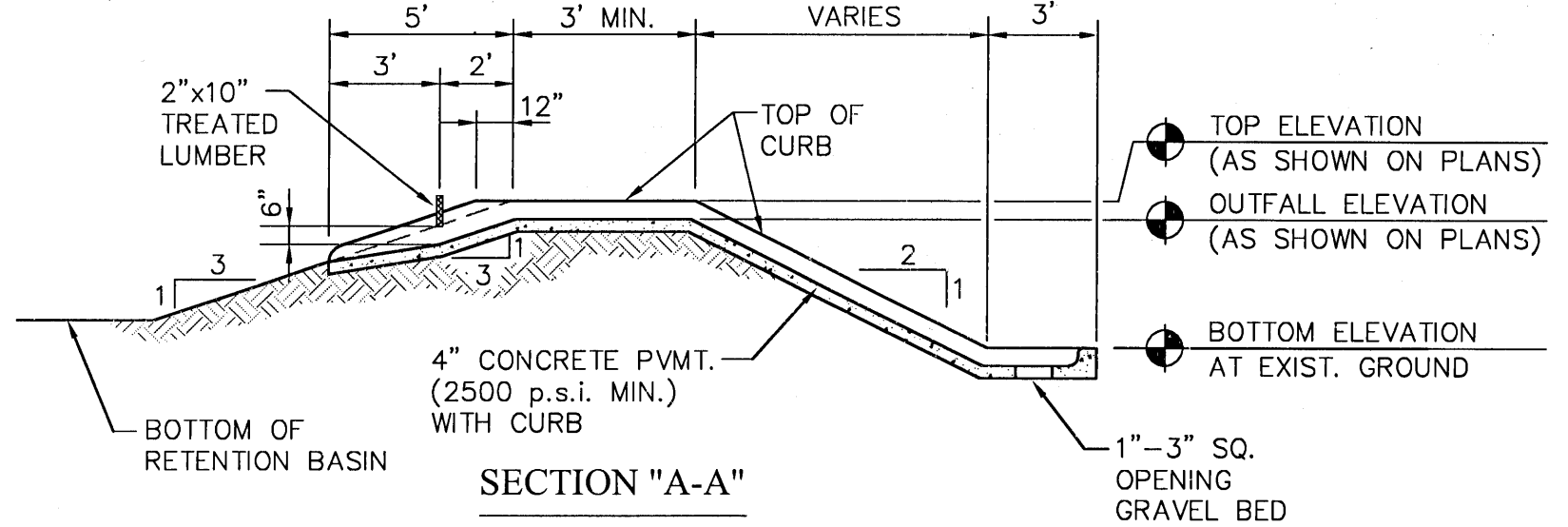
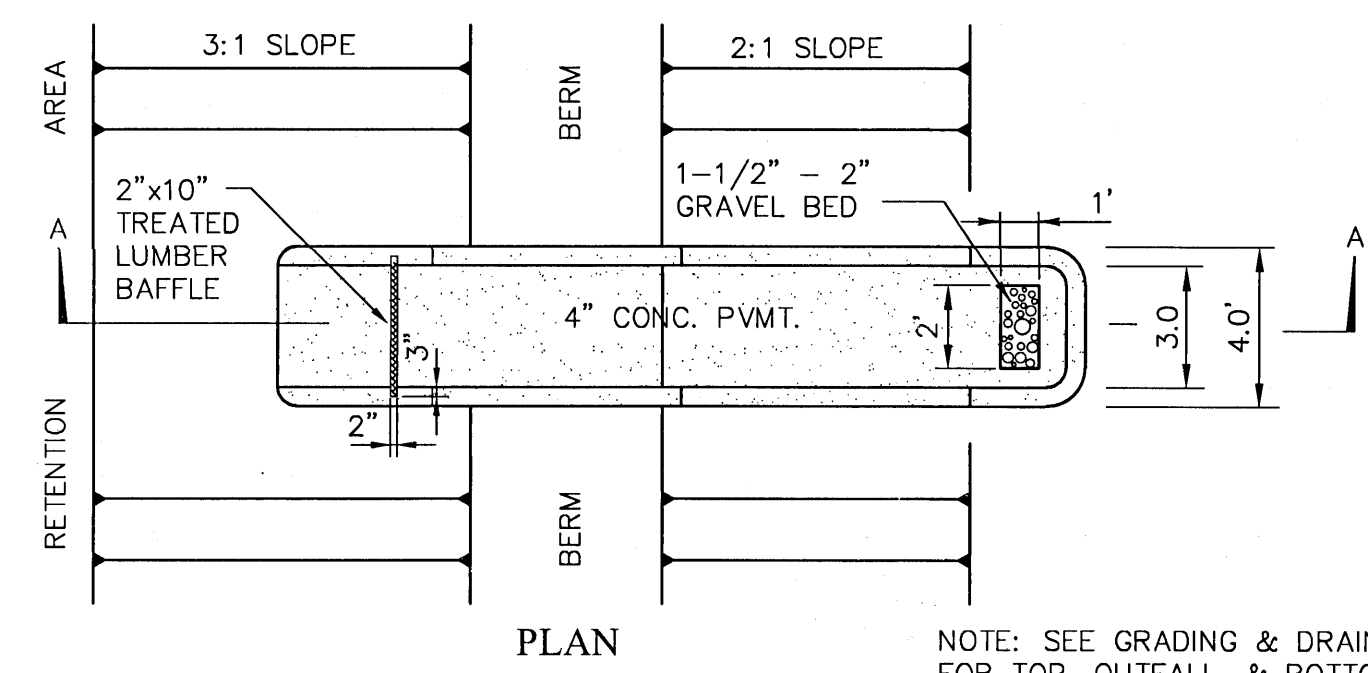
② MITERED END SECTION  
SCALE: 3/4" = 1'-0"



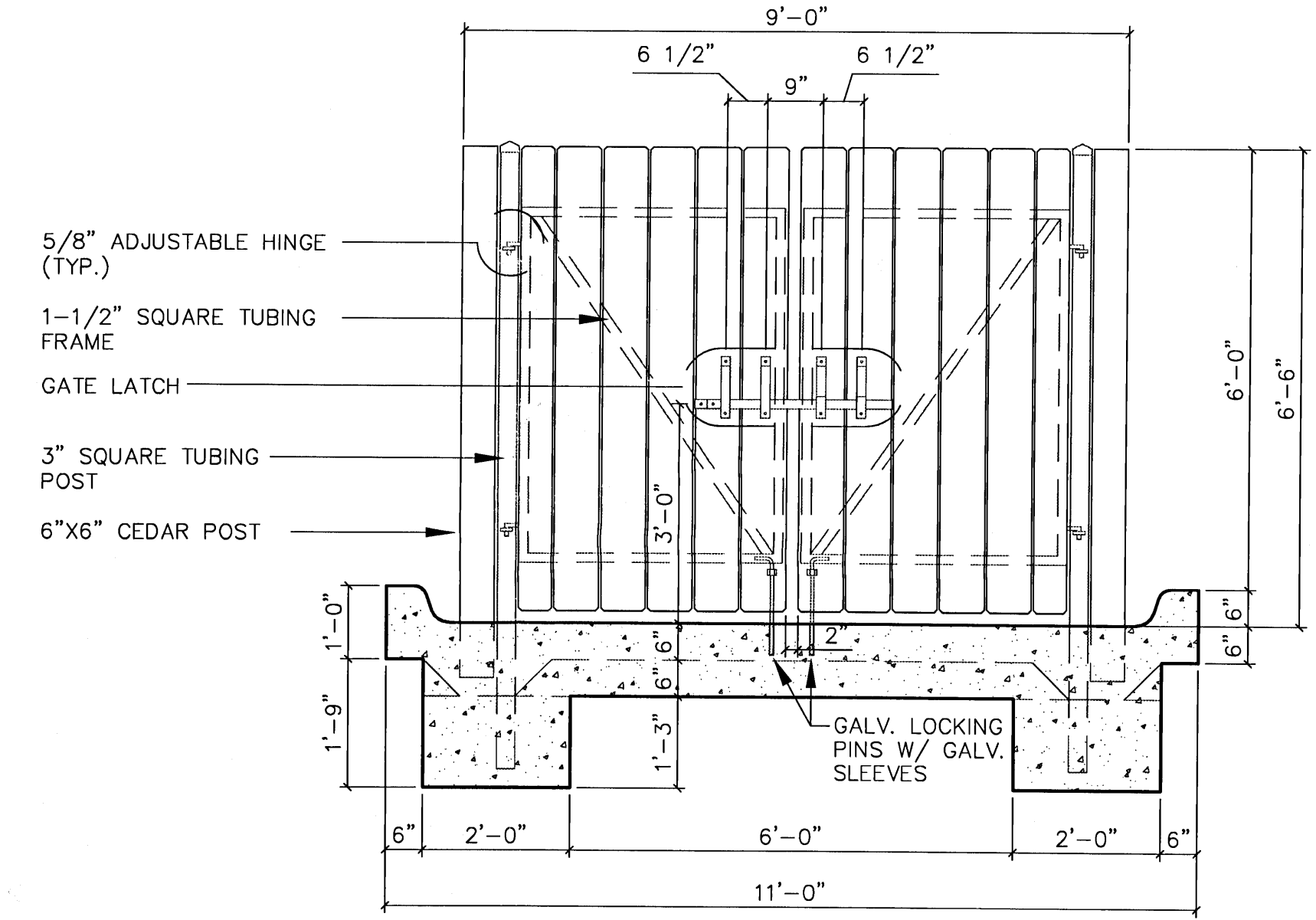
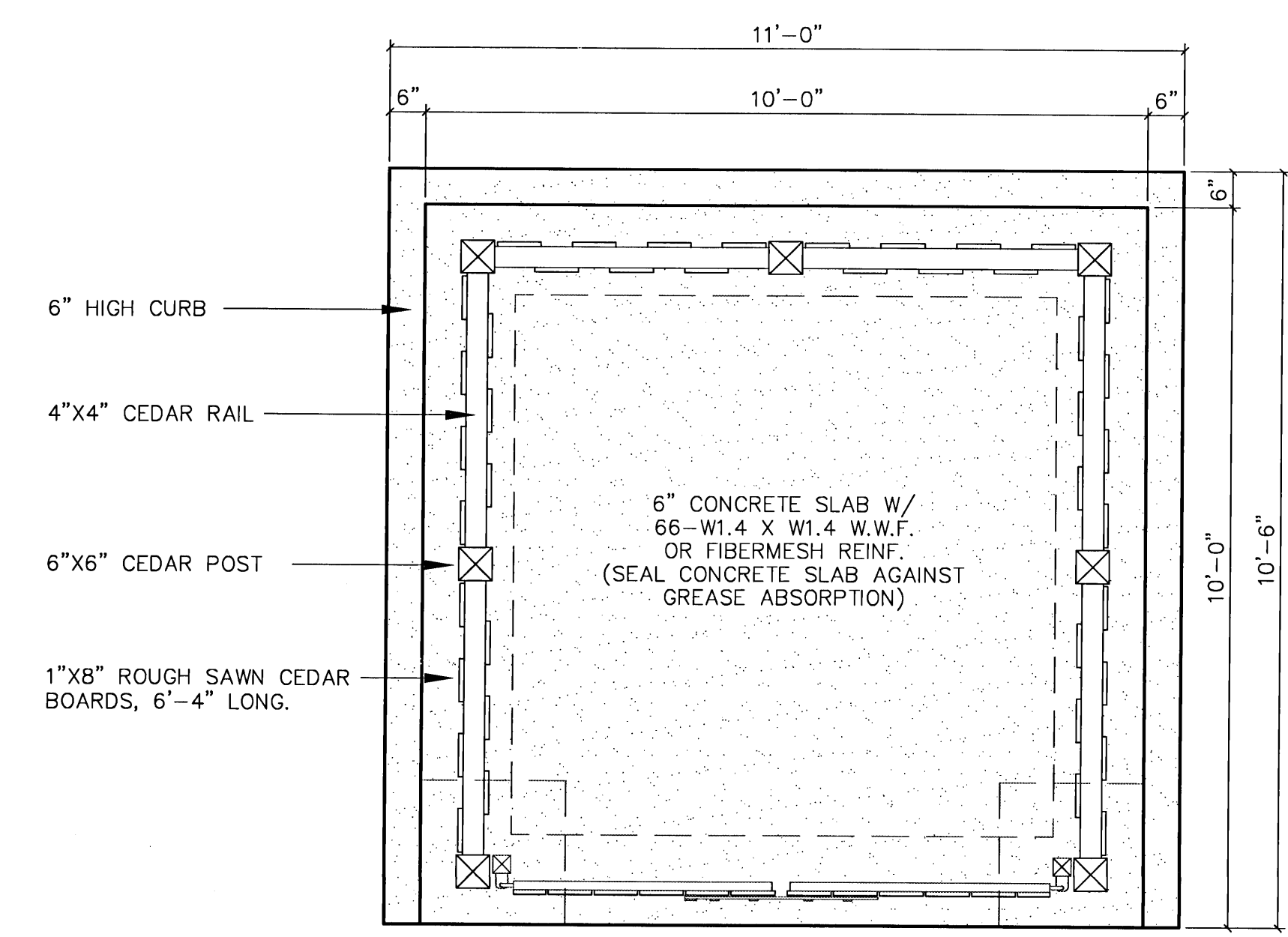
① TYPE 4 CURB INLET  
SCALE: 1/2" = 1'-0"



