

PROJECT SPECIFICATIONS, PERMITS AND GEOTECHNICAL REPORT



Detroit Boulevard Intersection Improvements at Pine Forest Road Escambia County, Florida

BID PACKAGE

PREPARED FOR
ESCAMBIA COUNTY ENGINEERING DEPARTMENT

Prepared By:



August 2016

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Appendix A

ESCAMBIA COUNTY GENERAL PAVING AND DRAINAGE TECHNICAL SPECIFICATIONS



Board of County Commissioners • Escambia County, Florida

PUBLIC WORKS DEPARTMENT Engineering Division

Escambia County Technical Specifications

GENERAL EXCEPTIONS*: Any reference to *FDOT Standard Specifications for Road and Bridge Construction, Latest Edition, Division I General Requirements & Covenants* shall be excluded and not applicable to any specification referred herein or otherwise listed in this document.

Work shall comply with requirements of *FDOT Standard Specifications for Road and Bridge Construction*, latest edition, as modified herein.

**Note: The General Exception above does not apply when utilizing Federal Highway Administration (FHWA) funding.*

A handwritten signature in blue ink that reads "Joy D. Blackmon".

County Engineer
Joy D. Blackmon, P.E.

Effective Date: February 01, 2015

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SECTION 01000 – DEFINITIONS

PART 1 - GENERAL

The following terms, when used in the Contract Documents, have the meaning described

Advertisement

The public announcement, as required by law, inviting bids for work to be performed or materials to be furnished, usually issued as “Notice to Contractors,” or “Notice to Bidders.”

Bidder

An individual, firm, or corporation submitting a proposal for the proposed work.

Bridge

A structure, including supports, erected over a depression or over an obstruction such as water, highway or railway, or for elevated roadway, for carrying traffic or other moving loads, and having a length, measured along the center of the roadway, of more than 20 feet between the inside faces of end supports. A multiple-span box culvert is considered a bridge, where the length between the extreme ends of the openings exceeds 20 feet.

Calendar day

Every day shown on the calendar, ending and beginning at midnight.

Contract

The term “Contract” means the entire and integrated agreement between the parties there under and supersedes all prior negotiations, representations, or agreements, either written or oral. The Contract Documents form the Contract between the County and the Contractor setting forth the obligations of the parties thereunder, including, but not limited to, the performance of the Work and the basis of payment.

Contract Documents

The term “Contract Documents” includes: Advertisement for Proposal, Proposal, Certification as to Publication and Notice of Advertisement for Proposal, Appointment of Agent by Nonresident Contractors, Noncollusion Affidavit, Warranty Concerning Solicitation of the Contract by Others, Resolution of Award of Contract, Executed Form of Contract, Performance Bond and Payment Bond, Specifications, plans (including revisions thereto issued during construction), Addenda, or other information mailed or otherwise transmitted to the prospective bidders prior to the receipt of bids, work orders and supplemental agreements, all of which are to be treated as one instrument whether or not set forth at length in the form of contract.

Contract Bond

The security furnished by the Contractor and the surety as a guaranty that the Contractor shall fulfill the terms of the Contract and pay all legal debts pertaining to the construction of the project.

Contract Letting

The date that the County opened the bid proposals.

Contract Time

The number of calendar days allowed for completion of the Contract work, including authorized time extensions.

Contractor

The individual, firm, joint venture, or company contracting with the County to perform the work.

Contractor's Engineer of Record

A Professional Engineer registered in the State of Florida, other than the Engineer of Record or his subcontracted consultant, who undertakes the design and drawing of components of the permanent structure as part of a redesign or Cost Savings Initiative Proposal, or for repair designs and details of the permanent work. The Contractor's Engineer of Record may also serve as the Specialty Engineer. The Contractor's Engineer of Record must be an employee of a pre-qualified firm. Any Corporation or Partnership offering engineering services must hold a Certificate of Authorization from the Florida Department of Business and Professional Regulation.

As an alternate to being an employee of a pre-qualified firm, the Contractor's Engineer of Record may be a pre-qualified Specialty Engineer. For items of the permanent work declared by the State Construction Office to be "major" or "structural", the work performed by a prequalified Specialty Engineer must be checked by another pre-qualified Specialty Engineer. An individual Engineer may become pre-qualified in the work groups listed in the Rules of the Department of Transportation, Chapter 14-75, if the requirements for the Professional Engineer are met for the individual work groups. Pre-qualified Specialty Engineers are listed on the State Construction Website. Pre-qualified Specialty Engineers will not be authorized to perform redesigns or Cost Savings Initiative Proposal designs of items fully detailed in the plans.

Controlling Work Items

The activity or work item on the critical path having the least amount of total float. The controlling item of work will also be referred to as a Critical Activity.

County

Escambia County Public Works Department

Culverts

Any structure not classified as a bridge that provides an opening under the roadway.

Delay

Any unanticipated event, action, force or factor which extends the Contractor's time of performance of any controlling work item under the Contract. The term "delay" is intended to cover all such events, actions, forces or factors, whether styled "delay", "disruption", "interference", "impedance", "hindrance", or otherwise, which are beyond

the control of and not caused by the Contractor, or the Contractor's subcontractors, materialmen, suppliers or other agents. This term does not include "extra work".

Department

State of Florida Department of Transportation.

Developmental Specification

See definition for Specifications.

Engineer of Record

The Professional Engineer or Engineering Firm registered in the State of Florida that develops the criteria and concept for the project, performs the analysis, and is responsible for the preparation of the Plans and Specifications. The Engineer of Record may be County in-house staff or a consultant retained by the County.

The Contractor shall not employ the Engineer of Record as the Contractor's Engineer of Record or as a Specialty Engineer.

Equipment

The machinery and equipment, together with the necessary supplies for upkeep and maintenance thereof, and all other tools and apparatus necessary for the construction and acceptable completion of the work.

Extra Work

Any "work" which is required by the Engineer to be performed and which is not otherwise covered or included in the project by the existing Contract Documents, whether it be in the nature of additional work, altered work, deleted work, work due to differing site conditions, or otherwise. This term does not include a "delay".

Highway, Street, or Road

A general term denoting a public way for purposes of vehicular travel, including the entire area within the right-of-way.

Holidays

Days designated by the Board of County Commissioners as holidays, which include, but are not limited to, New Year's Day, Martin Luther King's Birthday, Memorial Day, Independence Day, Labor Day, Thanksgiving Day and the following Friday, and Christmas Day.

Inspector

An authorized representative of the County, assigned to make official inspections of the materials furnished and of the work performed by the Contractor.

Laboratory

The testing laboratory used by the Contractor.

Major Item of Work

Any item of work having an original Contract value in excess of 5% of the original

Contract amount.

Materials

Any substances to be incorporated in the work under the Contract.

Median

The portion of a divided highway or street separating the traveled ways for traffic moving in opposite directions.

Plans

The approved plans, including reproductions thereof, showing the location, character, dimensions, and details of the work.

Proposal (Bid, Bid Proposal)

The offer of a bidder, on the prescribed form, to perform the work and to furnish the labor and materials at the prices quoted.

Proposal Form

The official form or the expedite program generated bid item sheets on which the County requires formal bids to be prepared and submitted for the work.

Proposal Guaranty

The security furnished by the bidder as guaranty that the bidder will enter into the Contract for the work if the County accepts the proposal.

Right-of-Way

The land that the County has title to, or right of use, for the road and its structures and appurtenances, and for material pits furnished by the County.

Roadbed

The portion of the roadway occupied by the subgrade and shoulders.

Roadway

The portion of a highway within the limits of construction.

Section

A numbered prime division of these Specifications.

Special Provisions

See definition for Specifications.

Specialty Engineer

A Professional Engineer registered in the State of Florida, other than the Engineer of Record or his subcontracted consultant, who undertakes the design and drawing preparation of components, systems, or installation methods and equipment for specific temporary portions of the project work or for special items of the permanent works not fully detailed in the plans and required to be furnished by the Contractor such as but not limited to pot bearing designs, nonstandard expansion joints, MSE wall designs and

other specialty items. The Specialty Engineer may also provide designs and details for items of the permanent work declared by the State Construction Office to be “minor” or “non-structural”. The Specialty Engineer may be an employee or officer of the Contractor or a fabricator, an employee or officer of an entity providing components to a fabricator, or an independent consultant. For items of work not specifically covered by the Rules of the Department of Transportation, a Specialty Engineer is qualified if he has the following qualifications:

- (1) Registration as a Professional Engineer in the State of Florida.
- (2) The education and experience necessary to perform the submitted design as required by the Florida Department of Business and Professional Regulation.

Specifications

The directions, provisions, and requirements contained herein, together with all stipulations contained in the Contract Documents, setting out or relating to the method and manner of performing the work, or to the quantities and qualities of materials and labor to be furnished under the Contract.

- A. Standard Specifications: “Standard Specifications for Road and Bridge Construction” a bound book, applicable to all FDOT Contracts containing adopted requirements, setting out or relating to the method or manner of performing work, or to the quantities and qualities of materials and labor.
- B. Supplemental Specifications: Approved additions and revisions to the Standard Specifications, applicable to all Department Contracts.
- C. Special Provisions: Specific clauses adopted by the Department that add to or revise the Standard Specifications or supplemental specifications, setting forth conditions varying from or additional to the Standard Specifications applicable to a specific project.
- D. Technical Special Provisions: Specifications, of a technical nature, prepared, signed, and sealed by an Engineer registered in the State of Florida other than the State Specifications Engineer or his designee, that are made part of the Contract as an attachment to the Contract Documents.
- E. Developmental Specification: A specification developed around a new process, procedure, or material.

Standard Specifications

See definition for Specifications.

State

State of Florida.

Subarticle

A headed and numbered subdivision of an Article of a Section of these Specifications.

Subgrade

The portion of the roadbed immediately below the base course or pavement, including below the curb and gutter, valley gutter, shoulder and driveway pavement. The subgrade limits ordinarily include those portions of the roadbed shown in the plans to be constructed to a design bearing value or to be otherwise specially treated. Where no limits are shown in the plans, the subgrade section extends to a depth of 12 inches below the bottom of the base or pavement and outward to 6 inches beyond the base, pavement, or curb and gutter.

Substructure

All of that part of a bridge structure below the bridge seats, including the parapets, backwalls, and wingwalls of abutments.

Superintendent

The Contractor's authorized representative in responsible charge of the work.

Superstructure

The entire bridge structure above the substructure, including anchorage and anchor bolts, but excluding the parapets, backwalls, and wingwalls of abutments.

Supplemental Agreement

A written agreement between the Contractor and the County, and signed by the surety, modifying the Contract within the limitations set forth in these Specifications.

Supplemental Specifications

See definition for Specifications.

Surety

The corporate body that is bound by the Contract Bond with and for the Contractor and responsible for the performance of the Contract and for payment of all legal debts pertaining thereto.

Technical Special Provisions

See definition for Specifications.

Traveled Way

The portion of the roadway providing for the movement of vehicles, exclusive of shoulders and auxiliary lanes.

Unilateral Payment

A payment of money made to the Contractor by the Department pursuant to Section 337.11(12), Florida Statutes (2009), for sums the Department determines to be due to the Contractor for work performed on the project, and whereby the Contractor by acceptance of such payment does not waive any rights the Contractor may otherwise have against the Department for payment of any additional sums the Contractor claims are due for the work.

Work

All labor, materials and incidentals required to execute and complete the requirements of the Contract including superintendence, use of equipment and tools, and all services and responsibilities prescribed or implied.

Work Order

A written agreement between the Contractor and the County modifying the Contract within the limitations set forth in these Specifications. Funds for this agreement are drawn against the Initial Contingency Pay Item or a Contingency Supplemental Agreement.

Working Day

Any calendar day on which the Contractor works or is expected to work in accordance with the approved work progress schedule.

END OF SECTION 01000

SECTION 01300 - SUBMITTALS

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

Drawings and General and Supplemental Provisions of the Contract, apply to this Section.

1.2 SUMMARY

A. This Section includes administrative and procedural requirements for submittals required for performance of the Work, including, but not limited to the following:

1. Submittal Procedures
2. Contractor's Construction Schedule
3. Daily Construction Reports
4. Shop Drawings
5. Product Data
6. Samples
7. Quality Assurance Submittals
8. Licenses
9. Pictures, Video of Pre-Construction Conditions

B. Administrative Submittals: Refer to other Sections and other Contract Documents for requirements for administrative submittals. Such submittals include, but are not limited to, the following:

1. Permits
2. Applications for Payment
3. Performance and Payment Bonds
4. Insurance Certificates
5. List of Subcontractors
6. Licenses

1.3 SUBMITTAL PROCEDURES

A. Coordination: Coordinate preparation and processing of submittals with performance of construction activities. Transmit each submittal sufficiently in advance of performance of related construction activities to avoid delay.

1. Coordinate each submittal with fabrication, purchasing, testing, delivery, other submittals, inspections, and related activities that require sequential activity.
2. Coordinate transmittal of different types of submittals for related elements of the Work so processing will not be delayed by the need

to review submittals concurrently for coordination. The County reserves the right to withhold action on a submittal requiring coordination with other submittals until all related submittals are received.

3. Processing: To avoid the need to delay construction as a result of the time required to process submittals, allow sufficient time for submittal review, including time for re-submittals. Allow 2 weeks for initial review. Allow additional time if the County must delay processing to permit coordination with subsequent submittals.
 - a. If an intermediate submittal is necessary, process the same as the initial submittal.
 - b. Allow 2 weeks for reprocessing each submittal.
 - c. No extension of Contract Time will be authorized because of failure to transmit submittals to the County sufficiently in advance of the Work to permit processing.

B. Submittal Preparation: Place a permanent label or title block on each submittal for identification. Indicate the name of the entity that prepared each submittal on the label or title block.

1. Provide a space approximately 4 by 5 inches on the label or beside the title block on Shop Drawings to record the Contractor's review and approval markings and the action taken.
2. Include the following information on the label for processing and recording action taken.
 - a. Project Name.
 - b. Date.
 - c. Name and Address of the Engineer.
 - d. Name and Address of the Contractor.

C. Submittal Transmittal: Package each submittal appropriately for transmittal and handling. Four copies of each submittal (three hard copy and one digital) shall be transmitted. Transmit each submittal from the Contractor to the County, (copy Engineer) using a transmittal form. The County will not accept submittals received from sources other than the Contractor. Submittals must be approved by Contractor prior to review by County. On the transmittal, record relevant information and requests for data. On the form or on a separate sheet, record deviations from Contract Document requirements, including variations and limitations. Include Contractor's certification that the information complies with Contract Document requirements on each submittal.

1.4 CONSTRUCTION SCHEDULE/DOCUMENTATION

- A. Bar-Chart Schedule: Prepare a fully developed, horizontal bar-chart-type, contractor's construction schedule. Submit within 10 days of the issuance of the Notice to Proceed. The contractor shall submit an updated schedule at least once per month, showing any schedule changes. This may be requested up to three times per month by the County. Include dates of shop drawing submittals.
- B. Cost Correlation: At the head of the schedule, provide a cost correlation line, indicating planned and actual costs. On the line, show dollar volume of Work performed as of the dates used for preparation of payment requests.
- C. Pre-Construction Site Conditions Photos/Video: Contractor shall submit a DVD of photos and video of the site conditions prior to the performance of any work.
- D. Licenses: All required licenses to perform work shall be submitted prior to the commencement of construction.

1.5 DAILY CONSTRUCTION REPORTS

Prepare a daily construction report recording the following information concerning events at the site, and submit duplicate copies to the County at weekly intervals including, but not limited to:

1. Work performed.
2. Approximate count of personnel at the site.
3. Count and type of major equipment at the site.
4. High and low temperatures, general weather conditions, including daily rainfall amount from gauge installed on site jointly recorded by contractor and county representative.
5. Accidents and unusual events.
6. Meetings and significant decisions.
7. Stoppages, delays, shortages, and losses.
8. Emergency procedures.
9. Orders and requests of governing authorities.
10. Change Orders received, implemented.
11. Material Expenditures.