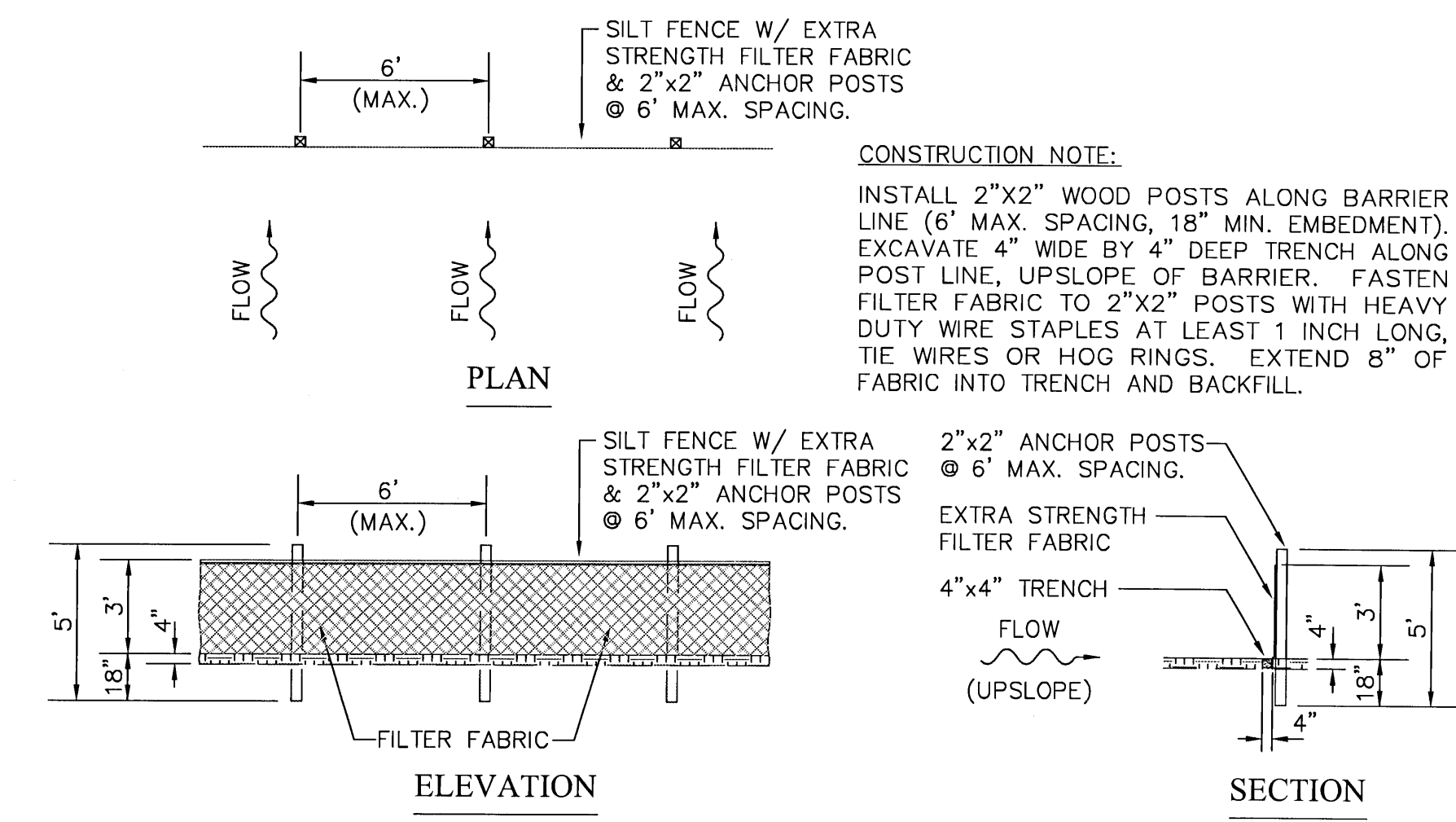
⑦ PAVEMENT MARKINGS  
SCALE: 1" = 20'-0"



③ OVERFLOW CONCRETE FLUME  
SCALE: 1" = 5'-0"



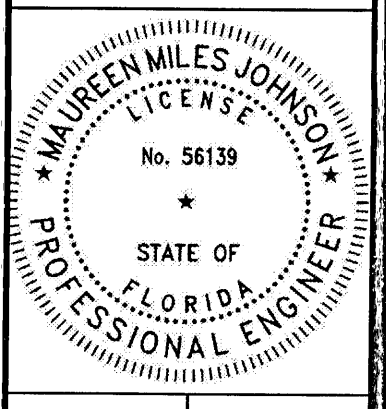
④ DUMPSTER PAD W/ ENCLOSURE  
SCALE: 1/2" = 1'-0"



⑤ SILT FENCE DETAIL  
SCALE: 1" = 5'-0"

*Maureen M. Johnson*  
05/05/16

**Maureen M. Johnson, PE, PA**  
4423 Windlake Drive ~ Niceville, FL 32578  
Ph 850.279.6670/850.699.0294  
FL PE #56139

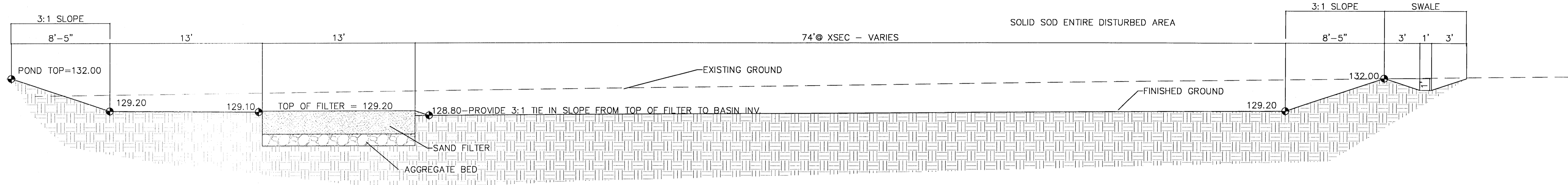


Arby's - Cantonment  
Details

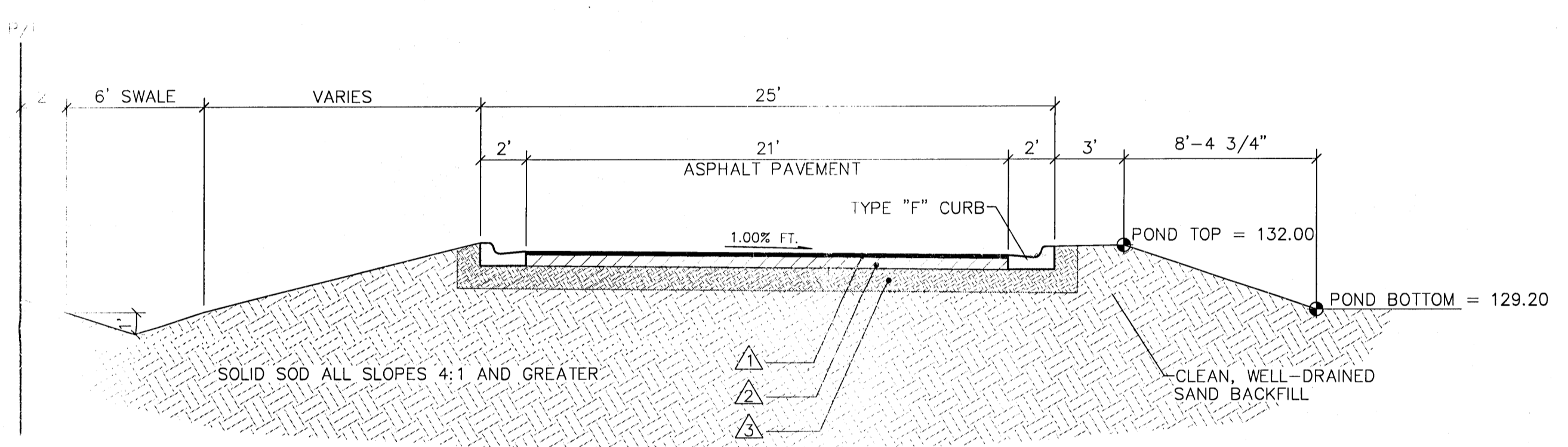
NOT RELEASED FOR CONSTRUCTION

SCALE AS SHOWN  
DATE 05/05/2016  
SHEET 6 OF 10

6

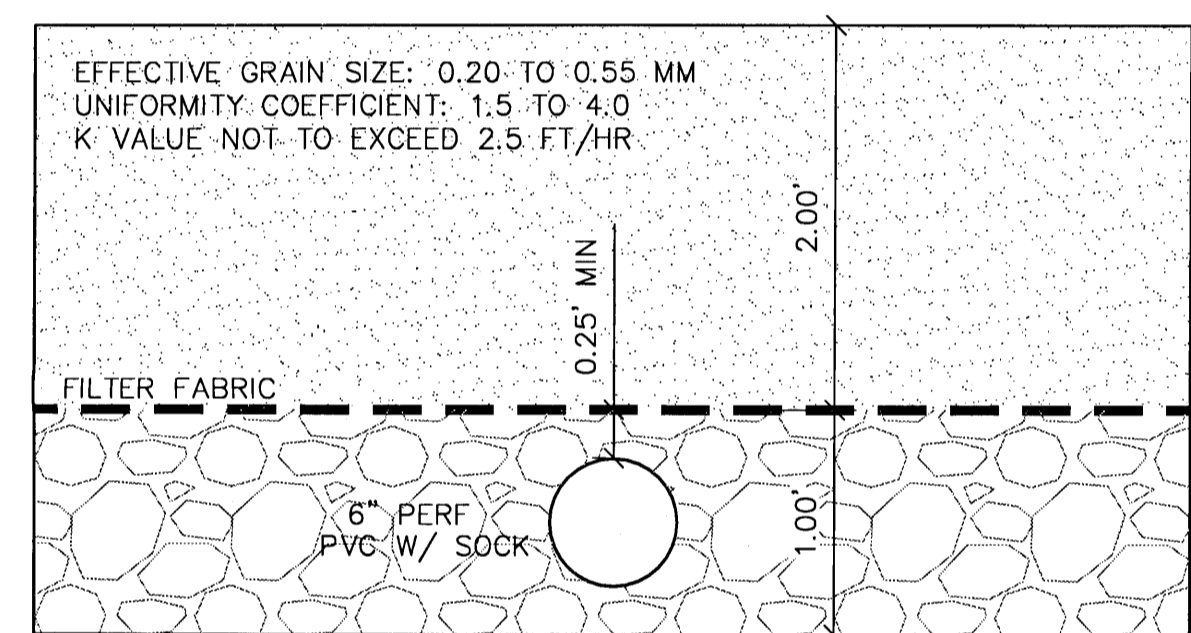


**1**  
**7** RETENTION SECTION  
SCALE: 1" = 5'-0"



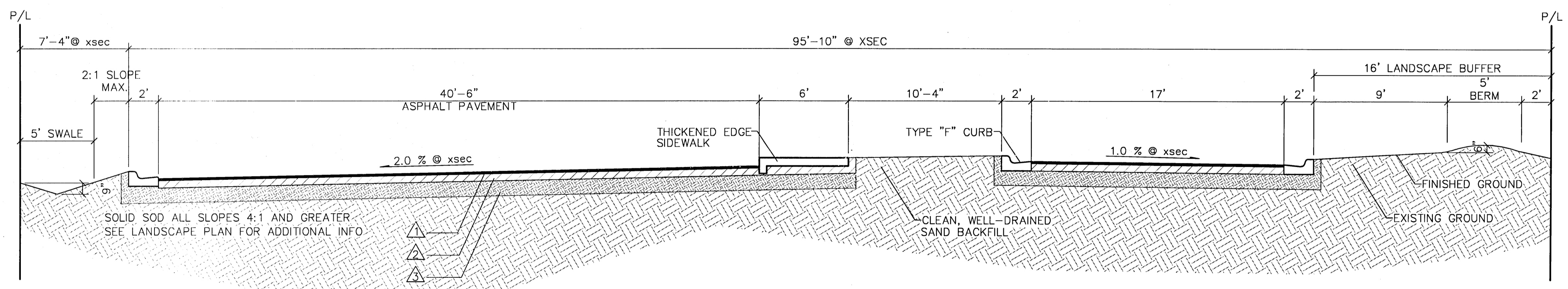
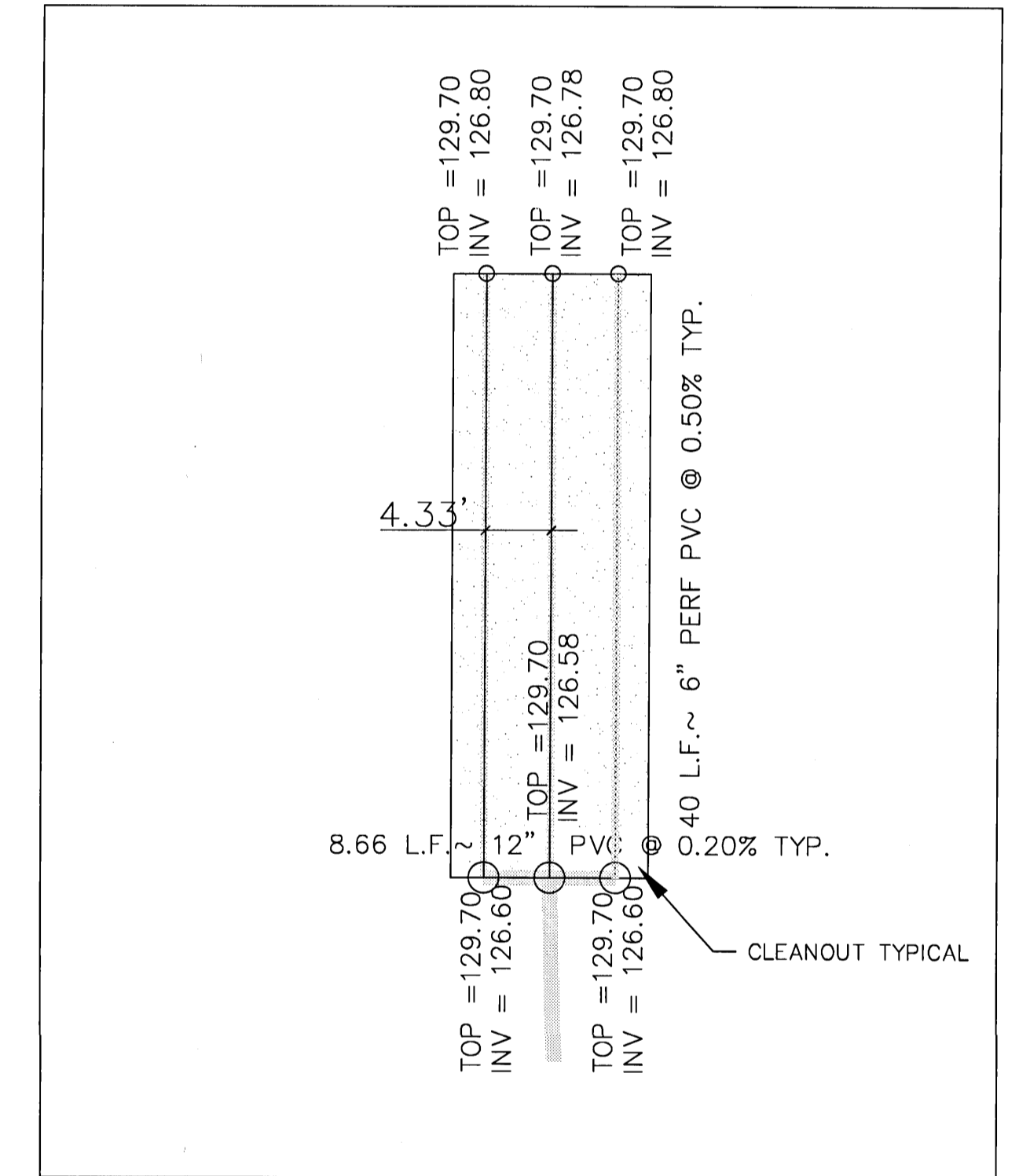
**2**  
**7** ENTRANCE FROM TATE RD SECTION  
SCALE: 1" = 5'-0"

PROVIDE CLEANOUT AT ALL JUNCTIONS  
6" NON PERF PVC PIPE W/ WATER TIGHT CAP  
WYE FITTINGS SHALL BE AT AN ANGLE NOT LESS THAN 45° FROM UPSTREAM  
UNDERDRAIN PIPES SHALL BE PERFORATED  
WITH 3" OF #57 STONE MINIMUM



1. THE CONTRACTOR SHALL INSTALL THE FILTER IMMEDIATELY AFTER THE POND HAS BEEN GRADED.
2. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HRS. PRIOR TO INSTALLATION.
3. A SILT FENCE SHALL BE INSTALLED AROUND THE PERIMETER OF THE FILTER.
4. THE POND SHALL BE SODDED SOLID IMMEDIATELY TO PREVENT SEDIMENT FROM ENTERING THE FILTER.
5. WRAP THE GRAVEL PACK WITH A PERMEABLE FILTER FABRIC. WRAP THE OUTSIDE PERIMETER OF SAND FILTER IN FILTER FABRIC.
6. AN ALUMINIUM GAUGE SHALL BE INSTALLED WITH INTERVALS TO THE 0.1 FT. AND ELEVATIONS CALLED OUT TO A 1.0 FT. TO FACILITATE A DRAWDOWN TEST.
7. SAND QUANTITY SHALL BE CALCULATED BASED ON A 6" OVERBURDEN TO ALLOW FOR THE CONTRACTOR TO SCRAPE THE FILTER ONCE THE ENTIRE SITE IS STABILIZED INCLUDING PAVEMENT INSTALLATION AND LANDSCAPING.

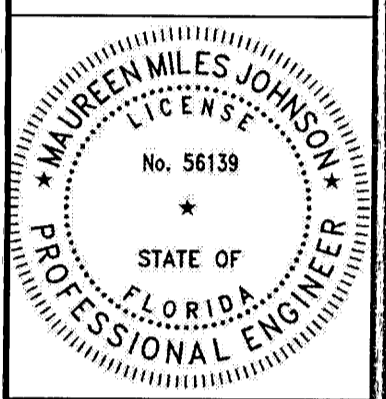
**3**  
**7** FILTER DETAIL  
SCALE: 1" = 20'-0"