1.6 SHOP DRAWINGS

- A. Submit shop drawings for structures unless FDOT approved structures are used.
- B. Shop Drawings – Including, but not limited to the following information:

1. Dimensions.
2. Identification of products and materials included by sheet and detail number.
3. Compliance with specified standards.

1.7 PRODUCT DATA

Product Data - Include the following information:

1. Manufacturer's printed recommendations.
2. Compliance with trade association standards.
3. Compliance with recognized testing agency standards.
4. Application of testing agency labels and seals.

1.8 SAMPLES

Submit samples as specified in the technical specifications.

1.9 QUALITY CONTROL (QC) / QUALITY ASSURANCE (QA) SUBMITTALS

- A. Submit the QC Plan to the County for approval within 21 calendar days after the Notice to Proceed. The County will review the QC Plan and respond to the Contractor within 21 calendar days of receipt.

If at any time the Contractor is not in compliance with the approved QC Plan, or a part thereof, affected portions of the plan will be disapproved. The contractor shall cease work in the affected operation(s) and submit a revision to the County. If the QC Plan, or a part thereof, must be revised, submit the revision to the County. The County will review the revision and respond within seven calendar days of receipt.

Continue to work on operations that are still in compliance with the approved sections of the QC Plan.

- B. Certifications: Where other Sections of the Specifications require certification that a product, material, or installation complies with specified requirements, submit to the County a certification from the manufacturer certifying compliance with specified requirements.
- C. Inspection and Test Reports: Requirements for specific testing are included in the technical specifications.
1. Submit to the County: Two (2) copies (one hard copy and one digital) of the inspection and test reports from a qualified, independent, geotechnical engineering testing agency, under the direction of a Professional Engineer, licensed in the State of Florida.

2. All testing required by the specifications or the County shall be at the contractors expense.
3. No additional work within/upon the tested area shall proceed until submitted test results confirm compliance with specification requirements.
4. Areas where submitted test results indicate non-compliance shall be removed, replaced, and retested. Extents of area out of compliance shall be determined by testing at 25' increments, in each direction within the construction area, until passing results are achieved.
5. Variations from testing requirements and frequency of testing may be authorized by the County and will be documented in writing.

1.10 ENGINEER'S ACTION

Except for submittals for the record or information, where action and return is required, the County will review each submittal, mark to indicate action taken, return to contractor within the timeframe allotted herein. Compliance with specified characteristics is the Contractor's responsibility.

PART 2 - PRODUCTS (Not Applicable)

PART 3 - EXECUTION (Not Applicable)

END OF SECTION 01300

SECTION 02230 - CLEARING & GRUBBING

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of the Contract, including General and Supplementary Conditions shall apply to this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Section 110, Latest Edition.*
- C. Emerald Coast Utility Authority (ECUA) *Engineering Manual, Latest Edition.*

1.2 SUMMARY

- A. This Section includes, but is not limited to, the following:
 - 1. Protection of existing trees indicated to remain.
 - 2. Removal of trees and other vegetation.
 - 3. Clearing and grubbing.
 - 4. Removing above-grade improvements.
 - 5. Removing below-grade improvements.
- B. Extent of clearing & grubbing shall remain in County right-of-way, easements (temporary or permanent), or approved written work agreement areas, unless otherwise noted or instructed.

1.3 PROJECT CONDITIONS

Provide protection for all public land corners and monuments within the limits of construction. Any Monuments disturbed while performing the work will be replaced at the contractor's expense.

PART 2 - PRODUCTS (Not Applicable)

PART 3 - EXECUTION

3.1 SITE CLEARING

- A. General: Remove trees, shrubs, grass, and other vegetation, improvements, or obstructions, as required, to permit installation of new construction. Remove similar items elsewhere on site or premises as specifically indicated. Removal includes digging out and off-site disposal of stumps and roots.

Carefully and cleanly cut minor roots and branches of trees indicated to

remain in a manner where such roots and branches obstruct installation of new construction.

- B. Clearing and Grubbing: Clear site of trees, shrubs, and other vegetation, except for those indicated to remain.
 - 1. Completely remove all stumps within the roadway. Remove roots and other debris to a depth of 12" below the ground surface or finished grade, whichever is lower.
 - 2. Use only hand methods for grubbing inside drip line of trees Indicated to remain.
 - 3. Fill depressions caused by clearing and grubbing operations with satisfactory soil material, unless further excavation or earthwork is indicated in accordance with Section 2300.
- C. Removal of Improvements: Remove existing above grade and below grade improvements as indicated and as necessary to facilitate new construction, and other work as indicated.

3.2 DISPOSAL OF WASTE MATERIALS

- A. Burning: Burning is not permitted on County property. Requests to burn will be considered on a case by case basis. If approved, Contractor is to acquire permits and provide copies to the County.
- B. Removal from County Property: Remove waste materials and unsuitable or excess topsoil from County property, and dispose of off site in a legal manner.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

- A. Lump Sum Payment: When direct payment is provided in the Contract for the quantity to be paid for as the lump sum quantity cleared and grubbed, no additional measurements will be made.
- B. Payment By The Acre/Square Yard: For areas of Clearing and Grubbing that are designated to be paid for separately by the acre or square yard, the quantity to be paid for will be determined by measurement of the areas shown on the plans or authorized by the County to be cleared and grubbed, and acceptably completed.

4.2 BASIS OF PAYMENT

- A. General: Price and payment will be full compensation for all Clearing and Grubbing required for the roadway right-of-way and for lateral ditches, channel changes, or other outfall areas, and any other Clearing and Grubbing indicated, or required for the construction of the entire project, except for any areas designated to be paid for separately or to be specifically included in the costs of other work under the contract. Price and payment, either lump sum or by the acre/square yard will be full compensation for all the work specified in this Section, including all necessary hauling, furnishing equipment, equipment operation, furnishing any areas required for disposal of debris, leveling of terrain and the landscaping work of trimming, etc., as specified herein.
- B. Lump Sum Payment: Payment shall be made at the lump sum contract price for Clearing and Grubbing, lump sum.
- C. Payment: Payment shall be made at the per unit contract price for Clearing and Grubbing, per acre or square yard.

END OF SECTION 02230

SECTION 02300 - EARTHWORK

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of the Contract, including General and Supplementary Conditions, apply to this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Latest Edition*.

1.2 SUMMARY

- A. This Section includes preparing and grading for pavement, curb, subgrades, drainage features, and general site work.
- B. Related Sections: The following Sections contain requirements that relate to this Section.
 - 1. Section 2230 "Clearing & Grubbing" for clearing, grubbing, and tree protection.
 - 2. Section 2600 "Stormwater System" for installation of stormwater systems.

1.3 DEFINITIONS

- A. Excavation consists of the removal of material encountered to subgrade elevations and the reuse or disposal of materials removed.
- B. Subgrade: The uppermost surface of an excavation or the top surface of a fill or backfill immediately below subbase, base, drainage fill, or topsoil materials.
- C. Borrow: Soil material obtained off-site when sufficient approved soil material is not available from on-site excavations.
- D. Subbase Course: The layer placed between the subgrade and base course in a paving system.
- E. Base Course: The layer placed immediately beneath the surface pavement in a paving system.
- F. Unauthorized excavation consists of removing materials beyond indicated subgrade elevations or dimensions without direction by the County. Unauthorized excavation, as well as remedial work directed by the Engineer, shall be at the Contractor's expense.

- G. Structures: Buildings, footings, foundations, retaining walls, slabs, tanks, curbs, mechanical and electrical appurtenances, or other man-made stationary features constructed above or below ground surface.
- H. Utilities include on-site above ground utilities, overhead utilities and underground utilities including: pipes, conduits, ducts, and cables, as well as related appurtenances and underground services within building lines.
- I. Unsuitable Material: Any material such as muck, wood, rock, peat, garbage, non-compactable soils in dry condition, and any other material that is considered by the County Engineer to be unsuitable.
- J. Topsoil: Topsoil is defined as the surface layer of soil found normally to a depth of at least 4 to 8 inches that typically contains organic materials. Satisfactory topsoil is reasonably free of roots, clay lumps, stones, other objects over 2 inches in diameter, and any other objectionable or deleterious material.

1.4 SUBMITTALS

- A. General: Submit the following in accordance with Section 1300, "Submittals."
- B. Product Data and Samples of the following:
 - 1. 1-lb representative samples of each proposed fill and backfill soil material from borrow sources as selected by the County.
 - 2. 12-by-12-inch sample of filter fabric.
 - 3. Representative samples of the proposed base and sub-base materials.
- C. Test Reports: In addition to test reports required under field quality control, submit the original directly to the County from the testing services, with a copy to the Contractor:
 - 1. Laboratory analysis as specified in 1.1 (Related Documents) of each soil material proposed for fill and backfill from borrow sources.
 - 2. One optimum moisture-maximum density curve for each soil material.
 - 3. Report of actual unconfined compressive strength and/or results of bearing tests of each stratum tested.

1.5 QUALITY CONTROL / QUALITY ASSURANCE

- A. Codes and Standards: Perform earthwork complying with all requirements of authorities having jurisdiction.
- B. Testing and Inspection Service: A qualified independent geotechnical engineering testing agency, under the direction of a Professional Engineer, licensed in the State of Florida to classify, perform soil tests, and provide inspection services for quality control. All proposed borrow soils will require the testing agency to verify that soils comply with specified requirements and to perform required field and laboratory testing. Contractor shall replace materials removed for testing purposes. Should any work or materials fail to meet the requirements set forth in the plans and specifications, contractor shall reimburse for additional and re-testing.

1.6 PROJECT CONDITIONS

- A. Site Information: Data in the subsurface investigation Report, if available, is used for the basis of the design and is available to the contractor for information only. Conditions are not intended as representations or warranties of accuracy or continuity between soil borings. The County will not be responsible for interpretations or conclusions drawn from this data by the Contractor.
- B. Existing Utilities: After location of utilities by the appropriate utility company, it is the Contractor's responsibility to protect all such utility lines, including service lines and appurtenances, and to replace at his own expense any that may be damaged by the Contractor's equipment or forces during construction of the Project.
 - 1. Provide a minimum of 48-hours notice to the County and receive written notice to proceed before interrupting any utility.
 - 2. The contractor is responsible for contacting all utility companies to verify locations of all existing utilities, utility-related obstructions, or utility relocations that he may encounter during construction.
 - 3. Adequate provision shall be made for the flow of existing sewers, drains, and water courses encountered during construction, and structures which may be disturbed shall be satisfactorily restored by the Contractor at his expense.
- C. Should uncharted, or incorrectly charted, piping or other utilities be encountered during the course of the work, consult the County immediately for directions. Cooperate with the County and utility companies in keeping respective services and facilities in operation.

PART 2 - PRODUCTS

2.1 SOIL MATERIALS

- A. General: Soils used as fill shall be clean sands, similar to existing site soil, with less than 5% passing the number 200 sieve when existing subgrade conditions are considered wet as per the County. Soils as described above with less than 15% passing the number 200 sieve and meeting the requirements of Section 902-6 of the FDOT Specifications may be used when existing subgrade conditions are considered dry as per the County. The sand shall have a maximum dry density of at least 100 pounds per cubic foot, according to the Standard Proctor compaction test, AASHTO T-99, ASTM D698. Provide approved borrow soil materials from off-site when sufficient satisfactory soil materials are not available from on-site excavations.

If the Contractor elects to import any materials, then he will do so only with the approval of the County and at his own expense, unless separate payments for such items are called for in these specifications. Provide laboratory certification that soils meet requirements of specifications.

- B. Sub-Base Material: Naturally or artificially graded mixture of natural or crushed gravel, crushed stone, or sand. The material shall be stabilized in accordance with FDOT Standard Specification Section 160-5.4. ASTM D 2940, with at least 95 percent passing a 1-1/2-inch sieve, and not more than 8 percent passing a No. 200 sieve.

PART 3 - EXECUTION

3.1 DEWATERING

- A. Prevent surface water and subsurface or groundwater from entering excavations, from ponding on sub-grades in work areas, and from flooding project site and surrounding area.
- B. Protect subgrades and foundation soils from softening and damage by rain or water accumulation.
- C. The Contractor shall prevent the accumulation of water in excavated areas, and shall remove, by pumping or other means, any water that accumulates in the excavation. The Contractor shall prevent the accumulation of water in both structural and trench excavations and shall remove, by well point system or by other means, water which accumulates. The Contractor shall provide, install and operate a suitable and satisfactory dewatering system, when needed to dry sub-grades or other work areas. The Contractor shall comply with the latest testing requirements as set forth by the applicable regulatory agency. At a minimum, the contractor shall test once prior to dewatering, once within

the first week of dewatering, and once every thirty (30) days while dewatering.

- D. Establish and maintain temporary drainage ditches and other diversions outside excavation limits to convey rainwater and water removed from excavations to collection or runoff areas. Do not use trench excavations as temporary drainage ditches. Discharged water shall be clean, not silt or sediment laden, prior to discharge to untreated system and/or waters of the State.

3.2 EXCAVATION

- A. Explosives: Not permitted.
- B. Strip topsoil and significant root systems to whatever depths encountered in a manner to prevent intermingling with underlying subsoil or other objectionable material. Remove heavy growths of grass from areas before stripping. Where existing trees are indicated to remain, leave existing topsoil in place within drip lines to prevent damage to root systems.

3.3 STABILITY OF EXCAVATIONS

- A. Comply with local codes, ordinances, and requirements of authorities having jurisdiction to maintain stable excavations.
- B. All excavation work shall conform to all applicable OSHA Publications, Latest Editions. The Contractor's method of providing protective support to prevent cave-ins shall conform to OSHA requirements. Slope excavations, shoring, and trench box usage in the field must be based on tabulated data and designed by the Contractor. The contractor is solely responsible for job site safety and shall not be compensated for required safety equipment/devices.

3.4 EXCAVATION FOR STRUCTURES

Excavate to indicated elevations and dimensions within a tolerance of plus or minus 0.10 foot. Extend excavations a sufficient distance from structures for placing and removing concrete formwork, maintaining a safe slope, installing services and other construction, and for inspections.

- A. Footings and Foundations: Excavate by hand to final grade just before placing concrete reinforcement. Trim bottoms to required lines and grades to leave solid base to receive other work.
- B. Pile Foundations: After piles have been installed, remove loose and displaced material. Excavate to final grade, leaving solid base to receive concrete pile caps.

- C. Excavation for Underground Tanks, Basins, and Mechanical or Electrical Appurtenances: Excavate to elevations and dimensions indicated within a tolerance of plus or minus 0.10 foot.

3.5 EXCAVATION FOR WALKS AND PAVEMENTS

Excavate surfaces under walks and pavements to indicated cross sections, elevations, and grades. Consider Dewatering and other sections as applicable.

3.6 EXCAVATION FOR STORMWATER SYSTEMS

Excavate and compact the backfill of trenches to the densities specified for embankment or subgrade, as applicable, and in accordance with the requirements of Section 2600. Consider Dewatering and other sections as applicable.

3.7 STORAGE OF SOIL MATERIALS

Stockpile excavated materials acceptable for backfill, fill soil, and topsoil materials, including acceptable borrow materials. Stockpile soil materials without intermixing. Stockpiles shall be placed, graded, and shaped to drain surface water and prevent erosion. Cover to prevent wind-blown dust and/or erosion. Stockpile soil materials away from edge of excavations. Do not store within drip line of remaining trees.

3.8 BACKFILL

- A. Backfill excavations promptly, but not before completing the following:

1. Acceptance of construction below finish grade including, where applicable, filter fabric installation and gravel bedding.
2. Surveying locations of underground utilities for record documents.
3. Testing, inspecting, and approval of underground utilities.
4. Removal of trash and debris from excavation.
5. Removal of temporary shoring, bracing, and sheeting unless specified to remain.

- B. No backfill material shall be placed, spread or rolled during unfavorable weather conditions. When the work is interrupted by heavy rain, backfill operations shall not be resumed until the moisture content of the fill is as previously specified to achieve proper compaction.

3.9 FILL

- A. Preparation: Remove vegetation, topsoil, debris, wet and unsatisfactory soil materials, obstructions, and deleterious materials from ground surface prior to placing fills. Plow strip, or break up sloped surfaces steeper than 1 vertical to 4 horizontal so fill material will bond with existing surface. In order to insure proper bond and prevent slipping between the original ground and fill, the surface of the original ground shall be scarified to a depth of at least three inches. Each layer of fill material shall be compacted until the required density is achieved, and the density achieved should be verified in accordance with specifications using in-place density testing.
- B. When subgrade or existing ground surface is to receive fill and has a density less than that required for fill, break up ground surface to depth required, pulverize, moisture condition or aerate soil and re-compact to required density.
- C. Place fill material in layers to required elevations for each location listed below.
 - 1. Under grass, subbase or base material, use satisfactory excavated or borrow soil material.
 - 2. Under walks and pavements, curbs, steps, ramps, building slabs, footings and foundations use subbase and/or base material.

3.10 MOISTURE CONTROL

- A. Uniformly moisten or aerate subgrade and each subsequent fill or backfill layer before compaction to within 2 percent of optimum moisture content.
- B. Do not place backfill or fill material on surfaces that contain excessive moisture.
- C. Remove and replace, or scarify and air-dry satisfactory soil material that is too wet to compact to specified density. Stockpile or spread and dry removed wet satisfactory soil material.

3.11 COMPACTION

- A. Place backfill and fill materials in layers or lifts not more than 12 inches in loose depth for material compacted by heavy compaction equipment, and not more than 8 inches in loose depth for material compacted by hand-operated tampers.
- B. Place backfill and fill materials evenly on all sides of structures to required elevations. Place backfill and fill uniformly along the full length of each structure.

- C. Percentage of Maximum Dry Density Requirements: Compact soil to not less than the following percentages of maximum dry density according to ASTM Modified Proctor):
 - 1. Under structures, building slabs, steps, and pavements, compact each layer of backfill or fill material at a minimum of 98% Modified Proctor of the material's maximum dry density.
 - 2. Under lawn or unpaved areas, compact each layer of backfill or fill material at 95% Modified Proctor maximum dry density.

3.12 GRADING

- A. General: Uniformly grade areas to a smooth surface, free from irregular surface changes. Comply with compaction requirements and grade to cross sections, lines, and elevations indicated.
 - 1. Provide a smooth transition between existing adjacent grades and new grades.
 - 2. Cut out soft spots, fill low spots, and trim high spots to conform to required surface tolerances.
- B. Site Grading: Slope grades to direct water away from buildings and to prevent ponding. Finish subgrades to required elevations within the following tolerances:
 - 1. Lawn or Unpaved Areas: Plus or minus 0.10 foot.
 - 2. Walks: Plus or minus 0.10 foot.
 - 3. Pavements: Plus or minus ½ inch.

3.13 STABILIZED SUBGRADE

- A. For stabilized subgrade the type of materials, commercial or local, is at the Contractor's option and no separate payment for stabilizing materials will be made (other than as may be paid for as borrow).
- B. When stabilizing is designated as Type B, compliance with the bearing value requirements will be determined by the Limerock Bearing Ratio Method. Minimum LBR shall be 40.
- C. It is the Contractor's responsibility that the finished roadbed section meets the bearing value requirements, regardless of the quantity of stabilizing materials necessary to be added. Also, full payment will be made for any areas where the existing subgrade materials meet the design bearing value requirements without the addition of stabilizing additives, as well as areas where the Contractor may elect to place select high-bearing

materials from other sources, within the limits of the stabilizing.

- D. After the roadbed grading operations have been substantially completed, the Contractor shall make his own determination as to the quantity (if any) of stabilizing material, of the type selected by him, necessary for compliance with the bearing value requirements. The contractor shall notify the Engineer of the approximate quantity to be added, and the spreading and mixing-in of such quantity of materials shall meet the approval of the County as to uniformity and effectiveness.

3.14 FIELD QUALITY CONTROL

- A. Testing Agency Services: Allow testing agency to inspect and test each subgrade and each fill or backfill layer. Do not proceed until test results for previously completed work verify compliance with requirements.
 - 1. Perform field in-place density tests according to ASTM D 1556 (sand cone method), ASTM D 2167 (rubber balloon method), ASTM D 293 (drive cylinder method), or ASTM D 2922 (nuclear method), as applicable.
 - a. Field in-place density tests may also be performed by the nuclear method according to ASTM D 2922, provided that calibration curves are periodically checked and adjusted to correlate to tests performed using ASTM D 1556. With each density calibration check, check the calibration curves furnished with the speedy moisture meter according to ASTM D 3017.
 - b. When field in-place density tests are performed using nuclear methods, make calibration checks of both density and speedy moisture meter at beginning of work, on each different type of material encountered, and at intervals as directed by the Engineer.
 - 2. Paved Areas: Make at least one field density test of subgrade, base, and each compacted fill layer for every 300 linear feet of roadway or equivalent area, but in no case less than two tests. Tests shall be staggered to ensure representative sampling.
 - 3. Unpaved Areas: Make at least one field density test of each compacted fill layer or subgrade for every 1000 square yards of area, but in no case less than two tests.
 - 4. Other tests may be required at County's discretion.
- B. If, in the opinion of the County, based on testing service reports and

inspection or the Engineer's observations, subgrades, fills, or backfills are below specified density, scarify and moisten or aerate as needed, or remove and replace soil to the depth required, re-compact, and re-test until required density is obtained at no additional expense.

3.15 REPAIR & CORRECTIONS

- A. Protecting Graded Areas: Protect newly graded areas from traffic and erosion. Keep free of trash and debris. Repair and re-establish grades to specified tolerances where completed or partially completed surfaces become eroded, rutted, settled, or lose compaction due to subsequent construction operations or weather conditions. Scarify or remove and replace material to depth directed by the Engineer; reshape and re-compact at optimum moisture content to the required density.
- B. Settling: Where settling occurs, remove finished surfacing, backfill with additional approved material, compact, and reconstruct surfacing. Restore appearance, quality, and condition of finished surfacing to match adjacent work, and eliminate evidence of restoration to the greatest extent possible.
- C. When traffic must cross open trenches, the contractor shall provide suitable bridge of graded aggregate base or temporary asphalt paving as directed by County at no additional expense. (See Section 4060 for additional requirements.)
- D. Erosion Control: The Contractor shall be responsible for the prevention of erosion from the site and for maintaining filled and graded surfaces for the duration of the project. This includes, but is not limited to, the erection of a silt fence and hay bale barricade as per Florida Stormwater Erosion and Sedimentation Control Inspector's Manual and/or as shown in the construction plans. The Contractor shall take whatever steps necessary to prevent erosion and sedimentation, and will be responsible for any damages which might occur to down-land properties as a result of run-off from the site during sitework construction at no additional cost. Provide erosion control measures to prevent erosion or displacement of soils and discharge of soil-bearing water runoff or airborne dust to adjacent properties and walkways.

3.16 DISPOSAL OF SURPLUS AND WASTE MATERIALS

Surplus excavated material becomes the property of the Contractor unless otherwise noted. Waste materials, including unsatisfactory soils, trash and debris shall be removed and legally disposed of, off the Owner's property.

3.17 CLEAN-UP AND FINAL INSPECTION

Before final inspection and acceptance the Contractor shall clean ditches, shape

shoulders and restore all disturbed areas, including street crossings, grass plots, re-grassing if necessary, to as good a condition as existed before work started.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

- A. Excavation: When payment for excavation is on a volumetric basis, the quantity to be paid for will be the volume, in cubic yards, calculated by the method of average end areas according to the survey and plans. If actual quantities vary in field, contractor shall communicate with Engineer and/or County to request additional payment. The measurement will include the net volume of material between the original ground surface and the surface of completed earthwork according to the survey and plans. If actual quantities vary in field, contractor shall communicate with the County to request additional payment. Excavation for swales and channels will be included in the total quantity for Excavation. Subsoil Excavation will be measured to the lines and grades indicated on the plans or as approved by the County. Backfill material shall either include normal excavation material from within project limits or borrow material supplied by the Contractor.
- B. Embankment: Quantities for Embankment will be calculated by the method of average end or square yard areas, and will include material placed above the original ground line, within the lines and grades indicated on the plans or as directed by the County.
- C. Calcium Chloride for Dust Control: The quantity to be paid for will be the weight, in tons, of calcium chloride authorized and acceptably spread on the road, within the limits specified by the County. The quantity will be determined from scales, certified freight bills, or other sources, the accuracy of which can be authenticated.

4.2 BASIS OF PAYMENT

- A. General: Prices and payments for the various work items included in this section will be full compensation for all work described herein, including excavating, dewatering, dredging, hauling, placing, and compacting. Separate pay items will be provided for all devices required to maintain control of erosion according to plans and NPDES permit. Additional devices shall be no additional cost.
- B. Excavation: Unit prices will be established for required cubic yard volumes of Regular Excavation, Subsoil Excavation, and Borrow Excavation as necessary. When subsoil excavation is required to a depth greater than plans and specifications require, and additional excavation is not due to unsuitable, a change order will be required to establish a new quantity utilizing the current unit price.

- C. Embankment: Payment shall be made at the unit contract price for Embankment, cubic yard or square yard, in place, according to plans.
- D. Calcium Chloride for Dust Control: Price and payment will be full compensation for all work and materials specified for this item, including specifically all required shaping and maintenance of the treated area and all water furnished and applied to the area.
- E. Dewatering: The contractor shall include the cost of dewatering in the unit price bid for the stormwater pipe if there is not a specific line item used in the contract.

END OF SECTION 02300

SECTION 02320 - UNDERDRAIN AND EXFILTRATION TRENCH

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawing and general provisions of Contract, including General and Supplementary Conditions and other Specification Sections, apply to work of this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Section 440, and Design Standards, Latest Edition.*

1.2 DESCRIPTION OF WORK

This section shall cover the work of furnishing pipe for underdrain and exfiltration trenches, the type and size shown on the plans or in the proposal in accordance with the requirements of these specifications, and installing such pipe at the locations shown on the plans or designed by the County in substantial conformity with the established lines, trench widths, filter media, and grades. The work shall include furnishing and construction of such joints and connections to other pipes as may be required to complete the work, as shown on the plans or directed, together with the specified trench, filter media, and filter fabric materials. Filter media may be sand, gravel, gravel pack, and/or a combination thereof.

PART 2 - PRODUCTS

- A. Underdrain Pipe: Underdrain pipe shall conform to the requirements of Florida Department of Transportation Standard Specification 948-2, Latest Edition. The perforations shall meet the requirements for perforations as specified in AASHTO M294. Pipe shall be perforated polyethylene ADS N-12 or approved equal.
- B. Filter Aggregate shall be clean, washed gravel free of organic material and fines with minimum 33% percent voids. Aggregate shall be at least FDOT stone or equivalent.
- C. Filter Fabric shall be Terratex EP (Woven) or approved equal meeting the requirements of FDOT Specification Section 985.
- D. Filter sand shall be clean sand, free of organic materials and fines, with an effective grains size of 0.20 to 0.55 mm, uniformity coefficient of 1.5 to 4.0, a K value (recommended design permeability) not to exceed 2.5 ft/hr, and the contractor shall provide a grain-size analysis to the County for the project.

PART 3 - EXECUTION

3.1 STANDARD INSTALLATION:

- A. Trenches shall be excavated to the dimensions shown on the plans or as directed. A bedding layer of filter aggregate or filter sand of the size shown on the plans shall be placed in the bottom of the filter fabric lined trench for its full width and length and compacted as directed.
- B. Subdrainage pipe of the type and size specified shall be embedded firmly in the bedding material. All pipe sections shall be securely joined with the appropriate coupling fittings or bands as per manufacturer's specifications.
- C. After the pipe installation has been inspected and approved, the specified filter aggregate, gravel pack, and/or filter sand shall be placed as shown on the drawings and filter fabric wrapped around the filter media with a minimum overlap (as required) on top. Install wire mesh at opening/outfall. Care shall be taken not to displace the pipe.
- D. The Contractor shall take every precaution to prevent the entrance of soil and sediments into the filter bed during construction, which would sacrifice the integrity of the filter fabric and aggregate installed. Stormwater runoff and sedimentation controls to be provided so as to protect the underdrain or exfiltration trench system.
- E. Installation of the corrugated polyethylene pipe shall be in accordance with the ASTM D-2321 Latest Revision.
- F. Gravel packs shall be installed around the filter-drain underdrain pipe an average thickness of at least six inches from the underdrain pipe for all underdrain systems located within pond bottoms and pond banks. The minimum separation between the gravel pack and the top of the filter bed shall be two feet. A permeable filter fabric shall be wrapped around the gravel pack.
- G. Cleanouts or inspection boxes shall be installed, at minimum, every 400 feet or as specified by plan, at every directional change or bend, and at the beginning (upstream) and at the terminus (downstream) of the underdrain pipes or exfiltration trench systems.
- H. Cleanouts shall have vertical portions non-perforated, include water-tight caps, and shall incorporate fittings (wye fittings or elbow bends) that have an angle no less than 45 degrees.

PART 4 – MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The quantities to be paid for will be the length, in feet, of underdrain, which include trench filter fabric, measured in place, along the centerline and gradient of the underdrain, completed and accepted. The quantities to be paid for will be the length, in feet, of outlet pipe measured in place, along the centerline and gradient of the outlet pipe, completed and accepted. The quantity of underdrain inspection boxes and cleanouts to be paid for will be the number completed and accepted. When payment for gravel or sand filter media is on a volumetric basis, the quantity to be paid for will be the volume, in cubic yards, calculated based upon the length, width, and depth of the underdrain or exfiltration trench dimensions, minus the pipe volume, as shown on the plans, unless the filter media is specified as a bid item included in the costs of the underdrain or exfiltration system.

4.2 BASIS OF PAYMENT

Prices and payment for this item shall include all labor, equipment and materials necessary to complete the work in accordance with the plans and specifications. Materials covered under this pay item include but are not limited to: pipe including fittings, filter fabric, filter media, filter sand, filter aggregate, cleanout structures and inspections boxes. Payment shall be made for the underdrain or exfiltration pipe based upon the length of pipe. Unit prices will be established for the number of cleanouts structures and inspection boxes, unless otherwise specified as a bid item included in the under underdrain or exfiltration trench system. Unit prices will also be established for required cubic yard volume of gravel or sand filter media based upon the length, width, and depth of the underdrain or exfiltration trench, minus the pipe volume, as shown on the plans, unless otherwise specified as a lump sum bid item included in the costs of the underdrain or exfiltration trench system. No additional payment will be made for filter media overages larger than the specified plan volume. No additional payment will be made for excavation of the trench or lining the trench with filter fabric. No additional payment will be made for underdrain pipe with a sock filter fabric.

END OF SECTION 02320

SECTION 02340 - RIPRAP

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of Contract, including General and Supplementary Conditions and other Specification Sections, apply to work of this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Section 530*, and *Design Standard Index 281, Latest Edition*.

1.2 DESCRIPTION OF WORK

This section shall cover the work of furnishing and constructing the Riprap which shall consist of a protective course of stone or other approved materials on embankment slopes, in channels, or other work as shown on the plans or directed, with or without a Filter Blanket, all in accordance with these Specifications and in conformity with the lines and grades noted in the plan details.

PART 2 - PRODUCTS

2.1 MATERIALS

Rubble\Stone Riprap shall comply with Florida Department of Transportation *Standard Specification 530-2.2*

- A. Banks and shore protection shall comply with Florida Department of Transportation *Standard Specification 530-2.2.1*.
- B. Ditch lining shall comply with Florida Department of Transportation *Standard Specification 530-2.2.2*.
- C. Broken stone and broken concrete shall comply with Florida Department of Transportation *Standard Specification 530-2.2.3*.
- D. Geotextile fabric shall comply with Florida Department of Transportation *Standard Specification 514* and Florida Department of Transportation *Design Standards, Index No. 199* according to its application.
- E. Bedding stone shall comply with Florida Department of Transportation *Standard Specification 530-2.3*.
- F. Sand/Cement Riprap: Materials and placement shall comply with Florida Department of Transportation *Standard Specification 530-2.1*.