**4**  
**7** PARKING SECTION  
SCALE: 1" = 5'-0"

*Maureen M. Johnson*  
05/05/16

**Maureen M. Johnson, PE, PA**  
4423 Windlake Drive ~ Niceville, FL 32578  
Ph 850.279.6670/850.699.0294  
FL PE #56139

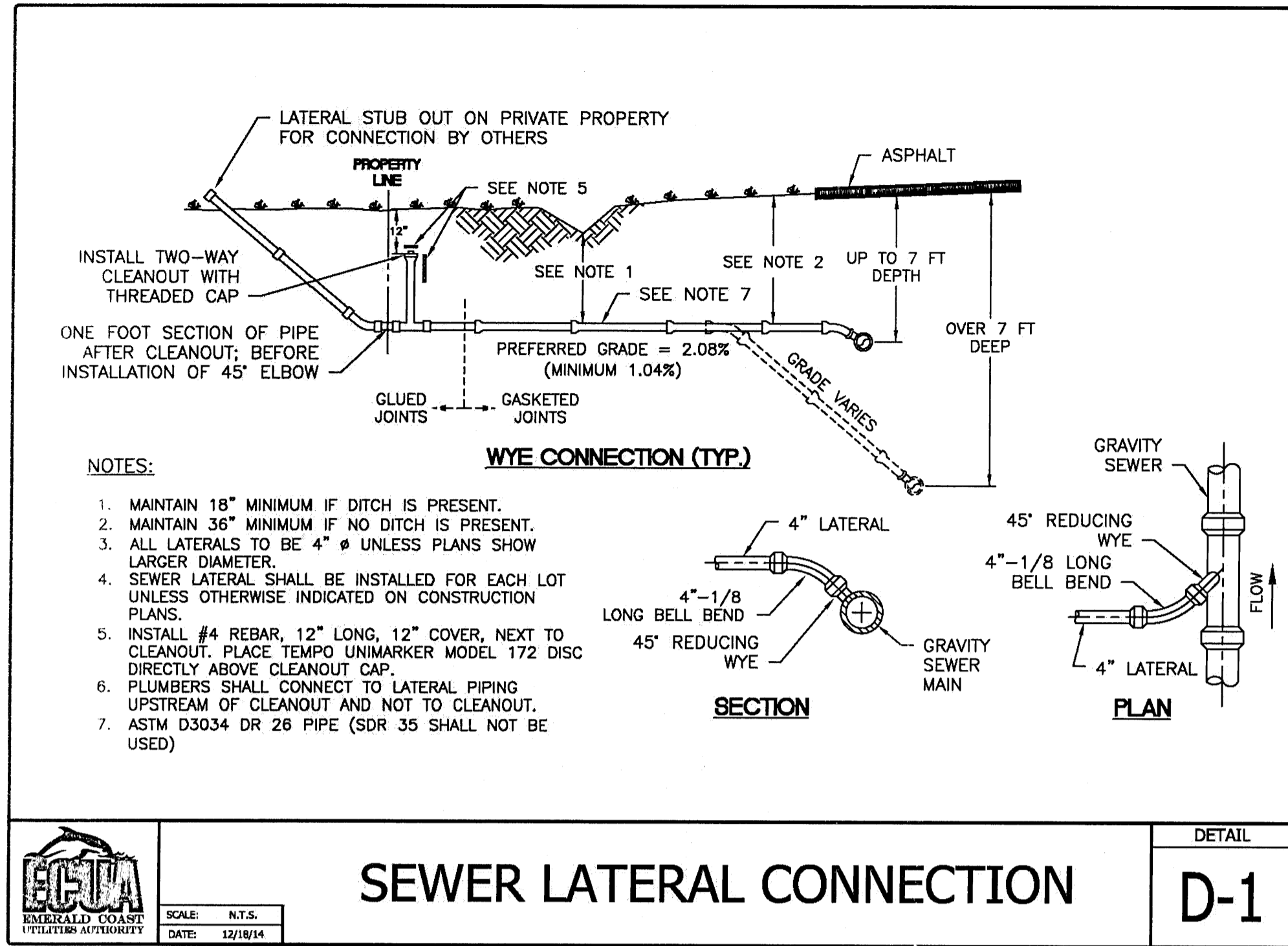


Arby's - Cantonment  
Details

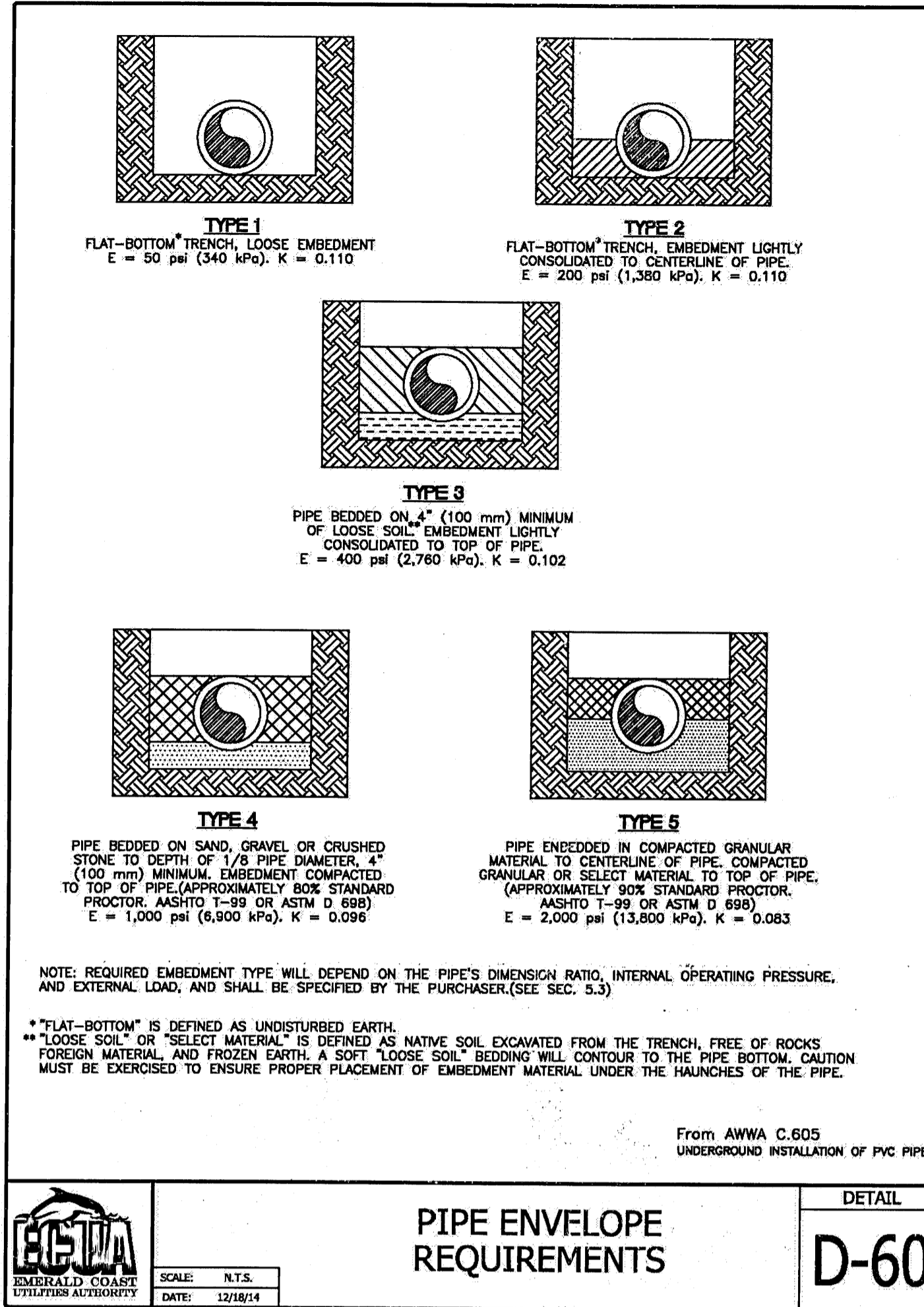
NOT RELEASED  
FOR CONSTRUCTION

SCALE  
AS SHOWN  
DATE  
05/05/2016  
SHEET  
7 OF 10

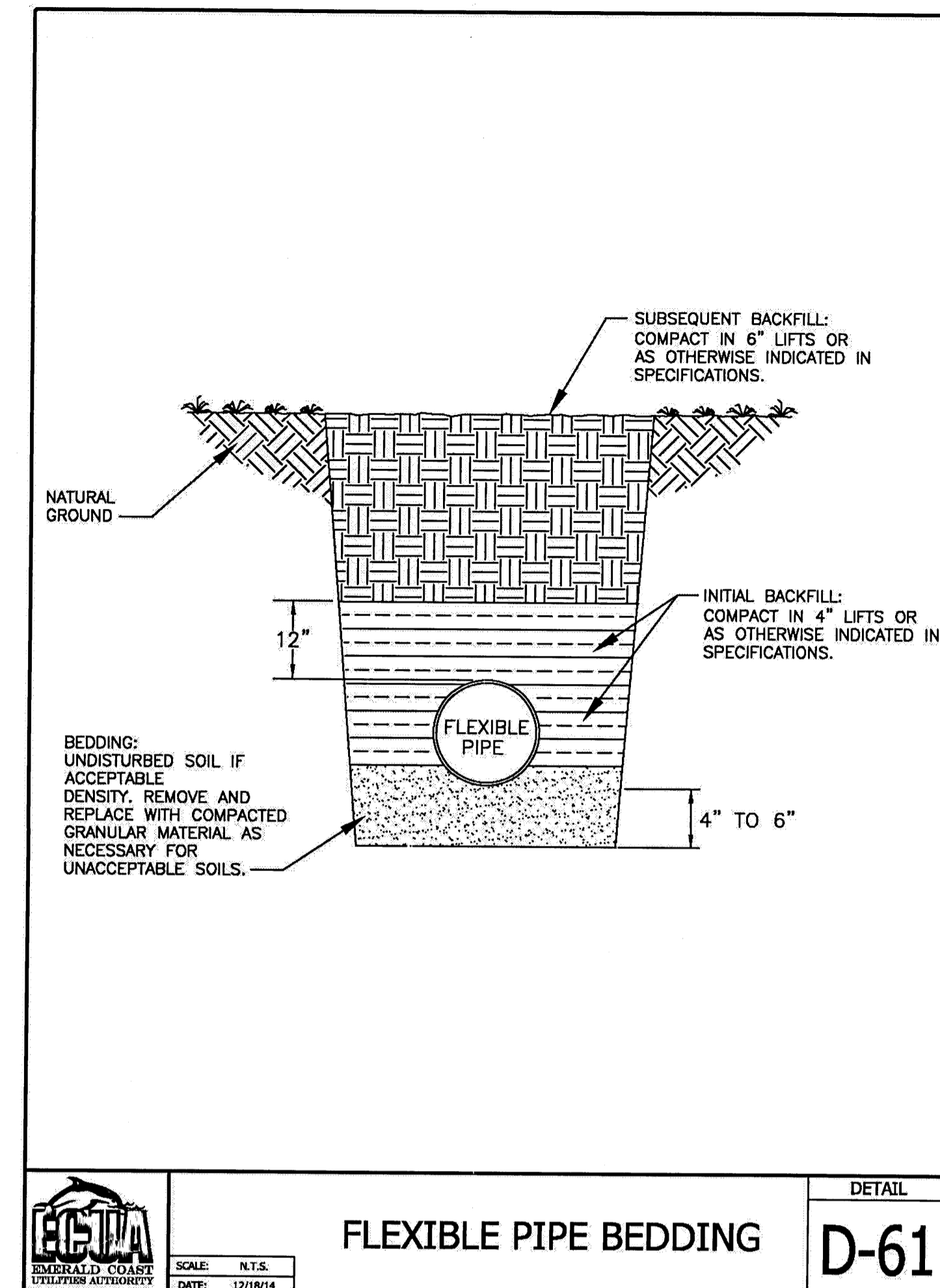