PART 3 - EXECUTION

3.1 EXECUTION

A. Construction Requirements:

General: All slopes to be treated with riprap shall be trimmed to the lines and grades indicated by the plans or directed, such that the plan grades are the top of the placed riprap, unless otherwise noted. Loose material shall be compacted by methods approved by the Engineer or removed.

Slopes which require a filter blanket under the riprap shall, in addition to the above, be prepared as noted below.

1. Placement of any riprap on a filter blanket shall be by such means that will not damage or destroy the blanket. Any damage to the blanket shall be repaired without additional compensation.
2. Unless directed otherwise by the Engineer or shown by plan details, all outer edges and the top of riprap where the riprap terminates shall be formed so that the surface of the riprap will be embedded and even with the surface of the ground and/or slope.
3. All riprap construction shall begin at the bottom of the slope and progress upward.
4. Filter Blanket: Unless otherwise specified by the plans or ordered in writing, a fabric blanket will not be allowed for soils with 85% by weight passing the No. 200 sieve (U.S. Std.)
5. The bedding stone shall be constructed in accordance with Florida Department of Transportation Specification 530-3.3.
6. Foundation Preparation: Areas on which filter fabrics are to be placed shall be uniformly trimmed and dressed to conform to cross-sections shown by the plans.

B. Plastic Filter Fabric (Geotextile):

Plastic filter fabric shall be placed in the manner and at the locations shown in the plans or as directed by the Engineer. At the time of installation, fabric shall be rejected if it has defects, rips, holes, flaws, deterioration or damage incurred during manufacturer, transportation or storage. The fabric shall be placed with the long dimension parallel to the centerline of the channel or shoreline unless otherwise directed by the Engineer, and shall be laid smooth and free of tension, stress, folds, wrinkles or creases. The strips shall be placed to provide a minimum

width of 24 inches of overlap for each joint with the upstream strip of fabric overlapping the downstream strip. Overlap joints and seams shall be measured as a single layer of cloth. Securing pins with washers shall be inserted through both strips of overlapped cloth as recommended by the manufacturer, but no greater than the following intervals along a line through the midpoint of the overlap.

<u>Pin Spacing</u>	<u>Slope</u>
2 ft.	Steeper than 3:1
3 ft.	3:1 to 4:1
5 ft.	Flatter than 4:1

The fabric shall be turned down and buried two feet at all exterior limits except where a stone-filled key is provided below natural ground.

Additional pins regardless of location shall be installed as necessary to prevent any slippage of the filter fabric. Overlaps in the fabric shall be placed so that any upstream strip of fabric will overlap the downstream strip. Should the Engineer direct that the fabric be placed with the long dimension perpendicular to the centerline of the channel or shoreline, the lower strip of fabric shall underlap the next higher strip. Each securing pin shall be pushed through the fabric until the washer bears against the fabric and secures it firmly to the foundation. The fabric shall be protected at all times during construction from contamination by surface runoff and any fabric so contaminated shall be removed and replaced with uncontaminated fabric. Any damage to the fabric during its installation or during placement of riprap shall be replaced by the Contractor. The work shall be scheduled so that the manufacturer's recommendation for UV exposure is not exceeded or 5 days does not expire between placement of the fabric and the covering of the fabric with riprap, whichever is less.

3.2 STONE AND CONCRETE RUBBLE RIPRAP

General: Unless otherwise shown by plan details or directed, stone or concrete shall not be placed on slopes steeper than the natural angle of repose of the riprap material.

Placement of stone or concrete may, unless otherwise noted hereinafter, be placed by methods and equipment suitable for the purpose of placing the riprap in accordance with the requirements for the class riprap involved without damaging any existing facility or construction material.

The stone or concrete shall be placed in such a manner as to produce a reasonably well graded mass of rock with the minimum practical percentage of voids. Stone or concrete shall be laid with close broken joints and resting on the embankment slope. The top of the riprap shall be constructed to the lines, grades and thickness shown by the plans or as directed. Riprap shall be placed

to its full course thickness in one operation and in such a manner as to avoid displacing or damaging the filter blanket material. The larger stone or concrete shall be well distributed and the entire mass of stone or concrete, in their final position, shall conform to a reasonable uniform gradation. The finished riprap shall be free from objectionable pockets of small stone or concrete and clusters of larger stone or concrete. Open joints shall be filled with spalls, or small stone or concrete in such manner that all stone or concrete are tightly wedged or keyed. Placing riprap by dumping into chutes or by other methods likely to cause segregation of sizes will not be permitted. The desired distribution of the various sizes of stone or concrete throughout the mass shall be obtained by selective loading of the material at the source, by controlled dumping of successive loads during final placing, or by other methods of placement which will produce the specified results. The individual pieces of stone or concrete in each horizontal course shall be laid so that they will not break away from embankment. Rearranging of individual stone or concrete by mechanical equipment, or by hand, will be required to the extent necessary to obtain a reasonably well graded distribution of stone or concrete as specified above.

3.3 SAND/CEMENT RIPRAP

- A. Placing: Immediately following mixing, the mixture shall be placed in the bags, tied (so that when laid in position, they will flatten out and give a thickness of not less than six inches) and placed flat on the area designed. Use only one type of bag per structure. Bags shall be layered and wedged against each other to form closed joints, with tied ends of sacks all laid in the same direction. Sacks ripped or torn in placing shall be removed and replaced with sound, unbroken sacks. When required to be placed under water, special care shall be taken to see that bags are closely jointed to give the same tight joints as required on dry slopes. After the riprap is placed, it shall be sprinkled with water as directed and kept damp for not less than three days. No sand/cement riprap shall be mixed in freezing weather.
- B. Grouting: Immediately after watering, all openings between sacks shall be filled with dry grout composed of one part Portland cement and five parts sand.
- C. Pinned/Staked Bags: Bags shall be pinned/staked when called for on drawings.

3.4 CLEAN UP

Before final inspection and acceptance, the Contractor shall remove all excess material from site and restore all disturbed areas to as good a condition as existed before work started.

3.5 MAINTENANCE

The Contractor shall maintain all riprap until the contract work is accepted, and shall replace, without additional compensation, any damaged or missing riprap.

PART 4 – MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

- A. Sand-Cement: The quantity to be paid for will be the volume, in cubic yards, of sand actually used in the sand cement mixture and grout, satisfactorily placed and accepted. If sand is proportioned by volume, the sand will be measured loose in an approved measure prior to mixing with cement. If sand cement is proportioned by weight, approved scales will be used for this purpose and the volume will be calculated using a standard conversion factor for sand of 85lbs. /cubic feet. No adjustment of batch weights to allow for varying moisture content of the sand will be made.
- B. Stone/Concrete Rubble and Bedding Stone: The quantities to be paid for will be, as per plans/bid schedule, and either by the weight in tons in surface dry natural state; by railroad scales, truck scales, or barge displacement, or by square yards (according to plan thickness.) The Contractor shall determine the weights as follows:
1. Railroad Weights: The Contractor shall weight railroad cars on railroad scales, before and after loading or before and after unloading. If weighed by other than the Engineer, a certified statement of weights will be required. Certificates of weight, furnished by the railroad company, will be accepted without further certification.
 2. Truck Weights: The Contractor shall weigh trucks on certified scales, loaded and empty, as prescribed above for railroad weights. The Contractor shall weigh trucks in presence of the Engineer, or furnish certificates of weights.
 3. Barge Displacement: The Engineer will measure each barge. The Contractor shall fit each barge with gauges graduated in tenths of a foot increment. The Contractor shall locate a gauge at each corner of the barge near the lower end of the rake. The Contractor shall furnish additional gauges amidships, if the Engineer deems necessary. The Engineer will review and check all computed weights. Weight certificates may be submitted.
 4. In Place Measurement: The Contractor shall measure surface area (in square yards) of area riprap has been placed.

4.2 BASIS OF PAYMENT

- A. Sand-Cement: Price and payment will be full compensation for all work specified in this Section, including all materials, labor, hauling, excavation, and backfill. The Contractor shall include the cost of dressing and shaping the existing fills (or subgrade) for placing riprap in the Contract unit price for Riprap (Sand-Cement.)
- B. Stone/Rubble: Price and payment will be full compensation for all work specified in this Section, including all materials, hauling, excavation, and backfill. The Contractor shall include the cost of dressing and shaping the existing fill (or subgrade) for placing riprap in the Contract unit price for Riprap (Stone/Rubble). As an exception to the above, concrete that is shown to be removed from the project site and subsequently disposed of by being crushed and used in the embankment as riprap will not be paid for under this section. Include the cost of such work order under Removal of Existing Structures.
- C. Bedding Stone: Price and payment will be full compensation for all work specified in this Section, including all materials and hauling. The Contractor shall include the cost of dressing and shaping the existing fills (or subgrade) for placing bedding stone in the Contract unit price for Riprap (Stone/Rubble).

END OF SECTION 02340

SECTION 02400 - GRADED AGGREGATE BASE

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of the contract, including General and Supplementary Conditions and other Specification Sections, apply to the work of this section.

1.2 DESCRIPTION OF WORK

This item shall consist of a base course of graded aggregate constructed on a subgrade prepared in accordance with the specifications and in conformity with the line, grades and typical cross-section as shown on the drawings.

PART 2 - PRODUCTS

2.1 MATERIALS

Use graded aggregate material which yields a satisfactory mixture meeting all the requirements of these Specifications after it has been crushed and processed as a part of the mining operations.

The Contractor may furnish the material in two sizes of such gradation that, when combined in a central mix plant pugmill, the resultant mixture meets the required specifications.

Use graded aggregate base material of uniform quality throughout, substantially free from organic matter, shale, lumps and clay balls, and having a Li merock Bearing Ratio value of not less than 98. Use material retained on the No. 10 sieve composed of aggregate meeting the following requirements:

Soundness Loss, Sodium, Sulfate: AASHTO T 104.....15%

Percent Wear: AASHTO T 96 (Grading A)

Group 1 Aggregates..... 45%

Group 2 Aggregates..... 65%

Group 1 : This group of aggregates is composed of limestone, marble, or dolomite.

Group 2: This group of aggregates is composed of granite, gneiss, or quartzite.

Use graded aggregate base material meeting the following gradation:

Sieve Size	Percent by Weight Passing
2 inch	100
1 1/2 inch	95 to 100
3/4 inch	65 to 90
3/8 inch	45 to 75
No. 4	35 to 60

No. 10	25 to 45
No. 50	5 to 25
No. 200	0 to 10

For Group 1 aggregates, ensure that the fraction passing the No. 40 sieve has a Plasticity Index (AASHTO T 90) of not more than 4.0 and a Liquid Limit (AASHTO T 89) of not more than 25, and contains not more than 67% of the weight passing the No. 200 sieve.

For Group 2 aggregates, ensure that the material passing the No. 10 sieve has a sand equivalent (AASHTO T 176) value of not less than 28.

The Contractor may use graded aggregate of either Group 1 or Group 2, but only use one group on any Contract. (Graded aggregate may be referred to hereinafter as "aggregate".)

2.2 EQUIPMENT

The aggregate shall be spread by mechanical rock spreaders, equipped with a device which strikes off the aggregate uniformly to laying thickness, and capable of producing an even distribution of the aggregate. For crossovers, intersections and ramp areas; for roadway widths of 20 feet or less; for the main roadway area when forms are used and for any other areas where the use of a mechanical spreader is not practicable; spreading may be done by bulldozers or blade graders. All equipment for proper construction of this project shall be in first-class working condition.

PART 3 - EXECUTION

3.1 TRANSPORTING GRADED AGGREGATE

The graded aggregate shall be transported to the point where it is to be used, over aggregate previously placed if practical, and dumped on the end of the preceding spread. Hauling over the subgrade and dumping on the subgrade will be permitted when, in the County's opinion, these operations will not be detrimental to the subgrade.

3.2 SPREADING GRADED AGGREGATE

- A. Method of Spreading: The graded aggregate shall be spread uniformly. All segregated areas of fine or coarse aggregate shall be removed and replaced with properly graded aggregate.
- B. Number of Courses: When the specified compacted thickness of the base is greater than six inches, the base shall be constructed in two courses. The thickness of the first course shall be approximately one-half the total thickness of the finished base, or enough additional material added to bear the weight of the construction equipment without disturbing the

subgrade. When compacted thickness is six inches or less, graded aggregate shall be placed in one lift.

3.3 COMPACTING AND FINISHING BASE

- A. Single-Course Base: For single-course base, after the spreading is completed, the entire surface shall be scarified and then shaped so as to produce the required grade and cross-section, free of scabs and laminations, after compaction.
- B. Multiple-Course Base: For multiple-course base, the first course shall be cleaned of foreign material and bladed and brought to a surface cross-section approximately parallel to that of the finished base. Prior to the spreading of any material for the upper course, the density tests for the lower course shall be made, and the County shall have proof that the required compaction has been obtained. After the spreading of the material for the second course is completed, its surface shall be finished and shaped so as to produce the required grade and cross-section after compaction, and free of scabs and laminations.
- C. Moisture Content: When the material does not have the proper moisture content to ensure the required density, wetting or drying will be required. When water is added, it shall be uniformly mixed-in by disking to the full depth of the course which is being compacted. Water shall be added before beginning compaction operations. Wetting or drying operations shall involve manipulation, as a unit, of the entire width and depth of the course which is being compacted. This shall be performed utilizing the speedy moisture meter.

3.4 DENSITY REQUIREMENTS

As soon as proper conditions of moisture are attained, the material shall be compacted to a density of not less than 98% of the modified proctor maximum density as determined by AASHTO T-180 (Modified Proctor.)

3.5.1 TESTING SURFACE, PROTECTION, AND MAINTENANCE

- A. Density Tests: A minimum of at least one field density test on each course of compacted base shall be performed for every 500 square yards, or every 300 linear feet of road pavement, or as directed by the Engineer. Additional tests may be made if deemed necessary by the Engineer and/or County/CEI.
- B. During final compacting operations, if blading of any areas is necessary to obtain the true grade and cross-section, the compacting operations for such areas shall be completed prior to making the density tests on the finished base.

- C. Correction of Defects: Contamination of Base Material: If, at any time, the subgrade material should become mixed with the base course materials, the Contractor shall, without additional compensation, dig out and remove the mixture, reshape and compact the subgrade and replace the materials removed with clean base material, which shall be shaped and compacted as specified above.
- D. Cracks and Checks: If cracks or checks appear in the base, either before or after priming, which in the opinion of the County, would impair the structural efficiency of the base, the Contractor shall remove the cracks or checks by re-scarifying, reshaping, adding base material where necessary, and re-compacting, without additional compensation.
- E. Compaction of Widening Strips: Where base construction consists of widening strips and the trench width is not sufficient to permit use of standard base compaction equipment, compaction shall be accomplished by use of vibratory compactors, trench rollers, mechanical plate tampers, or other special equipment which will achieve the density requirements specified herein. When multiple-course base construction is required by the plans or specifications, the required compaction shall be achieved in each course prior to spreading material for the overlaying course.
- F. Testing Surface: The finished surface of the base course shall be checked from the required crown and ensure longitudinally a smooth, consistent surface for the placement of the asphalt course(s). All irregularities, greater than 1/4 inch per 15' straight edge test, shall be corrected, after which the entire area shall be re-compacted and tested as specified herein before. In the testing of the surface, the measurements will not be taken in small holes caused by individual pieces of rock having been pulled out by the grader.
- G. Priming and Maintaining:
- Priming: The prime coat shall be applied only when the base meets the specified density requirements and the moisture content in the top half of the base does not exceed 90% of the optimum moisture of the base material. At the time of priming, the base shall be firm, unyielding and in such condition that no undue distortion will occur. See FDOT Prime Coat Specification.
- Maintaining: The Contractor will be responsible for assuring that the true crown and template are maintained, with no rutting or other distortions, and that the base meets all the requirements, at the time the surface course is applied.
- H. Thickness Requirements:
- Measurements: Thickness of the base shall be measured at intervals in

such a manner that each test represents 500 square yards, or every 300 linear feet of road pavement, or as otherwise directed by the County. Measurements shall be taken at various points on the cross-section, through holes not less than three inches in diameter.

Areas Requiring Correction: Where the compacted base is deficient by more than ½ inch from the thickness called for in the plans, the Contractor shall correct such areas. The affected areas shall then be brought to the required state of compaction and to the required thickness and cross-section.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT:

The quantity to be paid for will be the area, in square yards, completed and accepted.

4.2 BASIS OF PAYMENT:

Price and payment will be full compensation for all work specified in this section, including dust abatement, correcting all defective surfaces and deficient thickness, removing cracks and checks, the additional aggregate required for such crack elimination, and the prime coat.

END OF SECTION 02400

SECTION 02410 – RECYCLED CONCRETE AGGREGATE BASE
(CRUSHED CONCRETE)

PART 1 – GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of contract, including General and Supplementary Conditions and other Specification Sections, apply to the work of this section.

1.2 The County recognizes the beneficial reuse of construction materials where said materials can be used in a manner that provides a construction product meeting specifications adopted by state and/or federal agencies. As such, the County shall allow, as noted below, the use of Recycled Concrete Aggregate (RCA) for the purpose of constructing an aggregate base course for the placement of an asphaltic concrete surface course in accordance with this section.

1.3 This section shall conform to section 2400 G.A.B. except as noted.

1.4 SPECIFIC CONSENT REQUIRED

Recycled Concrete Aggregate may only be used on projects with specific written consent of the County Engineer, or designee, subject to the conditions outlined herein. No such permission shall be given until the requirements of Items 2.1 through 3.1.A-D have been affirmatively addressed by the Engineer of Record for the project. Additionally, construction may not proceed until the requirements of Item 3.2.B have been addressed. Should construction commence more than six months after the date of the geotechnical report and/or roadway locations and/or elevations change, a geotechnical addendum shall be submitted confirming groundwater elevations. Should County staff observe differing construction or material conditions after approval; additional testing or re-evaluation of existing conditions for use may be required. RCA installations not in compliance with these specifications shall be subject to removal and replacement.

PART 2 - PRODUCTS

2.1 MATERIALS

The material requirements of Recycled Concrete Aggregate shall be in accordance with the Florida Department of Transportation Special Provisions Specification Section 204, "Graded Aggregate Base."

- A. Recycled Concrete Aggregate used shall not be required to comply with FDEP source approval requirements specified in FAC 62-701.730 or be qualified as a clean debris source under FDEP rules, as outlined in Florida Department of Transportation Special Provisions Specification Section 204-2.2, "Graded Aggregate Base." The aggregate supplied shall be

capable of meeting the requirements outlined in this item. No material shall be used that has been obtained from potentially contaminated sources that may contain asbestos or other hazardous materials.

PART 3 - EXECUTION

3.1 SPECIFIC USE & LOCATION REQUIREMENTS

- A. The use of Recycled Concrete Aggregate shall be in accordance with the Florida Department of Transportation *Special Provisions Specification Section 204, "Graded Aggregate Base."* It should be noted RCA may not be used on FHWA funded projects.
- B. Unless the Engineer of Record can demonstrate that the grading plan provides a minimum vertical separation of 18 inches between the bottom of the base and the seasonal high water table provided in the geotechnical report, permanent groundwater control measures shall be incorporated in the roadway design (i.e. under drains with positive outfalls, etc.). If the subsurface conditions encountered in the geotechnical study indicate the presence of, or the potential for, perched groundwater, the geotechnical engineer shall address the appropriate measures to remedy perched groundwater in the geotechnical report or addendum.
- C. The Engineer of Record shall as part of the original construction plans approved by the County or by addendum approved by the County provide construction details and specifications for the road section(s) to be used with RCA. The sections shall include material types, thickness requirements, and compaction requirements for all materials required for the flexible pavement section. The plan and profiles shall detail groundwater levels (apparent or perched) and the use of under drains with positive outfalls, where applicable/appropriate. Where under drains are required, they shall be detailed in cross section relative to the road section. The Engineer of Record shall also provide calculations which detail the calculated capacity of all components of the underdrain section, transmission pipes and receiving system.
- D. If, during the time period of construction, water is observed bleeding thru the pavement, new or additional permanent groundwater control measures to a positive, approved, outfalls shall be installed. Such measures shall be submitted to the County for review and approval per Item C above.

3.2 TESTING & CERTIFICATION REQUIREMENTS

- A. The installation of Recycled Concrete Aggregate shall be in accordance with the Florida Department of Transportation *Special Provisions Specification Section 204, "Graded Aggregate Base."*

- B. Upon delivery of RCA to the project site, the contractor, NOT the supplier, shall provide to the County a report from an independent FDOT certified testing laboratory certifying that the material meets the gradation and Limerock Bearing Ratio requirements of Florida Department of Transportation *Special Provisions Specification Section 204, "Graded Aggregate Base."*
- C. During the course of construction, quality control samples of RCA delivered to the project site shall be selected under the supervision of the County, or its representative, for gradation testing. Sampling shall be at a minimum frequency of one sample for every 3,600 square yards of in-place base material.
- D. All testing shall be in accordance with Florida Department of Transportation *Special Provisions Specification Section 204, "Graded Aggregate Base,"* and/or the County requirements, whichever is more stringent.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT:

The quantity to be paid for will be the area, in square yards, completed and accepted.

4.2 BASIS OF PAYMENT:

Price and payment will be full compensation for all work specified in this section, including dust abatement, correcting all defective surfaces and deficient thickness, removing cracks and checks, the additional aggregate required for such crack elimination, and the prime coat.

END OF SECTION 02410

SECTION 02440 – SUPERPAVE ASPHALT BASE

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of Contract, including General and Supplementary Conditions and other Specifications sections, apply to work of this section.

1.2 SUBMITTALS

Submit certification of compliance with applicable specifications in accordance with *Section 01300, "Submittals"*.

1.3 ENVIRONMENTAL CONDITIONS

Construct bituminous courses when underlying course is dry, and when atmospheric temperature is 40°F and rising for courses 1½" or greater, and 45°F and rising for courses less than 1½".

1.4 CONSTRUCTION EQUIPMENT

- A. Spreading Equipment: Self-propelled electronically controlled type, unless other equipment is authorized. Spreading equipment shall be capable of spreading hot bituminous mixtures without tearing, shoving, or gouging and to produce a finished surface of specified grade and smoothness. The use of a spreader that leaves indented areas or other objectionable irregularities in the fresh laid mix during operations will not be permitted.
- B. Rolling Equipment: Self-propelled pneumatic-tired rollers supplemented by three-wheel and tandem type steel wheel rollers. The number, type and weight of rollers shall be sufficient to compact the mixture to the required density without detrimentally affecting the compacted material. All rollers shall be suitable for rolling hot-mix bituminous pavements and capable of reversing without backlash. Pneumatic-tired rollers shall be capable of being operated both forward and backward without turning on the mat, and without loosening the surface being rolled. Equip rollers with suitable devices and apparatus to keep the rolling surfaces wet and prevent adherence of bituminous mixture.

At the Contractor's option, vibratory rollers especially designed for bituminous concrete compaction may be used, provided rollers do not impair stability of pavement structure and any underlying layers. Repair depressions in pavement surfaces resulting from use of vibratory rollers at no cost to the Owner. Rollers shall be self-propelled, single or dual vibrating drums, and steel drive wheels, as applicable; equipped with variable amplitude and separate controls for energy and propulsion.

- C. Hand Tampers: Hand tampers shall weigh not less than 25 pounds and have a tamping face of not more than 50 square inches.
- D. Mechanical Hand Tampers: Commercial type, operated by pneumatic pressure or by internal combustion.

PART 2 - PRODUCTS

2.1 MATERIALS

All materials shall conform to the requirements of these Specifications for the pavement sections as shown on the drawings.

PART 3 - EXECUTION

3.1 TRANSPORTATION OF BITUMINOUS MIXTURES

Deliver mixture to the area to be paved in such a manner that the temperature, at the time of dumping into the spreader, shall be not less than 285°F or greater than 345°F or that temperature required to obtain the specified compaction. Reject any load that has become wet prior to placing or falls outside of the above temperature ranges.

3.2 PLACING

Provide line and grade stakes as necessary for control. Place grade stakes in lanes parallel to centerline of area to be paved, and suitably space for string lines. Place and compact bituminous courses in such thicknesses as to achieve density and smoothness requirements. Maximum lift of bituminous base course shall not exceed 3 inches. Prior to laying the base course, clean underlying course of foreign and objectionable matter with power blowers, power brooms, or hand brooms in places inaccessible to power equipment, and inspect for compaction and smoothness requirements. The range of temperatures of the mixtures at the time of spreading shall be between 285°F and 345°F. Reject bituminous mixture having a temperature outside these limits when dumped into the hopper of the spreader. Adjust mechanical spreader and regulate speed so that the surface of the course is smooth, and when compacted conforms to depth, cross sections, grades and contours indicated. When irregularities of surface or deficiency in depth is more than specified tolerances, remove defective work and replace with new material. Whenever possible, place the mixture in strips not less than 10 feet wide. Overlap rolling to previously placed strip and extend to overlap first strip. Place mixture as continuously as possible. Shovelers and rakers shall follow spreading equipment, adding hot mixture and raking as required to produce a course that, when completed, shall conform to requirements specified. In areas where the use of machine spreading is impractical, mixture may be spread by hand. Distribute mixture into place from dump boards by means of hot shovels and spread with hot rakes in a uniformly

loose layer of such thickness that, when completed, it conforms to required grade and thickness. Do not dump loads any faster than they can be handled by shovelers and rakers. Paint contact surfaces of previously constructed curbs, manholes, and similar structures with a thin coat of emulsion or other approved bituminous material prior to placing the bituminous mixture.

3.3 COMPACTION OF MIXTURE

A. Affect compaction by rolling. Begin rolling as soon after placing as the mixture will bear the roller without undue displacement. Delays in rolling freshly spread mixture will not be tolerated. Start rolling longitudinally at extreme sides of lanes and proceed toward center of pavement, overlapping on successive trips by at least one-half the width of rear wheel of roller. Alternate trips of roller shall be slightly different lengths. Affect initial longitudinal rolling by the use of steel roller. Make tests for conformity with specified crown, grade and smoothness immediately after initial compression. Before continuing rolling, correct any variations by removing or adding materials, then roll course using pneumatic-tired rollers or tandem rollers, while mixture is hot and in condition suitable for proper compaction. Speed of rollers shall not exceed 3 miles per hour and at all times be slow enough to avoid displacement of hot mixture. Correct any displacement of mixture at once by use of rakes and apply fresh mixture or remove mixture as required. Continue rolling until all roller marks are eliminated. During rolling, moisten rollers to prevent adhesion of mixture to rolling surfaces, but do not permit an excess of water. Provide sufficient rollers for each spreading machine in operation on the job and to handle plant output. In places not accessible to rollers, compact mixture with hot pneumatic or manual hand tampers. Skin patching of an area that has been rolled is not permitted. Remove any mixture that becomes mixed with foreign material or is defective, replace with fresh mixture, and compact to density of surrounding area. Roller shall not pass over unprotected edge until asphalt has cooled to at least 120°F. Contractor shall provide workmen who are capable of performing work incidental to correction of pavement irregularities. After final rolling, permit no traffic of any kind on the pavement until the surface temperature has cooled to at least 120°F. Surface temperature shall be measured with surface thermometers or other satisfactory methods.

B. Testing Base Course:

1. Density: Within the entire limits of the width and depth of the base, obtain a minimum density in all areas of the roadway of 98% of modified Proctor maximum density as determined by AASHTO FM 1-T 180, Method D. Compact the base of any LOT of shoulder pavement to not less than 95% of the modified Proctor maximum density as determined by FM 1-T 180, Method D. Additional tests and cores may be required at the County's discretion.

2. Thickness: Measure thickness throughout the placement of any and all courses. In addition, perform periodic checks on the yield during the placement of any and all courses. The maximum allowable deficiency at any point shall not be more than 1/4 inch less than the indicated thickness for the course. The average thickness of the course shall be not less than the indicated thickness. Where the deficiency is more than the specified tolerances, the contractor shall correct each such representative area or areas by removing the pavement in question and replacing with new pavement.
3. Smoothness: Straightedge the compacted surface of the course, utilizing a 15' rolling straightedge, as deemed necessary by the County. Apply a rolling straightedge parallel with the centerline of the road and a non-rolling straightedge at right angles to the centerline of the road after final rolling. Unevenness of the course shall not vary more than plus or minus 3/16 inch in 15 feet. Correct any portion of the pavement showing irregularities greater than that specified.
4. Thicknesses and Density Requirements: The thickness and density shall be checked at intervals not to exceed one per 300 linear feet of roadway, but in any case, should not be less than three tests. Tests shall be staggered to ensure representative sampling.

PART 4 – MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

ASPHALT BASE COURSE:

The quantity to be paid for will be the area, in square yards, of asphalt base course after adjustment to the equivalent area of specified thickness.

4.2 BASIS OF PAYMENT

ASPHALT BASE COURSE:

Prices and payments will be full compensation for all work specified in this Section.

END OF SECTION 02440

SECTION 02460 - SAND-CLAY BASE

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of the Contract, including General and Supplementary Conditions apply to this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction* (FDOT Specs), Sections 240 and 912, Latest Edition.

1.2 DESCRIPTION OF WORK

This item shall consist of a base course composed of sand-clay mixtures constructed on a subgrade prepared in accordance with the specifications and in conformity with the line and grades shown on the drawings.

1.3 USE

Sand-clay base may only be used on projects with specific written consent from the County Engineer, or designee, subject to the conditions outlined herein.

Graded aggregate base shall be utilized in unsuitable soils, moisture sensitive areas, where groundwater fluctuates to within two feet of average grade, adjacent to wetlands/surface waters where the subgrade soils have a low permeability, and for roadways that will be dedicated to the County for maintenance.

PART 2 - PRODUCTS

2.1 MATERIALS

- A. All materials shall be secured from sources approved by the County and shall be furnished by the Contractor.
- B. Sand-Clay shall consist of natural or artificial mixtures of clay or soil binder and gravel, sand or other aggregates. The materials shall be free from organic matter and trash and shall not contain any aggregate particles that will not pass a one -inch (1") sieve. It shall be uniform and shall not contain lumps or aggregate in sufficient quantity to prevent securing a smooth surface free from pits or pockets.
- C. The material passing the 10-mesh sieve shall meet the following requirements:
 - 1. Clay (material smaller than 0.005mm) 8-21%

- 2. Silt (material from 0.05 to 0.005mm) 0-10%
 - 3. Combined Clay and Silt 8-25%
- D. It shall have a plasticity index of not more than 6 and have a liquid limit of not more than 25. The material shall have the minimum limerock bearing ratio (LBR) of 75.

PART 3 - EXECUTION

3.1 PLACING AND MIXING MATERIALS

- A. Sand-Clay Base; minimum 6 -inch compacted thickness. At least 98 percent of the modified proctor maximum dry density must be achieved throughout the full material thickness. The proposed sand-clay base material must be checked by a registered geotechnical engineer or by using hydrometer testing to determine clay content.
- B. The material may be dumped directly on the subgrade, but shall be uniformly distributed. The loose thickness will be checked continuously by the Contractor to insure that the finished base course will have the thickness and shape required by the typical section.