**7**



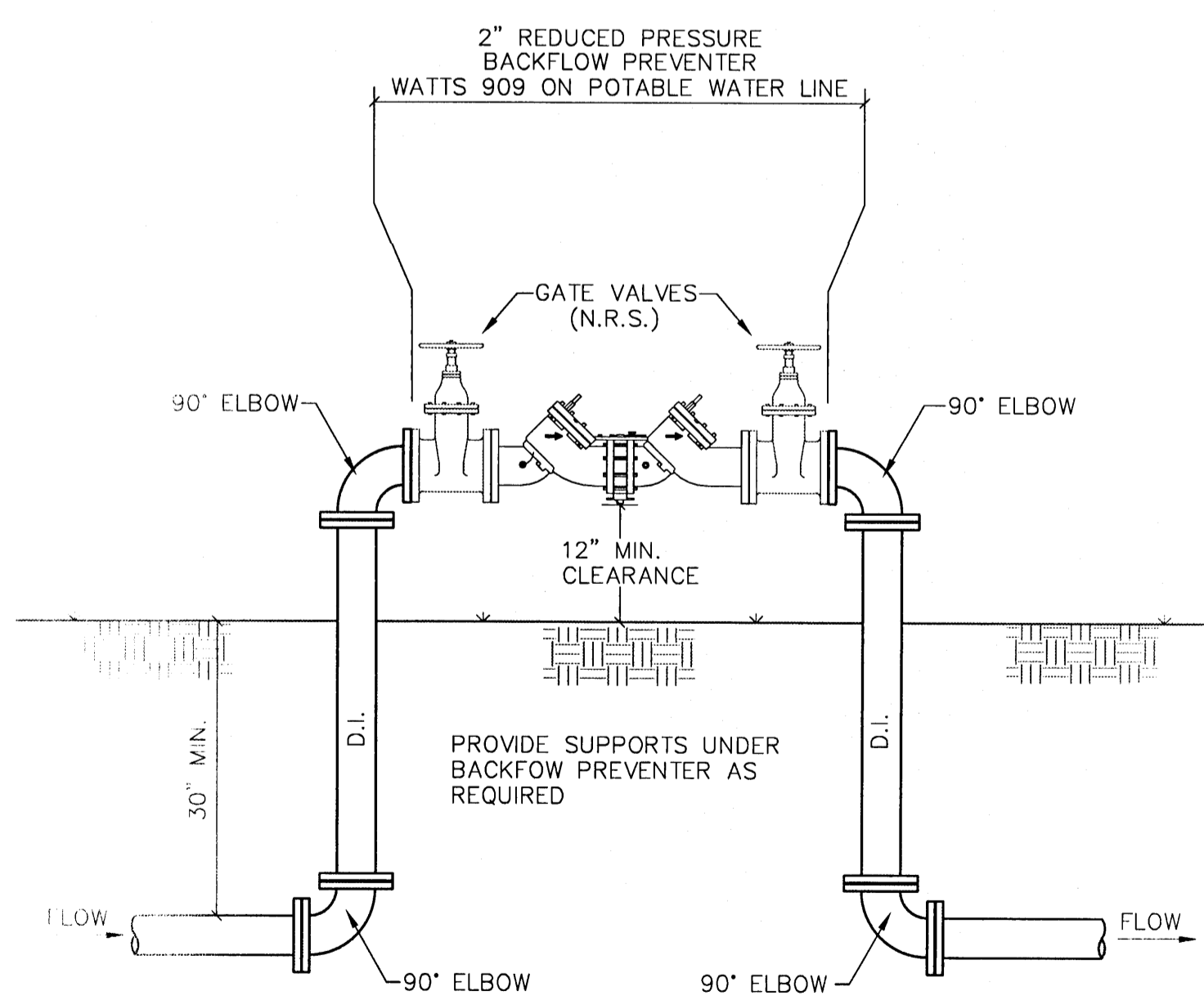
1/8 SEWER SERVICE NTS



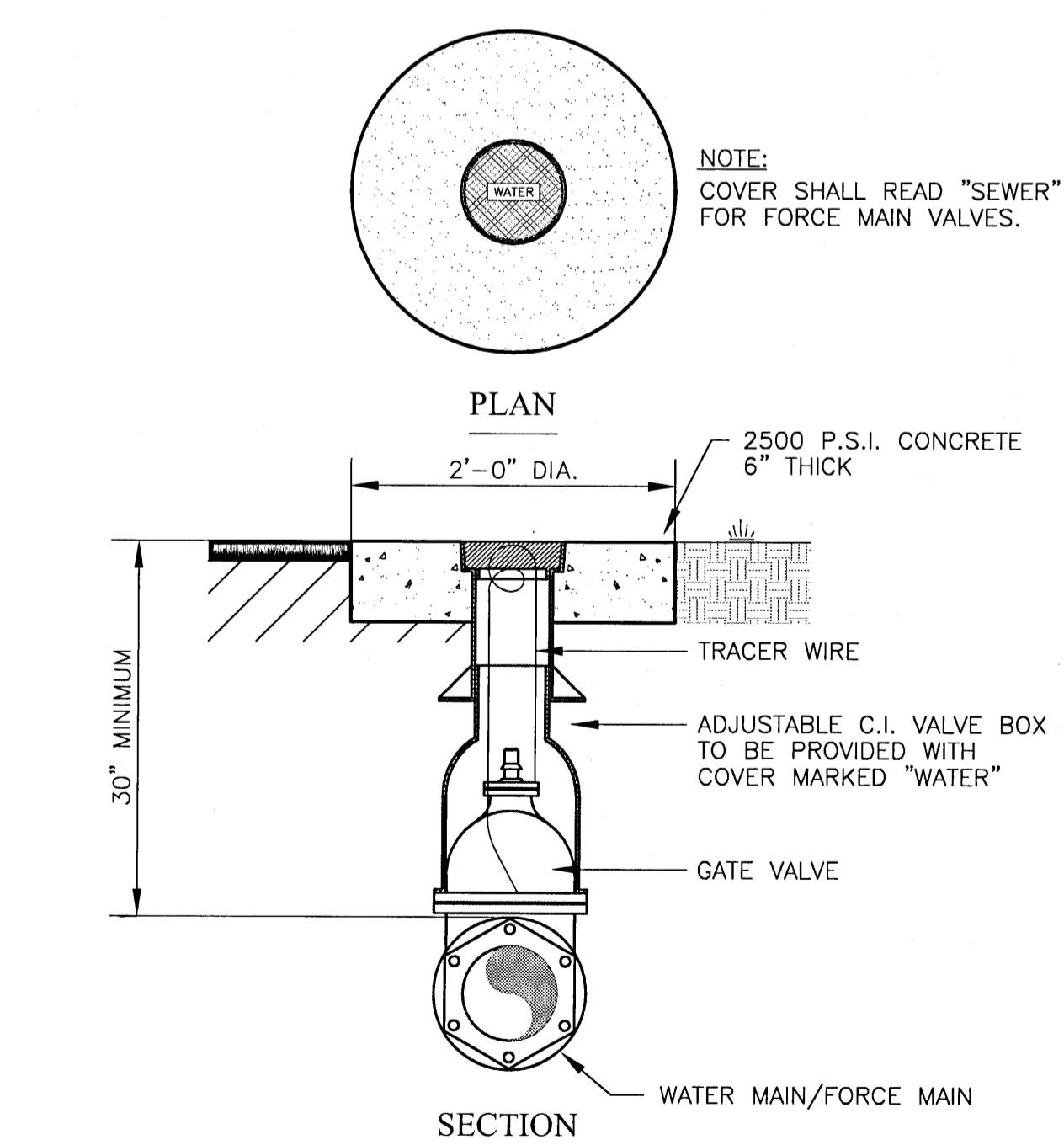
2/8 PIPE ENVELOPE NTS



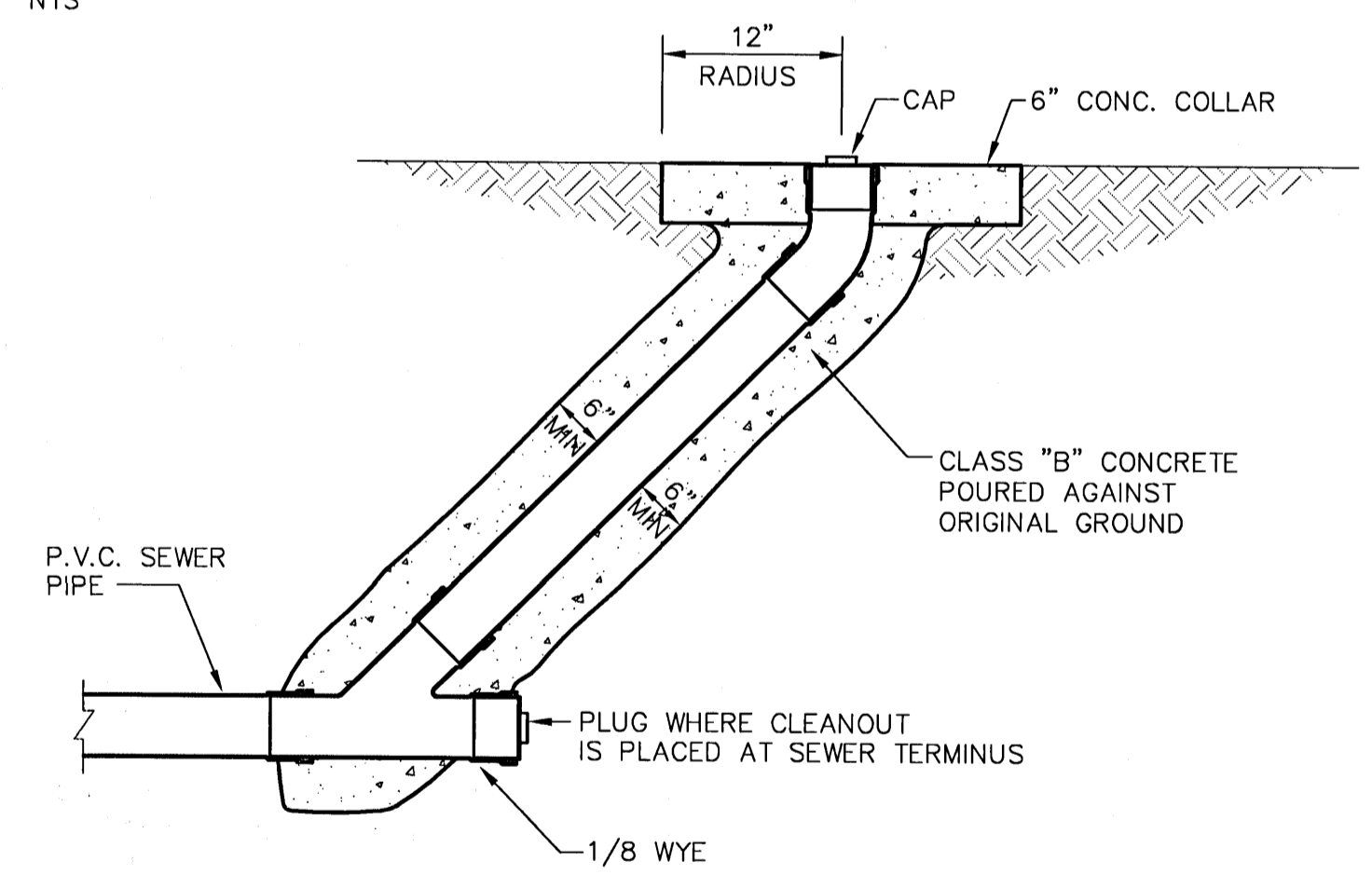
3/8 PIPE BEDDING NTS



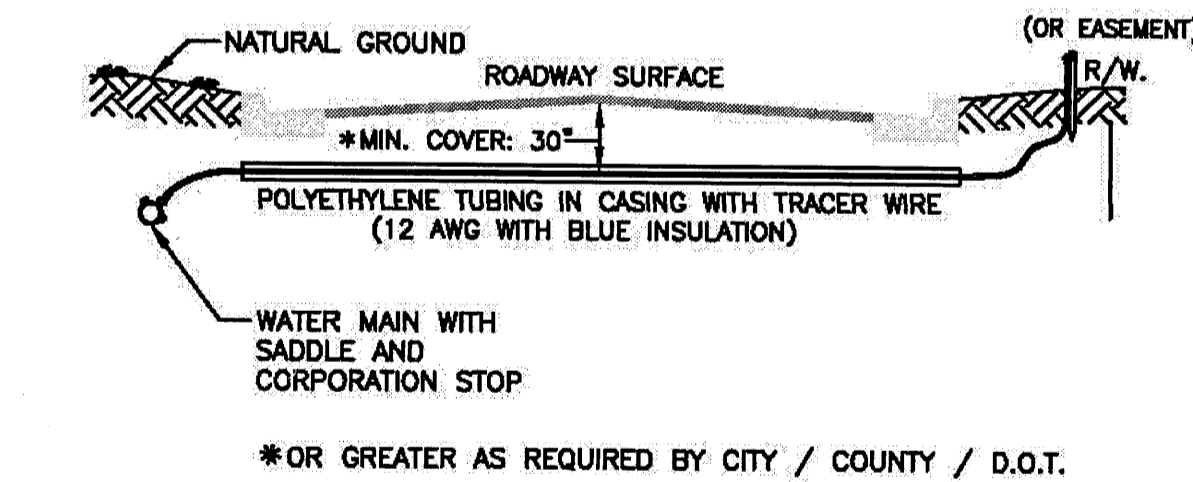
4/8 BACKFLOW PREVENTER DETAIL



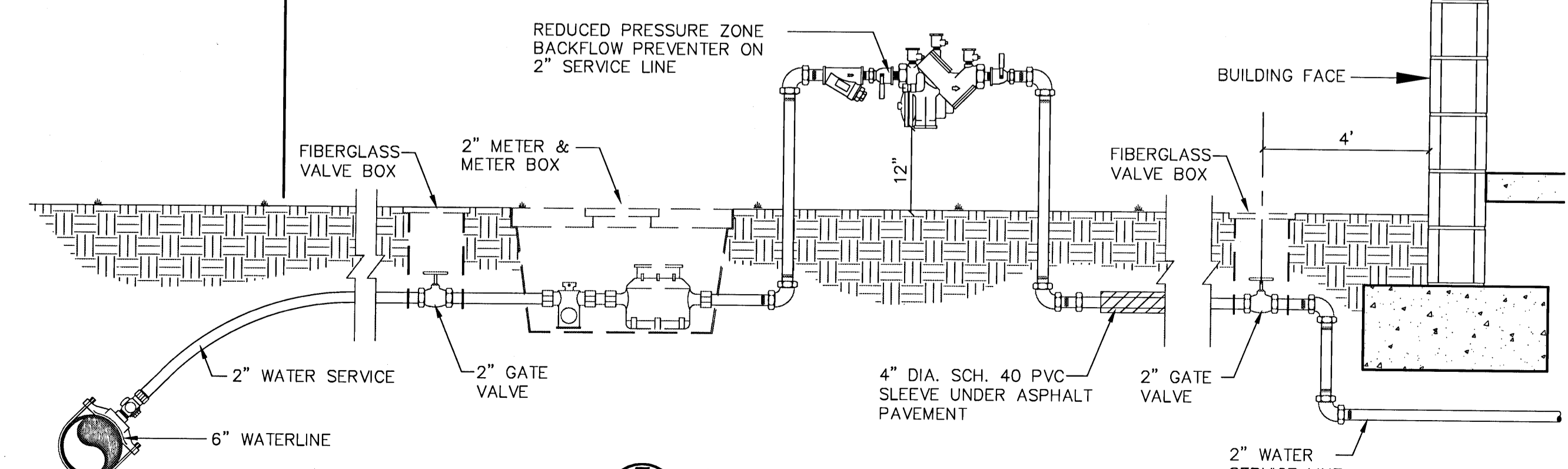
5/8 GATE VALVE AND VALVE BOX DETAIL



6/8 CLEANOUT DETAIL



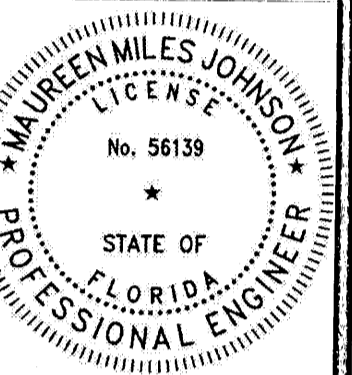
7/8 TYPICAL LONG SERVICE - STREET CROSSING