3.2 COMPACTING AND FINISHING SAND-CLAY BASE

- A. General: After spreading is completed, the base shall be compacted with water being added as required, until the required density has been obtained.
- B. Density Requirements: As soon as the proper condition of moisture is attained, the material shall be compacted to a density not less than 98 percent of maximum density as determined by modified proctor test ASTM 1557.
- C. Finishing: Upon completion of the initial compaction, the entire surface shall be scarified and then shaped to exact crown and cross-section. The base shall be re-watered, if needed, before final compaction. Final compaction shall be done with any type compacting equipment, in conjunction with traffic rollers, which will obtain the required density. Compaction shall continue until the required density has been obtained and until free water disappears from the surface.
- D. Correction of Defects: If at any time, the sub-grade material should become mixed with the base course material, the Contractor shall, without additional compensation, dig out and remove the mixture, reshape and compact the sub-grade and replace the materials removed with base material, which shall be watered, if needed, and rolled until the required

density is obtained.

- E. Priming: Allow base to cure until moisture does not exceed 90 percent of optimum moisture content for the base course material. Priming shall meet the requirements of Section 300 of FDOT *Standard Specifications, Latest Edition*.

3.3 TESTING SURFACE, PROTECTION AND MAINTENANCE

- A. Testing Surfaces: The finished surface of the base course shall be true to the grades shown on drawings. All irregularities greater than 3/8-inch shall be corrected by scarifying and removing or adding base material as may be required, after which the entire area shall be re-compacted to meet the specified density requirements.
- B. Thickness of Base: A three-eighths inch (3/8") under tolerance in the base will be allowed. All areas where the thickness of the completed base is less than the thickness required after such tolerance shall be corrected by scarifying, adding base material and re-compacting.
- C. Protection, Priming and Maintaining: The base shall be kept well drained at all times. Wherever ruts or low spots are found, the areas affected shall be brought to grade and, if necessary, shall be kept moist until the prime coat is applied, so as to prevent dusting and raveling.
- D. Thicknesses and Density Requirements: The thickness and density shall be checked at intervals not to exceed one per 300 linear feet of roadway. In no case shall less than three tests be performed. Tests shall be staggered to ensure representative sampling.
- E. Paved Areas: Make at least one field density test of each compacted fill layer (subgrade, base, etcetera) for every 300 linear feet of roadway or equivalent area, but in no case less than three tests. Tests shall be staggered to ensure representative sampling.

PART 4 – MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

Sand-clay Base to be paid for will be the plan quantity, in square yards.

4.2 BASIS OF PAYMENT

Price and Payment will be full compensation for all work specified in this Section, including all materials; all clearing and grubbing of material pits; all stripping of overburden from the pits, if required; all hauling of material, application of prime and all incidentals necessary to complete the work.

END OF SECTION 02460

SECTION 02500 – SUPERPAVE ASPHALT CONCRETE

PART 1 - GENERAL

1.1 GENERAL

- A. Construct a Type SP Asphalt pavement for local agencies using the type of mixture specified in the Contract, or when offered as alternates, as approved.
- B. For this Section only, all references to the Department shall mean the County. All references to the Engineer shall mean the Engineer of Record, designated Engineer of Escambia County and/or CEI.
- C. The County will accept the work based on one of the following methods as described in Part 5: 1) Certification, 2) Certification and process control testing by the Contractor, 3) acceptance testing by the County, or 4) other method(s) as determined by the Contract.

1.2 LAYER THICKNESSES

- A. Use only fine graded Type SP asphalt mixes. Fine graded mixes are defined as having a gradation that passes above the restricted zone when plotted on an FHWA 0.45 Power Gradation Chart.
- B. FINE MIXES: The allowable structural layer thicknesses for fine Type SP Asphalt Concrete mixtures are as follows:

Type SP 9.5	1-1 ½ inches
Type SP 12.5	1 ½ - 2 ½ inches
Type SP 19.0	2-3 inches

In addition to the minimum and maximum thickness requirements, the following restrictions are placed on fine mixes when used as a structural course:

Type SP 9.5 - Limited to the final (top) structural layer, one layer only

Type SP 12.5 - May not be used in the first layer of courses over 3 1/2 inches thick, nor in the first layer of courses over 2 3/4 inches thick on limited access facilities.

The thickness of the new pavement may be checked by core samples, as determined by the Engineer. The Contractor shall be required to correct any deficiency either by replacing the full thickness; or overlaying the area as directed by the Engineer. County inspection shall be performed and all base failures shall be corrected prior to asphalt installation.

Type SP 19.0 - May not be used in the final (top) structural layer.

C. ADDITIONAL REQUIREMENTS: The following requirements also apply to fine Type SP Asphalt Concrete mixtures:

1. A minimum 1 1/2 inch initial lift is required over an Asphalt Rubber Membrane Interlayer (ARMI).
2. When construction includes the paving of adjacent shoulders (5 feet wide or less), the layer thickness for the upper pavement layer and shoulder shall be the same and paved in a single pass, unless shown differently in the plans.
3. Use the minimum and maximum layer thicknesses as specified in 1.2 B above unless shown differently in the plans. On variable thickness overbuild layers, the minimum allowable thickness may be reduced by 1/2 inch, and the maximum allowable thickness may be increased 1/2 inch, unless shown differently in the plans.

PART 2 - PRODUCTS

2.1 GENERAL REQUIREMENTS

Meet the material requirements specified in FDOT Standard Specifications Division III. Specific references are as follows:

Superpave PG Asphalt Binder or Recycling Agent – Sections 916-1, 916-2
 Coarse Aggregate, Stone, Slag or Crushed Gravel – Section 901
 Fine Aggregate – Section 902

Aggregates utilized on Escambia County projects must be in accordance with FDOT Qualified Products List

2.2 GRADATION REQUIREMENTS

Combine the coarse and fine aggregate in proportions that will produce an asphalt mixture meeting all of the requirements defined in this Specification and conform to the gradation requirements at design as defined in Table 1 below. Aggregates from various sources may be combined.

Table 1 Aggregate Gradation Control Points (Gradation Design Ranges)						
Sieve Size	Type SP Asphalt Mixture (Percent Passing)					
	SP 9.5		SP 12.5		SP 19.0	
	Min.	Max.	Min.	Max.	Min.	Max.
1 inch	-	-	-	-	100	-

3/4 inch	-	-	100	-	90	100
1/2 inch	100	-	90	100	-	90
3/8 inch	90	100	-	90	-	-
No. 4	-	90	-	-	-	-
No. 8	32	67	28	58	23	49
No. 200	2	10	2	10	2	8
For additional information, refer to AASHTO M-323-04, Table 3						

2.3 RESTRICTED ZONE

The gradation identified in 2.2 shall pass above the restricted zone specified in Table 2 below.

Table 2 Aggregate Gradation Restricted Zone (Design Only)						
Sieve Size within Restricted Zone	Boundaries of Restricted Zone Type SP Asphalt Mixture (Percent Passing)					
	SP 9.5		SP 12.5		SP 19.0	
	Min.	Max.	Min.	Max.	Min.	Max.
No. 4	-	-	-	-	-	-
No. 8	47.2	47.2	39.1	39.1	34.6	34.6
No. 16	31.6	37.6	25.6	31.6	22.3	28.3
No. 30	23.5	27.5	19.1	23.1	16.7	20.7
For additional information, refer to AASHTO M-323-04, Table 4						

2.4 AGGREGATE CONSENSUS PROPERTIES

A. Meet the following consensus properties at design for the aggregate blend:

1. Coarse Aggregate Angularity: When tested in accordance with ASTM D 5821, meet the coarse aggregate angularity requirement defined in Table 3 below.

Table 3 Coarse Aggregate Angularity Criteria (Minimum Percent Fractured Faces)				
	Depth of Top of Pavement Layer From Surface			
	≤4 inches		>4 inches	
	1 or More Fractured Faces (%)	2 or More Fractured Faces (%)	1 or More Fractured Faces (%)	2 or More Fractured Faces (%)
	85	80	60	-
For additional information, refer to AASHTO M-323-04, Table 5				

2. Fine Aggregate Angularity: When tested in accordance with AASHTO T-304, meet the fine aggregate angularity requirement defined in Table 4 below.

Table 4 Fine Aggregate Angularity Criteria		
	Depth of Top of Pavement Layer From Surface	
	≤4 inches	>4 inches
	Minimum Uncompacted Void Content (%)	Minimum Uncompacted Void Content (%)
	45	40
For additional information, refer to AASHTO M-323-04, Table 5		

3. Flat and Elongated Particles: When tested in accordance with ASTM D 4791, use a ratio of maximum to minimum dimensions of 5:1 and do not exceed 10% as the maximum amount of flat and elongated particles.

2.5 USE OF RECLAIMED (MILLED) ASPHALT PAVEMENT

- A. General Requirements: Reclaimed Asphalt Pavement (RAP) may be used as a component material of the asphalt mixtures subject to the following:
 1. The Contractor assumes responsibility for the design of asphalt mixes which incorporate RAP as a component material.
 2. For design purposes, the Contractor assumes responsibility for establishing accurate specific gravity values for the RAP material. This may be accomplished by one of the following methods:
 - a. Calculation of the bulk specific gravity value based upon the effective specific gravity of the RAP, determined on the basis of the asphalt binder content and maximum specific gravity. The Engineer and/or Engineer of Record will approve the estimated asphalt binder absorption value used in the calculation.
 - b. Testing of the extracted aggregate obtained through a vacuum extraction or ignition oven extraction.
 3. The amount of RAP material used in the mix is not to exceed 50% by weight of total aggregate.
 4. Use a gizzly or grid over the RAP cold bin, in-line roller crusher, screen, or other suitable means to prevent oversized RAP material from showing up in the completed recycled mixture.

If oversized RAP material appears in the completed recycled mix, take the appropriate corrective action immediately. If the appropriate corrective actions are not taken immediately, plant operations should be stopped.

5. Provide stockpiled RAP material that is reasonably consistent in characteristics and contains no aggregate particles that are soft or conglomerates of fines.
 6. Provide RAP, having minimum average asphalt content of 4.0% by weight of total mix. The Engineer may sample the stockpile to verify that this requirement is met.
- B. Binder for Mixes with RAP: Select the appropriate binder based on the table below. The Engineer and/or Engineer of Record reserves the right to change binder type and grade at design based on the characteristics of the RAP binder, and reserves the right to make changes during production. Maintain the viscosity of the recycled mixture within the range of 4,000 to 12,000 poises. Obtain a sample of the mixture for the Engineer within the first 1,000 tons and at a frequency of approximately one per 4,000 tons of mix.

Binder Grade for Mixes Containing RAP	
% RAP	Asphalt Binder Grade
<20	PG 67-22
20-29	PG 64-22
≥ 30	Recycling Agent
Note: When a PG 76-22 Asphalt Binder is called for in the Contract, limit the amount of RAP material used in the mix to a maximum of 15%.	

PART 3 - GENERAL COMPOSITION OF MIXTURE

3.1 GENERAL

Compose the asphalt mixture using a combination of aggregate (coarse, fine or mixtures thereof), mineral filler, if required, and asphalt binder material. Size, grade and combine the aggregate fractions to meet the grading and physical properties of the approved mix design. Aggregates from various sources may be combined.

3.2 MIX DESIGN

- A. Design the Type SP asphalt mixture in accordance with AASHTO PP-28, except as noted herein, to meet the requirements of this Specification. Use only previously approved designs. Prior to the production of any Type SP asphalt mixture, submit the proposed mix design with supporting

test data indicating compliance with all Type SP asphalt mix design criteria.

The Engineer and/or Engineer of Record will consider any marked variations from original test data for a mix design or any evidence of inadequate field performance of a mix design as sufficient evidence that the properties of the mix design have changed, and the Engineer and/or Engineer of Record will no longer allow the use of the mix design.

1. Grading Requirements: Meet Gradation Design Ranges in PART 2.
2. Gyrotory Compaction: Compact the design mixture in accordance with AASHTO TP-4. Use the number of gyrations as defined in the table below.

Type SP Design Gyrotory Compactive Effort			
	N _{initial}	N _{design}	N _{maximum}
SP Mixes	7	75	115

3. Volumetric Criteria: Use an air void content of the mixture at design of 4.0% at the design number of gyrations (N_{design}). Meet the requirements of the table below.

Mixture Densification Criteria			
	% G _{mm}		
	N _{initial}	N _{design}	N _{maximum}
SP Mixes	≥ 89.0	96.0	≤ 98.0

4. VMA Criteria: Meet the requirements of the table below for Voids in the Mineral Aggregate (VMA) of the mixture at the design number of gyrations.

VMA Criteria	
Type Mix	Minimum VMA (%)
SP 9.5	15.0
SP 12.5	14.0
SP 19.0	13.0

5. VFA Criteria: Meet the requirements of the table below for voids filled with asphalt (VFA) of the mixture at the design number of gyrations.

VFA Criteria	
	Design VFA (%)
SP Mixes	65 - 75

6. Dust Proportion: Use an effective dust-to-binder ratio as defined in FDOT Section 334-3.2.5.
7. Moisture Susceptibility: Provide a mixture (4 inch specimens) having a retained tensile strength ratio of at least 0.80 and a minimum tensile strength (dry and unconditioned) of 100 psi.
8. Additional Information: In addition to the requirements listed above, provide the following information with each proposed mix design submitted for use:
 - a. The design number of gyrations (N_{design}).
 - b. The source and description of the materials to be used.
 - c. The FDOT source number product code of the aggregate components furnished from an FDOT approved source.
 - d. The gradation and proportions of the raw materials as intended to be combined in the paving mixture. The gradation of the component materials shall be representative of the material at the time of use. Compensate for any change in aggregate gradation in handling and processing as necessary.
 - e. A single percentage of the combined mineral aggregate passing each specified sieve. Degradation of the aggregate due to processing (particularly -No. 200 [-75 μm]) should be accounted for and identified for the applicable sieves.
 - f. The bulk specific gravity value for each individual aggregate (and RAP) component as identified in the FDOT aggregate control program.
 - g. A single percentage of asphalt binder by weight of total mix intended to be incorporated in the completed mixture, shown to the nearest 0.1%.
 - h. A target temperature at which the mixture is to be discharged from the plant and a target roadway temperature (per 30-6.3). Do not exceed a target temperature of 340°F for modified asphalts and 315°F for unmodified asphalts.
 - i. Evidence that the completed mixture conforms to all specified physical requirements.
 - j. The name, seal, and/or certification of the Mix Designer.

3.3 REVISION OF MIX DESIGN

During production, the Contractor may request a target value revision to a mix design, subject to: (1) the target change falls within the limits defined in the table below, (2) appropriate data exists demonstrating that the mix complies with production air voids specification criteria, and (3) the mixture gradation meets the basic gradation requirements defined in 2.2 and 2.3.

Limits for Potential Adjustments to Mix Design Target Values	
Characteristic	Limit from Original Mix Design
No. 8 sieve and Coarser	± 5.0%
No. 16 sieve	± 4.0%
No. 30 sieve	± 4.0%
No. 50 sieve	± 3.0%
No. 100 sieve	± 3.0%
No. 200 sieve	± 1.0%
Asphalt Binder Content (1)	± 0.3%

(1) Reductions to the asphalt binder content will not be permitted if the VMA during production is lower than 1.0% below the design criteria.

Submit all requests for revisions to mix designs, along with supporting documentation, to the Engineer. In order to expedite the revision process, the request for revision or discussions on the possibility of a revision may be made verbally, but must be followed up by a written request. The initial mix design will remain in effect until a change is authorized by the Engineer and/or Engineer of Record. In no case may the effective date of the revision be established earlier than the date of the first communication between the Contractor and the Engineer regarding the revision.

A new design mix will be required for any substitution of an aggregate product with a different aggregate code, unless approved by the Engineer and/or Engineer of Record.

3.4 PAVING EQUIPMENT

A. Mechanical Spreading and Screeding Equipment:

1. General: Provide mechanical spreading and screeding equipment of an approved type that is self-propelled and can be steered. Equip it with a receiving and distribution hopper and a mechanical screed. Use a mechanical screed capable of adjustment to regulate the depth of material spread and to produce the desired cross-section.
2. Automatic Screed Control: For all asphalt courses, placed with mechanical spreading and finishing equipment, equip the paving

machine with automatic longitudinal screed controls of either the skid type, traveling stringline type, or non-contact averaging ski type. Ensure that the length of the skid, traveling stringline, or non-contact averaging ski is at least 25 feet. On the final layer of base, overbuild, structural, and friction courses, use the joint matcher in lieu of the skid, traveling stringline, or non-contact averaging ski on all passes after the initial pass. Furnish a paving machine equipped with electronic transverse screed controls when required by the Contract Documents.

3. Inflation of Tires: When using paving machines equipped with pneumatic tires, the Engineer may require that the tires be ballasted.
4. Screed Width: Provide paving machines on full width lanes that have a screed width greater than 8 feet. Does not use extendable screed strike-off devices that do not provide preliminary compaction of the mat in place of fixed screed extensions. The Contractor may use a strike-off device on irregular areas that would normally be done by hand and on shoulders 4 feet or less in width. When using the strike-off device on shoulders in lieu of an adjustable screed extension, the Contractor must demonstrate the ability to obtain an acceptable texture, density, and thickness. When using an extendable screed device to extend the screed's width on the full width lane or shoulder by 24 inches or greater, an auger extension, paddle, or kicker device is required unless the Contractor provides written documentation from the manufacturer that these are not necessary.
5. Motor Graders: Provide two motor graders for spreading widening courses with prior approval from the Engineer only. Use motor graders that are rated at not less than 6 tons and are self-propelled and power-controlled. Mount them on smooth tread or rib-type tires (no lug types allowed) with a wheel base of at least 15 feet. Equip the front motor grader with a spreader box capable of spreading the mix at the required rate.
6. Rollers:
 - a. Steel-Wheeled Rollers: Provide compaction equipment capable of meeting the density requirements described in these Specifications. Provide a tandem steel-wheeled roller weighing a minimum of 8 tons for seal rolling, and for the final rolling, use a separate roller with a minimum weight of 8 tons. Variations from these requirements shall be approved by the Engineer.

- b. Traffic Rollers: Provide compaction equipment capable of meeting the density requirements described in these specifications. Provide a self-propelled, pneumatic-tired traffic roller equipped with at least seven smooth-tread, low pressure tires, equipped with pads or scrapers on each tire. Maintain the tire pressure between 50 and 55 psi or as specified by the manufacturer. Use rollers with a minimum weight of 6 tons. Do not use wobble-wheeled rollers. Variations from these requirements shall be approved by the Engineer.
 - c. Prevention of Adhesion: Do not allow the mixture to adhere to the wheels of any rollers. Do not use fuel oil or other petroleum distillates to prevent adhesion. Do not use any method which results in water being sprinkled directly onto the mixture.
- 7. Trucks: Transport the mixture in trucks of tight construction, which prevents the loss of material and the excessive loss of heat. Provide each truck with a tarpaulin or other waterproof cover mounted in such a manner that it can cover the entire load when required. When in place, overlap the waterproof cover on all sides so that it can be tied down.
 - 8. Coring Equipment: Furnish a suitable saw or drill for obtaining the required density cores.
 - 9. Hand Tools: Provide the necessary hand tools such as rakes, shovels, etc., and a suitable means for keeping them clean.

PART 4 - CONTRACTOR'S PROCESS CONTROL

4.1 GENERAL

- A. Personnel: Provide qualified personnel (certified technician) for sampling, testing (by certified lab), and/or sign-off by P.E., and inspection of materials and construction activities. Ensure that qualifications are maintained during the course of sampling, testing and inspection.

Construction operations that require a qualified technician must not begin until the Department verifies that the technician is on the CTQP (Construction Training Qualification Program) list of qualified technicians. The CTQP lists are subject to satisfactory results from periodic Independent Assurance evaluations.

- B. Calibration of the Gyratory Compactor: Calibrate the Gyratory Compactor in accordance with the manufacturer's recommendations prior to

producing the mixture for any project. Check the height calibration, the speed of rotation; ram pressure and angle of gyration.

- C. Plant Testing Requirements: During the initial production of a mix design, test mixture to ensure proper performance and provide results to the department.
- D. Roadway Testing Requirements: Areas that demonstrate concerns of the mix design quality or poor/improper compaction efforts may be subject to additional coring and testing as seen fit by the Engineer.
- E. Extraction Gradation Analysis: Sample the asphalt mixture at the plant and perform extraction test prior to asphalt being delivered to project. The percent asphalt binder content of the mixture will be determined in accordance with FM 5-563 (ignition oven). The gradation of the extracted mixture will be determined in accordance with FM 1-T 030. All test results will be shown to the nearest 0.01. All calculations will be carried to the nearest 0.001 and rounded to the nearest 0.01. All results shall be provided to the department prior to placement of asphalt on any project.

Run an extraction gradation analysis on the mixture at a minimum frequency of once per 1,000 tons or a maximum of four consecutive days of paving, whichever comes first.

The target gradation and asphalt content will be as shown on the mix design. Any changes in target will require a change in the mix design.

If the percentage of asphalt binder deviates from the optimum asphalt binder content by more than 0.55%, or the percentage passing any sieve falls outside the limits in the table below, immediately resample the mix and test to validate the previous test result, and if needed, make the necessary correction. If the results for two consecutive tests deviate from the optimum asphalt binder content by more than 0.55%, or exceed the limits in the table for any sieve, notify the Engineer and take immediate steps to identify and correct the problem, then resample the mix. If the results from this test deviate from the optimum asphalt binder content by more than 0.55%, or exceed the limits in the table for any sieve, stop plant operations until the problem has been corrected.

Tolerances for Quality Control Tests (Extraction Gradation Analysis)	
Size	Percent Passing
1 inch	7.0
3/4 inch	7.0
1/2 inch	7.0
3/8 inch	7.0
No. 4	7.0
No. 8	5.5
No. 16	5.0
No. 30	4.5
No. 50	4.5
No. 100	3.0
No. 200	2.0

- F. Volumetric Control: During production of the mix, monitor the volumetric properties of the Type SP asphalt mix with a Type SP Gyrotory Compactor to determine the air voids, VMA, VFA, and dust-to-effective asphalt binder ratio (dust proportion) at N_{design} .

Take appropriate corrective actions in order to maintain an air void content at N_{design} between 3.0 and 5.0% during production. When the air void content at N_{design} drops below 2.5 or exceeds 5.5%, stop plant operations until the appropriate corrective actions are made and the problem is resolved to the satisfaction of the Engineer and/or Engineer of Record. Evaluate any failing material in accordance with Part 6.

Determine the volumetric properties of the mixture at a minimum frequency of once per production day when the daily production is less than 1,000 tons. If the daily production exceeds 1,000 tons, monitor the volumetric properties two times per production day.

During normal production, volumetric properties of the mixture will not be required on days when mix production is less than 100 tons. However, when mix production is less than 100 tons per day on successive days, run the test when the accumulative tonnage on such days exceeds 100 tons.

Testing required for volumetric property determination includes AASHTO TP-4, FM 1-T 209, FM 5-563 and FM 1-T 030. Prior to testing samples in accordance with AASHTO TP-4 and FM 1-T 209, condition the test-sized sample for one hour at the compaction temperature in a covered container.

- G. Plant Calibration: At or before the start of mix production, perform an extraction gradation analysis of the mix to verify calibration of the plant.

The sample tested at the start of any project may be utilized for this requirement.

- H. Process Control of In-Place Compaction: Develop and implement a method to control the compaction of the pavement and ensure its compliance with the minimum specified density requirements. The department may require the use of a nuclear gauge to test areas suspected of not having proper compaction. Other density measuring devices may be used in lieu of the nuclear density gauge, provided that it is demonstrated to the satisfaction of the Engineer and/or Engineer of Record that the device can accurately measure the relative level of density in the pavement on a consistent basis.

PART 5 - ACCEPTANCE OF THE MIXTURE

5.1 GENERAL

The asphalt mixture will be accepted based on one of the following methods as determined by the Engineer and/or Contract Documents:

1. Certification by the Contractor
2. Certification and Process Control Testing by the Contractor
3. Acceptance testing by the Engineer
4. Other method(s) as determined by the Contract

5.2 CERTIFICATION BY THE CONTRACTOR

Submit a Notarized Certification of Specification Compliance letter on company letterhead to the Engineer that all material produced and placed on the project was in substantial compliance with these specifications.

5.3 CERTIFICATION AND PROCESS CONTROL TESTING BY THE CONTRACTOR

Submit a Notarized Certification of Specification Compliance letter on company letterhead to the Engineer that all material produced and placed on the project was in substantial compliance with these specifications, along with supporting test data documenting all process control testing. Utilize an Independent Laboratory as approved by the Engineer for the Process Control testing.

5.4 ACCEPTANCE TESTING BY THE ENGINEER

- A. Acceptance at the Plant:

1. The asphalt mixture will be accepted, with respect to gradation and asphalt binder content, based on the results from the start up test. However, any load or loads of mixture which, in the opinion of the Engineer and/or Engineer of Record, are unacceptable for reasons of excessive segregation, aggregates improperly coated, or of excessively high or low temperature will be rejected for use in the work.
2. Acceptance Procedures: Control all operations in the handling, preparation, and production of the asphalt mix so that the percent asphalt binder content and the percents passing the No. 8 and No. 200 sieves will meet the targets from the mix design within the tolerances shown in the table below.

Tolerances for Acceptance Tests	
Characteristic	Tolerance*
Asphalt Binder Content	±0.55%
Passing No. 8 Sieve	±5.50%
Passing No. 200 Sieve	±2.00%
*Tolerances for sample size of n=1.	

Calculations for the acceptance test results for asphalt binder content and gradation (percentages passing the No. 8 and No. 200 sieves) will be shown to the nearest 0.01. Calculations for arithmetic averages will be carried to the 0.001 and rounded to the nearest 0.01.

Payment will be based on the acceptance of the project by the Engineer.

B. Acceptance of the Roadway:

1. Density Control: The in-place density of any questionable section of a course of asphalt mix will be evaluated by the use of a nuclear gauge and/or by the testing of 6 inch diameter roadway cores.

The Engineer will not perform density testing on leveling courses, open-graded friction courses, or any course which does not show signs of poor/improper compaction efforts. In addition, density testing will not be performed on the following areas when they are less than 1,000 feet in length: crossovers, intersections, turning lanes, acceleration lanes or deceleration lanes. Compact these courses (with the exception of open-graded friction courses) in accordance with the appropriate rolling procedure as specified in these specifications or as approved by the Engineer.

2. Acceptance: The completed pavement will be accepted with respect to overall ride, overall appearance, and overall yield as determined by the Engineer or Engineer of Record.

Areas of question may be tested with a nuclear gauge or by the testing of the density of the cores, as determined by the engineer.

3. Additional Density Requirement: On shoulders with a width of 5 feet or less, Compact the pavement in accordance with the rolling procedure (equipment and pattern) as specified herein or as approved by the Engineer. Stop the production of the mix if the rolling procedure deviates from the approved procedure.
4. Surface Tolerance: The asphalt mixture will be accepted on the roadway with respect to surface tolerance by the use of a 15 ft rolling straight edge. The department will determine if the use of a straight edge test is warranted. Unevenness of the course shall not vary more than plus or minus 3/16 inch in 15 feet.

5.5 ADDITIONAL TESTS

The Department reserves the right to run any test at any time for informational purposes and for determining the effectiveness of the Contractor's quality control.

PART 6 - DISPOSITION OF FAILING MATERIAL

Any material that is represented by failing test results will be evaluated to determine if removal and replacement is necessary. Remove and replace any material, if required, at no cost to the Department. The evaluation will be conducted by the Engineer and/or Engineer of Record. If so directed, obtain an engineering analysis, as directed by the Engineer, by the independent laboratory (as approved by the Engineer) to determine if the material can (a) remain in place, for this case the appropriate pay factor will be applied, or (b) be removed and replaced at no cost to the Department. The analysis will be a signed and sealed report by a Professional Engineer licensed in the State of Florida.

PART 7 – MEASUREMENT/PAYMENT

7.1 METHOD OF MEASUREMENT

For the work specified under this Section the quantity to be paid for will be the in-place measurement of the area in square yards unless otherwise stated in the project plan details.

The bid price for the asphalt mix will include the cost of the liquid asphalt or the asphalt recycling agent. There will be no separate payment or unit price adjustment for the asphalt binder material in the asphalt mix.

7.2 BASIS OF PAYMENT

Price and payment will be full compensation for all the work specified under this section.

END OF SECTION 02500

SECTION 02510 – ASPHALT RUBBER MEMBRANE INTERLAYER (ARMI) CRACK RELIEF

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawing and general provisions of Contract, including General and Supplementary Conditions and other Specification Sections, apply to work of this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction*, **Section 440**, and *Design Standards, Latest Edition*.

1.2 DESCRIPTION OF WORK

- A. Construct an asphalt rubber membrane interlayer composed of a separate application of asphalt rubber binder covered with a single application of aggregate.

PART 2 - MATERIALS

- A. Asphalt Rubber Binder: **Use A RB-20**, or approved equal, meeting the requirements of FDOT *Standard Specification Section 336*.
- B. Cover Material: Use Size No. 6 stone, slag, or gravel meeting the requirements of FDOT *Standard Specification Section 901*.

PART 3 - EQUIPMENT

- A. Power Broom: Provide a power broom, for cleaning the existing pavement, capable of removing all loose material from the surface.
- B. Spreading Equipment: Provide a self-propelled aggregate spreader that can be adjusted to accurately apply the cover material at the specified rate and that spreads the material uniformly.
- C. Rollers: Provide self-propelled, pneumatic-tired traffic type rollers equipped with at least 7 smooth tread, low-pressure tires, and capable of carrying a gross load of at least 8 tons. Maintain a minimum tire inflation pressure of 90 psi, or as specified by the manufacturer, such that the air pressure in no two tires varies more than 5 psi. Load the traffic roller as directed by the Engineer.
- D. Mixing Equipment: Use mixing equipment for asphalt rubber binder designed for that purpose and capable of producing and maintaining a

homogeneous mixture of rubber and asphalt cement at the specified temperature.

- E. Pressure Distributor: Use a pressure type distributor to apply asphalt rubber binder capable of maintaining a homogeneous mixture of rubber and asphalt cement at the specified temperature and consistently apply the material in a uniform manner.

PART 4 - CONTRACTOR'S QUALITY CONTROL (QC) PLAN

Provide the necessary quality control of the asphalt rubber binder and construction in accordance with the Contract requirements. Provide in the QC Plan procedures for monitoring and controlling of rate of application. If the rate of application varies by more than 5% from the rate set by the County, in accordance with 4.6 herein, immediately make all corrections necessary to bring the spread rate into the acceptable range. The County may take additional measurements at any time. The County will randomly check the Contractor's measurement to verify the spread rate.

PART 5 - PREPARATION OF ASPHALT RUBBER BINDER

Combine the materials as rapidly as possible for such a time and at such a temperature that the consistency of the binder approaches that of a semi-fluid material. Use the time and temperature for blending of the asphalt rubber binder as specified in FDOT Standard Specifications Table 336-1. The manufacturer must ensure the material has reached application consistency. If not, the manufacturer will determine if an extender oil or diluents is needed. After reaching the proper consistency, proceed with application immediately. Never hold the mixture at temperatures over 350° F for more than six hours after reaching that temperature.

PART 6 - CONSTRUCTION PROCEDURE

- A. Preparation of Surface: Prior to application of the asphalt rubber binder, clean the existing pavement as specified in FDOT Standard Specifications 300-5.
- B. Application of Asphalt Rubber Binder: Apply the asphalt rubber binder only under the following conditions:
 - 1. The air temperature is above 50°F and rising.
 - 2. The pavement is absolutely dry.
 - 3. The wind conditions are such that cooling of the asphalt rubber binder will not be so rapid as to prevent good bonding of the aggregate.