7/8 WATER SERVICE

Maureen M. Johnson  
05/05/14

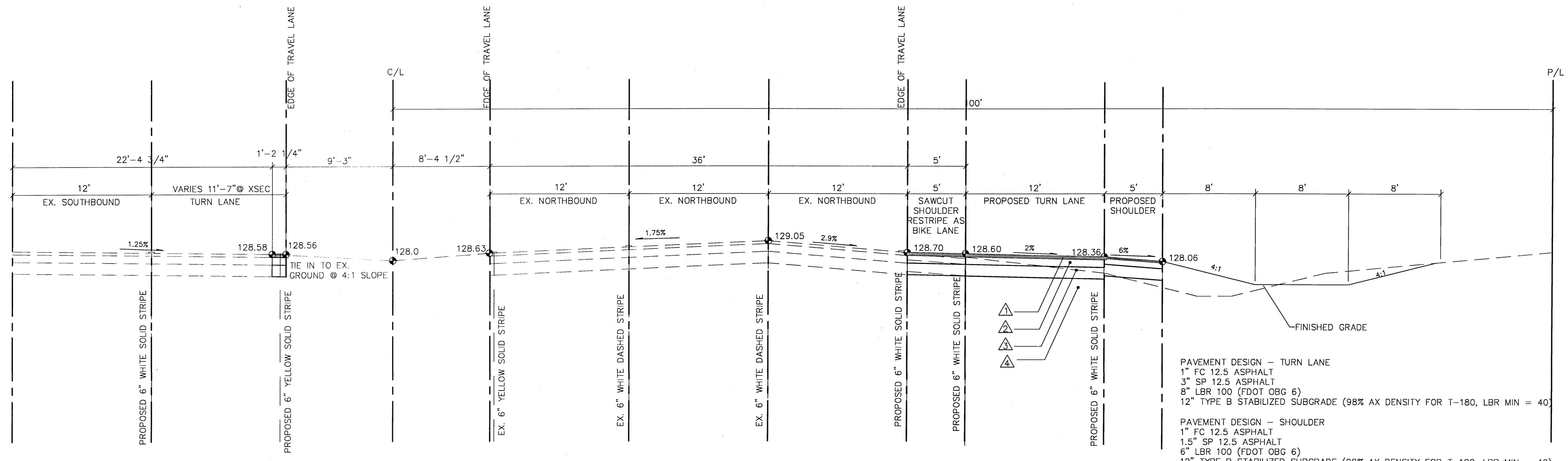
Maureen M. Johnson, PE, PA  
4423 Windlake Drive ~ Niceville, FL 32578  
Phone 850.279.6670 / 850.699.0294  
FL PE #56139



Arby's - Cantonment  
Utility Details

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DATE 05/05/2016  
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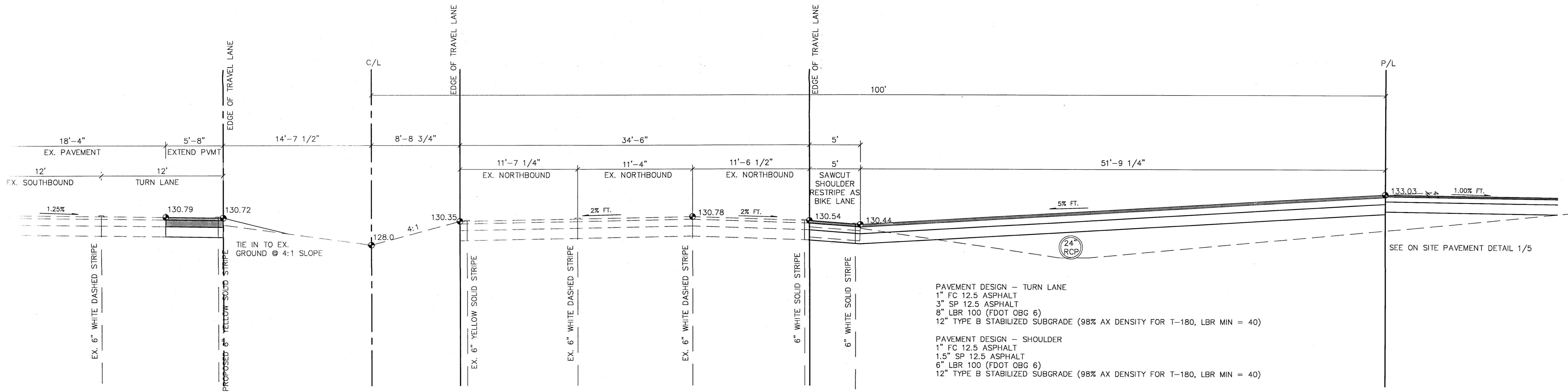
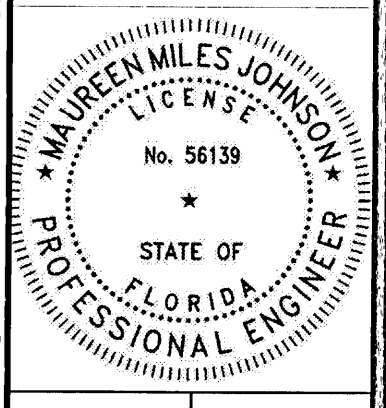
**1**  
9 US 29 CROSS SECTION MP 12.262  
SCALE: 1" = 5'-0"

PAVEMENT DESIGN - TURN LANE  
 1" FC 12.5 ASPHALT  
 3" SP 12.5 ASPHALT  
 8" LBR 100 (FDOT OBG 6)  
 12" TYPE B STABILIZED SUBGRADE (98% AX DENSITY FOR T-180, LBR MIN = 40)

PAVEMENT DESIGN - SHOULDER  
 1" FC 12.5 ASPHALT  
 1.5" SP 12.5 ASPHALT  
 6" LBR 100 (FDOT OBG 6)  
 12" TYPE B STABILIZED SUBGRADE (98% AX DENSITY FOR T-180, LBR MIN = 40)

*Maureen M. Johnson*  
 05/05/16

**Maureen M. Johnson, PE, PA**  
 4423 Windlake Drive ~ Niceville, FL 32578  
 Ph 850.279.6670/850.699.0294  
 FL PE #56139



**2**  
9 US 29 CONNECTION PROFILE MP 12.281  
SCALE: 1" = 5'-0"

PAVEMENT DESIGN - TURN LANE  
 1" FC 12.5 ASPHALT  
 3" SP 12.5 ASPHALT  
 8" LBR 100 (FDOT OBG 6)  
 12" TYPE B STABILIZED SUBGRADE (98% AX DENSITY FOR T-180, LBR MIN = 40)

PAVEMENT DESIGN - SHOULDER  
 1" FC 12.5 ASPHALT  
 1.5" SP 12.5 ASPHALT  
 6" LBR 100 (FDOT OBG 6)  
 12" TYPE B STABILIZED SUBGRADE (98% AX DENSITY FOR T-180, LBR MIN = 40)

SEE ON SITE PAVEMENT DETAIL 1/5

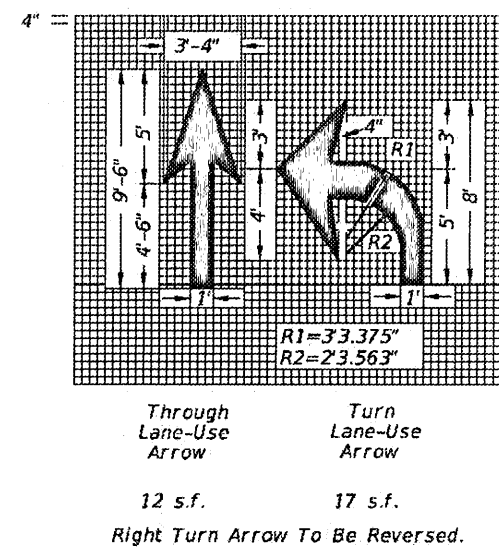
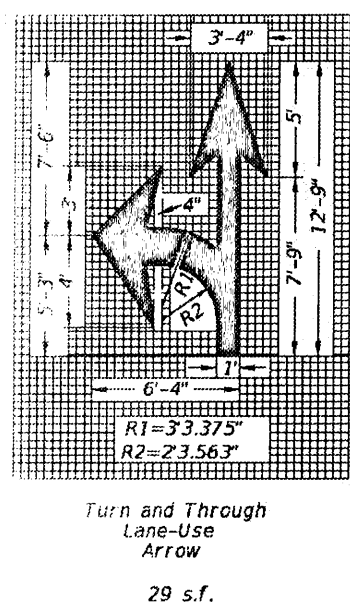
Arby's - Cantonment

FDOT Details

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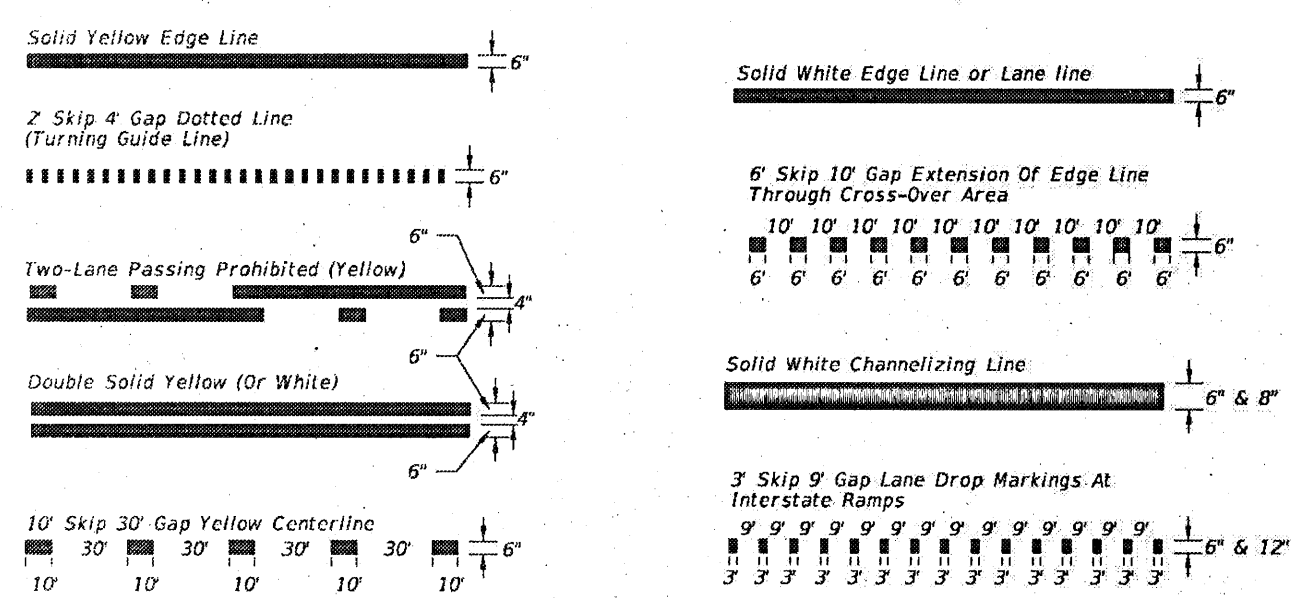
SCALE  
 AS SHOWN  
 DATE  
 05/05/2016  
 SHEET  
 9 OF 10

9

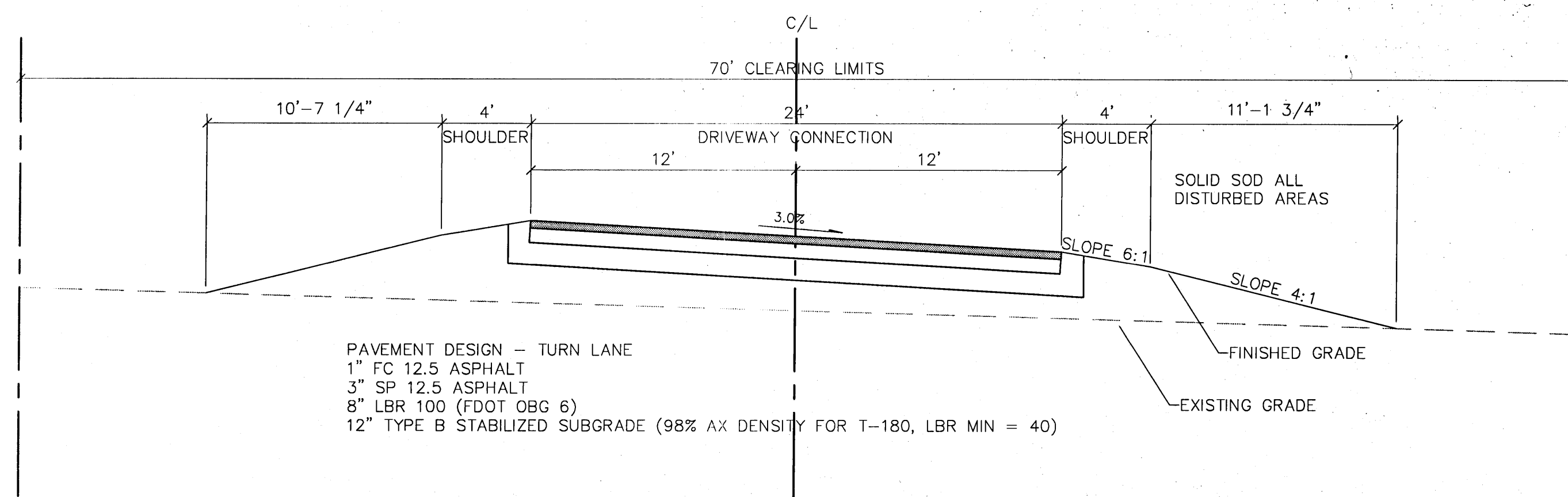


**BASIC COLOR RULE:**

White lines separate traffic in the same direction. Yellow lines separate traffic in opposing directions. Yellow dotted lines may be used in special cases. Black may be used in combination with white for skip lines where a light-colored pavement does not provide sufficient contrast with the markings.



**10 PAVEMENT MARKINGS**  
SCALE: 1" = 20'-0"



**2 FDOT CONNECTION**  
SCALE: 1" = 5'-0"

**FLAGGER CONTROL**

Where flaggers are used, a FLAGGER symbol or legend sign must replace the WORKERS symbol or legend sign.

The flagger must be clearly visible to approaching traffic for a distance sufficient to permit proper response by the motorist to the flagging instructions, and to direct traffic to reduce speed or to stop as required before entering the work zone. Flaggers shall be positioned to maintain maximum color contrast between the flagger's high-visibility safety apparel and equipment and the work area background.

**Hand-Signaling Devices**

STOP/SLOW paddles are the primary hand-signaling device. The STOP/SLOW paddle shall have an octagonal shape on a rigid handle. If the STOP/SLOW paddle is placed on a rigid staff, the minimum length of the staff, measured from the bottom of the paddle to the end of the staff that rests on the ground, must not be less than 6 ft. STOP/SLOW paddles shall be at least 24 inches wide with letters at least 6 inches high and shall be fabricated from light semirigid material. The background of the STOP face shall be red with white letters and border. The background of the SLOW face shall be orange with black letters and border. When used at night-time, the STOP/SLOW paddle shall be retroreflectORIZED.

Flag use is limited to immediate emergencies, intersections, and when working on the centerline or shared left turn lanes where two (2) flaggers are required and there is opposing traffic in the adjacent lanes. Flags, when used, shall be a minimum of 24 inches square, made of a good grade of red material, and securely fastened to a staff that is approximately 36 inches in length. When used at night-time, flags shall be retroreflectORIZED red.

Flashlight, lantern or other lighted signal that will display a red warning light shall be used at night.

**Flagger Stations**

Flagger stations shall be located far enough in advance of the work space so that approaching road users will have sufficient distance to stop before entering the work space. When used at night-time, the flagger station shall be illuminated.

**SURVEY WORK ZONES**

The SURVEY CREW AHEAD symbol or legend sign shall be the principal Advance Warning Sign used for Traffic Control Through Survey Work Zones and may replace the ROAD WORK AHEAD sign when lane closures occur, at the discretion of the Party Chief.

When Traffic Control Through Work Zones is being used for survey purposes only, the END ROAD WORK sign as called for on certain 600 Series indexes should be omitted.

**Survey Between Active Traffic Lanes or Shared Left Turn Lanes**

The following provisions apply to Main Roadway Traffic Control Work Zones. These provisions must be adjusted by the Party Chief to fit roadway and traffic conditions when the Survey Work Zone includes intersections.

- (A) A STAY IN YOUR LANE (MOT-1-06) sign shall be added to the Advance Warning Sign sequence as the second most immediate sign from the work area.
- (B) Elevation Surveys-Cones may be used at the discretion of the Party Chief to protect prism holder and flaggers) cones, if used, may be placed at up to 50' intervals along the break line throughout the work zone.
- (C) Horizontal Control-With traffic flow in the same direction, cones shall be used to protect the backsight tripod and/or instrument. Cones shall be placed at the equipment, and up to 50' intervals for at least 200' towards the flow of traffic.
- (D) Horizontal Control-With traffic flow in opposite directions, cones shall be used to protect the backsight tripod and/or instrument. Cones shall be placed at the equipment, and up to 50' intervals for at least 200' in both directions towards the flow of traffic.

**SIGNS**

**SIGN MATERIALS**

Mesh signs may be used only for Daylight Operations.

Vinyl signs may be used for Day or Night Operations not to exceed 1 day except as noted in the standards.

Rigid or Lightweight sign panels may be used in accordance with the vendor APL drawing for the sign stand to which they are attached.

**INTERSECTING ROAD SIGNING**

Signing for the control of traffic entering and leaving work zones by way of intersecting roads shall be adequate to make drivers aware of work zone conditions. When Work operations exceed 60 minutes, place the ROAD WORK AHEAD sign on the side street entering the work zone.

**ADJOINING AND/OR OVERLAPPING WORK ZONE SIGNING**

Adjoining work zones may not have sufficient spacing for standard placement of signs and other traffic control devices in their advance warning areas or in some cases other areas within their traffic control zones. Where such restraints or conflicts occur or are likely to occur, one of the following methods will be employed to avoid conflicts and prevent conditions that could lead to misunderstanding on the part of the traveling public as to the intended travel way by the traffic control procedure applied:

- (A) For scheduled projects the engineer in responsible charge of project design will resolve anticipated work zone conflicts during the development of the project traffic control plan. This may entail revision of plans on preceding projects and coordination of plans on concurrent projects.
- (B) Unanticipated conflicts arising between adjoining in progress highway construction projects will be resolved by the Resident Engineer for projects under his residency, and, by the District Construction Engineer for in progress projects under adjoining residencies.
- (C) The District Maintenance Engineer will resolve anticipated and occurring conflicts within scheduled maintenance operations.
- (D) The Unit Maintenance Engineer will resolve conflicts that occur within routine maintenance works; between routine maintenance work, unscheduled work and/or permitted work; and, between unit controlled maintenance works and highway construction projects.

**SIGN COVERING AND INTERMITTENT WORK STOPPAGE SIGNING**

Existing or temporary traffic control signs that are no longer applicable or are inconsistent with intended travel paths shall be removed or fully covered.

Sign blanks or other available coverings must completely cover the existing sign. Rigid sign coverings shall be the same size as the sign it is covering, and bolted in a manner to prevent movement.

Sign covers are incidental to work operations and are not paid for separately.

**SIGNING FOR DETOURS, LANE SHIFTS AND DIVERSIONS**

Detours should be signed clearly over their entire length so that motorists can easily determine how to return to the original roadway. The reverse curve (W1-4) warning sign should be used for the advanced warning for a lane shift. A diversion should be signed as a lane shift.

**EXTENDED DISTANCE ADVANCE WARNING SIGN**

Advance Warning Signs shall be used at extended distance of one-half mile or more when limited sight distance or the nature of the obstruction may require a motorist to bring their vehicle to a stop. Extended distance Advance Warning Signs may be required on any type roadway, but particularly be considered on multilane divided highways where vehicle speeds is generally in the higher range (45 MPH or more).

**UTILITY WORK AHEAD SIGN**

The UTILITY WORK AHEAD (W21-7) sign may be used as an alternate to the ROAD WORK AHEAD or the ROAD WORK XX FT (W20-1) sign for utility operations on or adjacent to a highway.

**LENGTH OF ROAD WORK SIGN**

The length of road work sign (G20-1) bearing the legend ROAD WORK NEXT \_\_\_\_\_ MILES is required for all projects of more than 2 miles in length. The number of miles entered should be rounded up to the nearest mile. The sign shall be located at begin construction points.

**SPEEDING FINES DOUBLED WHEN WORKERS PRESENT SIGN**

The SPEEDING FINES DOUBLED WHEN WORKERS PRESENT sign should be installed on all projects, but may be omitted if the work operation is less than 1 day. The placement should be 500 feet beyond the ROAD WORK AHEAD sign or midway to the next sign whichever is less.

**GROOVED PAVEMENT AHEAD SIGN**

The GROOVED PAVEMENT AHEAD sign is required 500 feet in advance of a milled or grooved surface open to traffic. The W8-15P placard shall be used in conjunction with the GROOVED PAVEMENT AHEAD sign.

**END ROAD WORK SIGN**

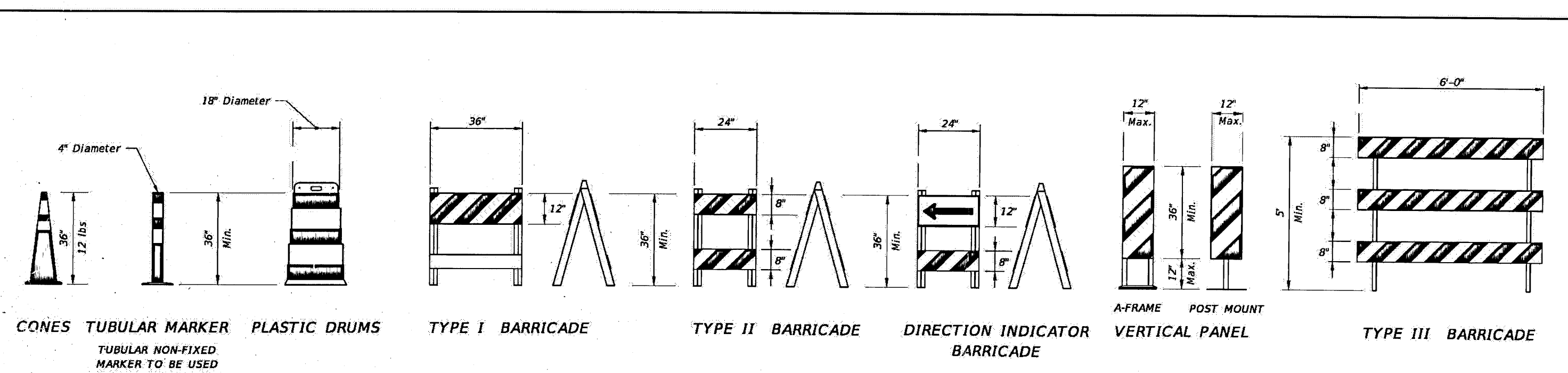
The END ROAD WORK sign (G20-2) should be installed on all projects, but may be omitted where the work operation is less than 1 day. The sign should be placed approximately 500 feet beyond the end of a construction or maintenance project unless other distance is called for in the plans. When other Construction or Maintenance Operations occur within 1 mile this sign should be omitted and signing coordinated in accordance with Index No. 600, ADJOINING AND/OR OVERLAPPING WORK ZONE SIGNING.

**PROJECT INFORMATION SIGN**

The Project information sign shall be installed when called for in the plans.

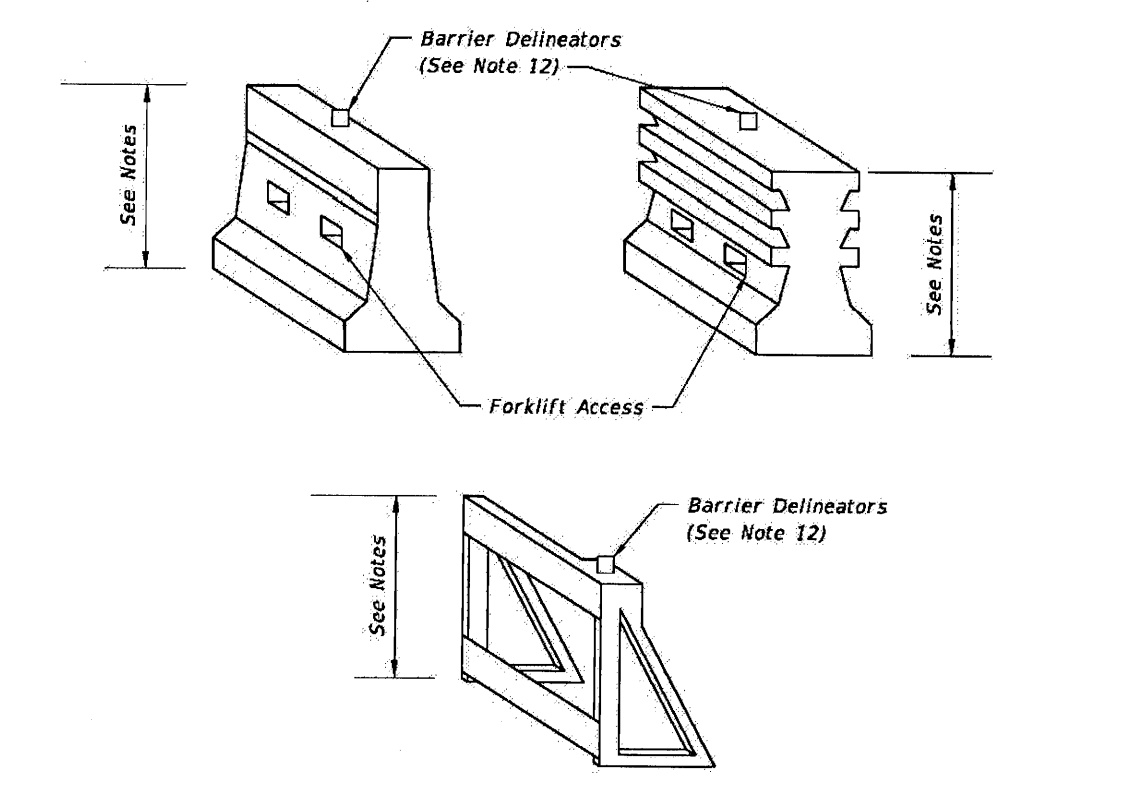
LAST REVISION 07/01/15	DESCRIPTION:	FDOT 2016 DESIGN STANDARDS	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES	INDEX NO. 600	SHEET NO. 4 of 12
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**3 TRAFFIC CONTROL**  
SCALE: 1" = 20'-0"



**CHANNELIZING DEVICE NOTES**

- The details shown on this sheet are for the following purposes:
  - For ease of identification and
  - To provide information that supplements or supersedes that provided by the MUTCD.
- The Type III Barricade shall have a unit length of 6'-0" only. When barricades of greater lengths are required those lengths shall be in multiples of the 6'-0" unit.
- No sign panel should be mounted on any channelizing device unless the channelizing device/sign combination was found to be crashworthy and the sign panel is mounted in accordance with the vendor drawing for the channelizing device shown on the APL.
- Ballast shall not be placed on top rails or any striped rails or higher than 13" above the driving surface.
- The direction indicator barricade may be used in tapers and transitions where specific directional guidance to drivers is necessary. If used, direction indicator barricades shall be used in series to direct the driver through the transition and into the intended travel lane.
- The splicing of sheeting is not permitted on either channelizing devices or MOT signs.
- For rails less than 3'-0" long, 4" stripes shall be used.
- Cones shall:
  - Be used only in active work zones where workers are present.
  - Not exceed 2 miles in length of use at any one time.
  - Be reflectORIZED as per the MUTCD with Department-approved reflective collars when used at night.
- Spacing for longitudinal channelizing devices when placed singly shall be the same as Type I or Type II barricades or drums.
- Vehicular longitudinal channelizing devices shall not exceed 36" in height. For vehicular longitudinal channelizing devices (LCDs) less than 32" in height, the LCD shall be supplemented with approved fixed (surface mounted) channelizing devices (tubular markers, vertical panels, etc.) along the run of the LCD, at the ends, at 50' centers on tangents, and 20' centers on radii. The cost of the fixed supplemented channelizing devices shall be included in the cost of the LCD. LCDs less than 32" in height shall not be used for speeds greater than 45 mph.
- For pedestrian longitudinal channelizing devices, the device shall have a minimum of 8" continuous detectable edging above the walkway. A gap not exceeding a height of 2" is allowed to facilitate drainage. The top surface of the device shall be a minimum height of 32" and have smooth connection points between the devices to facilitate hand trailing. The bottom and the top surface of the device shall be in the same vertical plane. If pedestrian drop-off protection is required, the device must be at least 42" in height above the walkway and be anchored or ballasted to withstand a 200 lb lateral point load at the top of the device.
- For vehicular longitudinal channelizing devices, use Barrier Delineators meeting Specifications Section 993. Place on top of unit so that retroreflective sheeting faces vehicular traffic. Spacing must be a maximum of 50' centers in transitions, 100' centers on curves and 200' centers on tangents. Color must match adjacent longitudinal pavement marking.



**IDENTIFICATIONS - CHANNELIZING DEVICES**

LAST REVISION 07/01/15	DESCRIPTION:	FDOT 2016 DESIGN STANDARDS	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES	INDEX NO. 600	SHEET NO. 11 of 12
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**GENERAL NOTES:**

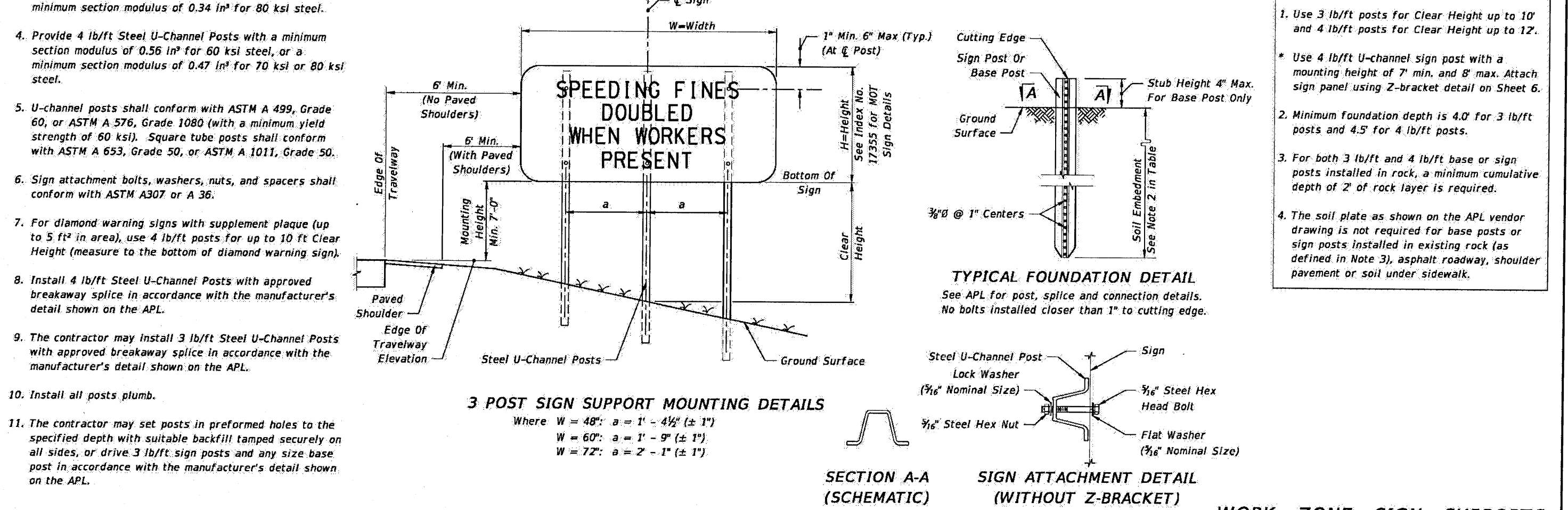
- All signs shall be post mounted when work operations exceed one day except for:
  - Road closure signs mounted in accordance with the vendor drawing for the Type III Barricade shown on the APL.
  - Pedestrian advance warning or regulatory signs mounted on sign supports in accordance with the vendor drawing shown on the APL.
  - Median barrier mounted signs per Index 11871.

**TEMPORARY SIGN SUPPORT NOTE:**

1. Signs mounted on temporary supports or barricades, and barricade/sign combination shall be crashworthy in accordance with NCHRP 350 requirements and included on the Approved Products List (APL).

**POST MOUNTED SIGN NOTES:**

- Use only approved systems listed on the Department's Approved Products List (APL).
- Manufacturers seeking approval of U-Channel and steel square tube sign support assemblies for inclusion on the Approved Products List (APL) must submit a APL application, design calculations (for square tube only), and detailed drawings showing the product meets all the requirements of this Index.



**POST AND FOUNDATION TABLE FOR WORK ZONE SIGNS**

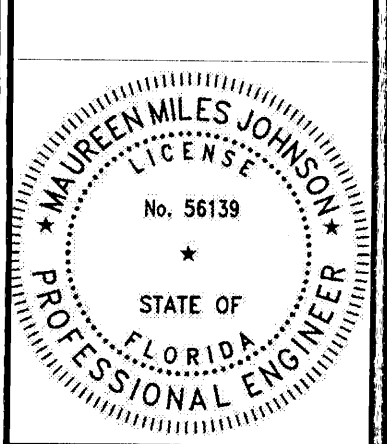
SIGN SHAPE	SIGN SIZE (Inches)	NUMBER OF STEEL U CHANNEL POSTS
Octagon	30x30	1
	36x36	1
	48x48	2
	60x60	2
Triangle	24x18	1
	24x30	1
	30x24	1
	36x18	1
	36x24	1
	48x18	1
	48x24	1
	48x30	2
	48x36	2
	48x48	2
60x54	3	
Square	24x48	2
	30x30	1
	36x36	1
Diamond (See Note 7)	48x48	2
	36x36	2
Circle	36x36	2

**Notes For Table:**

- Use 3 1/2" posts for Clear Height up to 10' and 4 1/2" posts for Clear Height up to 12'.
- Minimum foundation depth is 4.0' for 3 1/2" posts and 4.5' for 4 1/2" posts.
- For both 3 1/2" and 4 1/2" base or sign posts installed in rock, a minimum cumulative depth of 2' of rock layer is required.
- The soil plate as shown on the APL vendor drawing is not required for base posts or sign posts installed in existing rock (as defined in Note 3), asphalt roadway, shoulder pavement or soil under sidewalk.

LAST REVISION 07/01/15	DESCRIPTION:	FDOT 2016 DESIGN STANDARDS	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES	INDEX NO. 600	SHEET NO. 5 of 12
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Maureen M. Johnson, PE, PA  
4423 Windlake Drive ~ Niceville, FL 32578  
Ph 850.279.6670/850.699.0294  
FL PE #56139



Arby's - Cantonment  
FDOT Details

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SCALE AS SHOWN  
DATE 05/05/2016  
SHEET 10 OF 10

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