Uniformly apply the asphalt rubber binder, at the rate of 0.6 to 0.8 gal/yd² or as directed by the manufacturer. Use an application rate based on the unit weight as shown in FDOT Standard Specifications, Table 336-1. For conversions to standard 60 °F, refer to FDOT Standard Specifications 300-9. Determine the rate of application after each application operation.

- C. Application of Cover Material: Immediately after application of the asphalt rubber binder, uniformly spread the cover material at a rate of 0.26 and 0.33 ft³/yd² or as directed by the County. Determine the application rate at the beginning of each day's production, and as needed to control the operation, a minimum of twice per day. Maintain an application rate such that the pavement is covered uniformly with aggregate, and is one aggregate layer thick. For the cover material, use aggregate that is reasonably free of any adherent coatings and that does not contain excessive moisture. Immediately after the application of cover material, check the surface to ensure a uniform distribution of cover material and a smooth surface.

Do not separate the application of the asphalt rubber binder and the application of the cover material by more than 300 feet, unless approved by the County.

- D. Rolling: In order to ensure maximum embedment of the aggregate, cover the entire width of the mat immediately by traffic rollers. For the first coverage, provide a minimum of three traffic rollers in order to accomplish simultaneous rolling in echelon of the entire width of the spread.

After initial rolling, immediately correct all portions of the completed surface that the County deems are ineffective (not properly covered by aggregates, fat spots, excessive free aggregate, etc.)

Following the first coverage, make additional coverages with traffic rollers as directed by the County.

- E. Traffic Control: For the normal sequence of construction operations, place the first course of asphalt concrete overlay over the membrane prior to opening to traffic.

PART 7 - UNACCEPTABLE ASPHALT RUBBER MEMBRANE INTERLAYER

If the asphalt rubber membrane interlayer is unacceptable due to incorrect blending, application rate, or not meeting the requirements of this Section, or damaged prior to placement of the asphalt concrete layer, remove and replace it as directed by the County, at no additional cost to the County. Do not apply excessive amounts of asphalt rubber binder.

PART 8 - PLACEMENT OF ASPHALT CONCRETE OVERLAY

Ensure that the thickness and temperature of the initial layer of asphalt concrete placed on top of the asphalt rubber membrane interlayer are such that the overlay bonds to the interlayer and the underlying layer without voids or excessive binder. Core the asphalt overlay as directed by the County to evaluate the binder and aggregate spread rates, as well as the effectiveness of the asphalt concrete overlay in producing a well-bonded interlayer.

PART 9 – MEASUREMENT/PAYMENT

9.1 METHOD OF MEASUREMENT

- A. Asphalt Rubber Membrane Interlayer: The quantity to be paid for will be plan quantity, in square yards, completed and accepted.
- B. Bituminous Material (Asphalt Rubber Binder-Interlayer): The quantity will be the volume, in gallons, determined as provided in FDOT Standard Specifications 300-8.

9.2 BASIS OF PAYMENT

- A. Asphalt Rubber Membrane Interlayer: Price and payment will be full compensation for all work specified in this Section, including furnishing cover materials, handling, spreading, rolling, bituminous material, and other incidental work necessary to complete this item.
- B. Bituminous Material (Asphalt Rubber Binder-Interlayer): Payment will be included in the price of the asphalt rubber membrane interlayer and will be full compensation for furnishing asphalt cement, ground tire rubber, blending and handling.
- C. Payment Items: Payment will be made determined by the square yards of in-place product accepted.

END OF SECTION 02510

SECTION 02580 – HOT IN-PLACE RECLAIMED ASPHALT AND RESURFACING

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of the contract, including general and supplementary conditions and other specification sections, apply to work of this section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction Section 327, Latest Edition.*

1.2 DESCRIPTION OF WORK

This work consists of rehabilitating the surface layer of the existing asphalt roadway to a depth of one inch and placing a layer of new Hot Mix Asphalt Concrete (HMAC) material over the rehabilitated surface. This will be accomplished with a specially designed machine in a simultaneous process of heating, scarifying, applying an asphalt rejuvenating agent (emulsifier), thoroughly re-mixing and reshaping the existing surface, and application of the final overlay. The overlay of the new HMAC placed over the rejuvenated layer of existing asphalt, shall be in compliance with the lines, grades, thickness and typical cross section established by the County.

The machine that heats, scarifies, rejuvenates, and remixes must also lay the recycled asphalt material, as well as lay the new HMAC material. The County will provide the list of streets and surface selected for this application. Additional pre-heaters may be required to achieve the specified depth, as directed by the County.

PART 2 - MATERIALS

- A. Asphalt Recycling Agent (Emulsifier): The County will approve the asphalt-recycling agent. The recycling agent used to restore the plasticity of the existing asphaltic pavement shall be an emulsified agent. The recycling agent shall meet the requirements of ECR 1 or equal. A manufacturer's certification shall be submitted to the County for the recycling agent.
- B. Hot Mix Asphaltic Concrete (HMAC): The delivery of the new HMAC will be coordinated with the HMAC supplier by the selected recycling contractor.

PART 3 - JOB CONDITIONS

- A. Weather limitation for this work shall be a minimum of 50° F and rising.

- B. Prior to the repaving operation, the pavement shall be cleaned so as to be reasonably free from sand, dirt and other deleterious substances that would affect the quality of the recycled mix. No separate payment shall be made for this requirement.
- C. Existing manholes, water valves boxes, junction boxes, etc. that do not conform to the finished pavement grades shall be adjusted to finish grade.
- D. The Contractor shall be responsible for protecting the areas adjacent to the work from damage. (Heat, etc.)

PART 4 - EQUIPMENT

- A. All tools, equipment, and machinery shall be maintained in satisfactory working condition and shall be subject to the approval of the County Engineer.
- B. Repaving Machine: The machine shall be an approved, self-contained, self propelled, automated unit that heats, scarifies (or mills), automatically applies recycling agent at a uniform rate, thoroughly mixes, redistributes and levels the existing asphalt to the specified depth, and lays the new HMA material overlay. The new HMA must be laid within 30 seconds after the scarification begins to ensure a hot monolithic bond with the recycled asphalt pavement. The machine shall also be capable of reworking the material around manholes and other obstacles; the machine shall be capable of adding and mixing the recycling agent evenly and shall be equipped with a leveling blade and screed for re-grading of the existing asphaltic concrete surface.
- C. Pre-heater: This unit shall be hooded to prevent damage to adjacent property, including trees shrubs and landscaping. The heating hood shall be capable of heating the pavement surface to a minimum temperature of 225 degrees F, not to exceed 325 degrees F. This will allow for scarification to the required depth without breaking the aggregate particles or charring the pavement surface.
- D. Scarifying and Milling units shall be automatically controlled units in order to control the depth of penetration and to clear utility manholes and other obstructions. The depth of scarification shall be directed by the County. Note: Scarifying depth may vary in range from 3/4 of one inch to 1 1/2 inches.
- E. Recycling Agent Applicator: This system shall be automatically controlled; the recycling agent must be applied to the scarified material at a uniform rate. The application rate shall be synchronized with the machine's forward speed to maintain a tolerance within $\pm 5\%$ of the specified rate.
- F. Receiving hopper and Conveying System: The machine shall consist of a

hopper and conveyor system to collect and transport the new HMAC to the finishing unit without segregation of the new material.

- G. Recycling Unit: The machine shall consist of a system that mixes and redistributes and levels the scarified material over the width being processed to produce a uniform cross-section of recycled material. The recycling screed shall be heated and have crown control, and be capable of redistributing the recycled material to the desired longitudinal grade and transverse cross section.
- H. Finishing unit: The machine shall have an automatic controlled screed to produce a surface conforming to the surface thickness as required by the County. The thickness of the surface course lift shall not exceed 2 inches. This unit shall be capable of applying the new HMAC to a uniform longitudinal profile and cross slope of 1/4 inch per foot. The finishing screed must be heated and capable of electronically controlling the cross slope, and applying the new HMAC to produce a uniform surface and texture.
- I. Rollers: Rolling equipment shall be of sufficient type and weight to compact the new HMAC and the recycled material to the required density as specified in Section 2500. Sufficient numbers of rollers (2 minimum) shall be furnished to keep up with the operation. All rolling should be completed before the temperature of the new HMAC drops below 190 F.

PART 5 - TRAFFIC CONTROL

- A. Pavement markings shall conform to the requirements of *Section 04040*.
- B. Maintenance of Traffic: Suitable methods shall be used by the contractor to protect the new asphalt surface from all types of vehicular traffic without damage. Opening to traffic does not constitute acceptance of work. Conform to requirements of *Section 04060*.
- C. The Contractor will maintain at least one-way traffic and shall provide effective Traffic Control at all times. Two-Lane traffic shall be maintained wherever possible.
- D. No interruption of access to property shall be made unless prior arrangements acceptable to the occupant or owner of the affected property have been made and approved by the County.
- E. Submit a Traffic Control Plan for approval in accordance with *Section 04060*.

PART 6 - EXECUTION

- A. Notify the County at least 48 hours prior to commencement of any paving operation.
- B. The heating units shall produce sufficient heat to soften the pavement uniformly without burning or charring the existing asphalt pavement.
- C. The process shall produce a welded, longitudinal joint, the standing edge of the adjoining asphalt pavement shall be fully heated to a width of at least 2 inches beyond the width to be scarified and recycled.
- D. Immediately following heating of the pavement, the existing surface shall be scarified (milled) to the specified depth. The machine shall have the capability of maintaining a recycled mat with a minimum temperature of 190°F and a maximum temperature of 225°F throughout the repaving operation.
- E. Due to the varying locations and properties of the existing asphalt pavement, the following adjustments may be made, if directed by the County.
 - 1. Depth of scarification may be varied to correct existing cross slopes and grades.
 - 2. Application rate for the recycling agent may be adjusted as necessary to maintain a uniform mixture.
 - 3. Spot leveling may be necessary.
 - 4. Variable Message Boards may be required. No additional compensation will be made for these traffic control devices after contract is awarded.

F. CLEANUP

The Contractor will keep the work site free from accumulations of waste material, rubbish and debris from the Contractor's performance of the scope of work resulting from the use of all tools, construction equipment, and machinery, and surplus materials, and will leave the site clean and ready for use. The Contractor will restore to their original condition those portions of the work site, such as staging and stockpile areas, not designed for alteration as contained in the Contract Documents. This will include returning the area to the proper grade and slope, as well as replacing sod, if so required by the County.

PART 7 - QUALITY CONTROL

The County has the option of testing to ensure the surface is in compliance with thickness, smoothness, etc. and meets requirements of the specifications as directed by the County and as outlined in *Section 2500*.

Contractor will assign a Quality Control (QC) Supervisor to the project. The QC Supervisor will work in conjunction with the County.

PART 8 - MEASUREMENT/PAYMENT

8.1 METHOD OF MEASUREMENT

- A. The accepted quantities of asphalt pavement surface recycled will be measured and paid by the square yard. Pay item, Hot In-Place Recycling, Square Yard.
- B. Asphalt recycling agent will be measured by the gallon, used in place, as determined by the County and the Contractor Supervisor. Pay item, Emulsifier, Gallon.
- C. New HMAC will be measured by the tons used in place. Pay item, HMAC, Tons.

8.2 BASIS OF PAYMENT

- A. Price and payment will be full compensation for all work specified in this Section.
- B. No separate payment for traffic control will be made.
- C. Spot leveling will be paid for by the measured square yards under the unit price for Hot-In-Place Recycling.

END OF SECTION 02580

SECTION 02600 - STORMWATER SYSTEM

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of Contract, including General and Supplementary Conditions and other Specification Sections, specifically 2300, 3300, and *Design Standard Indexes*, apply to this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Sections 425, 430 and 530, Latest Edition*.

1.2 SUMMARY

This Section includes stormwater system piping and appurtenances. All labor, material, equipment, appurtenances, services, and other work or costs necessary to construct the facilities and place them into operation shall be furnished by the Contractor.

1.3 SUBMITTALS

- A. General: Submit the following in accordance with Conditions of Contract.
- B. Shop drawings for drainage pipe, pre-cast concrete storm drainage manholes and catch basins, including frames, covers, and grates.
- C. Shop drawings for cast-in-place concrete or field-erected masonry storm drainage manholes and catch basins, including frames and covers.

1.4 QUALITY ASSURANCE

- A. Environmental Compliance: Comply with applicable portions of local, state, and federal environmental agency regulations pertaining to stormwater systems impacts.
- B. Utility Compliance: Comply with local utility regulations and standards pertaining to relocation, clearances, et cetera related to installation of stormwater systems.
- C. Quality control to adhere to QA/QL Plan.

1.5 PROJECT CONDITIONS

Site Information: Perform site inspection, research public utility records, and verify existing utility locations. Verify that stormwater system piping may be installed in compliance with design plans and referenced standards. Locate existing stormwater system piping and structures that are out of service and

closed as per 3.8 this section.

1.6 SEQUENCING AND SCHEDULING

- A. Notify the County Inspector as signed to the subdivision or project coordinator assigned to project prior to pouring backfilling or form work.
- B. Coordinate connection to existing private and public drainage system with Owner and/or County.
- C. Coordinate with adjacent utilities work.

PART 2 - PRODUCTS

2.1 MATERIALS

2.1.1 PIPE

Meet the following requirements of *FDOT Specifications, Latest Edition*:

Reinforced Concrete Pipe	Section 449
Round Rubber Gaskets	Section 942
Corrugated Steel Pipe & Pipe Arch	Section 943
Corrugated Aluminum Pipe & Pipe Arch	Section 945
Corrugated Polyethylene Pipe	Section 948
Polyvinyl Chloride (PVC)	Section 948

2.1.2 MANHOLES

- A. Precast Concrete Manholes: Per FDOT Standard Specification 425-5 and ASTM C 478, precast reinforced concrete, of depth indicated with provision for rubber gasket joints.
- B. Cast-in-Place Manholes: Per FDOT Standard Specification 425-5 Cast reinforced concrete of dimensions and with appurtenances indicated.
- C. Manhole Frames and Covers: Construct Per FDOT Standard Specification 425-3.2 and Standard Indexes. All units shall bear the lettering "STORM SEWER" cast into cover. All proposed substitutes must have equal or greater opening sizes and weights.

2.1.3 INLETS

- A. Precast Concrete Catch Basins Inlets: Construct per FDOT Standard Specification 425-5.
- B. Cast-in-Place Inlets: Construct per FDOT Standard Specification 425 to dimensions and with appurtenances indicated.

1. Bottom, Walls, and Top: Reinforced concrete.
 2. Channel and Bench: Concrete.
- C. Inlet Frames and Grates: Per FDOT Standard Specification 425-3.2 & Standard Indexes. All units shall bear the lettering "STORM SEWER" cast into cover.

2.1.4 END TREATMENT

General: Head wall, apron, and mitered ends, per FDOT Standard Specification 430-4.6.

2.2 CONCRETE AND REINFORCEMENT

- A. Concrete: Portland cement mix, 3,000 psi; shall be in accordance with Section 03300.
1. Cement: ASTM C 150, Type II.
 2. Fine Aggregate: ASTM C 33, sand.
 3. Coarse Aggregate: ASTM C 33, crushed gravel.
 4. Water: Potable.
- B. Reinforcement: Steel conforming to the following:
1. Fabric: ASTM A 185, welded wire fabric, plain.
 2. Reinforcement Bars: ASTM A 615, Grade 60, deformed.
- C. Forms:
1. Form Materials: Plywood, metal, metal-framed plywood, or other acceptable panel-type materials to provide full-depth, continuous, straight, smooth exposed surfaces without distortion or defects. Materials shall be of size and strength to resist movement during concrete placement and to retain horizontal and vertical alignment until removal.
 2. Form Release Agent: Provide commercial formulation form-release agent with a maximum of 350 mg/l volatile or organic compounds (VOCs) that will not bond with, stain, or adversely affect concrete surfaces and will not impair subsequent treatments of concrete surfaces. Release agent to be within allowable volatile limits according to applicable local, state and federal codes.

2.3 MASONRY

Materials for accessories shall be per FDOT Standard Specification 949. Mortar shall be one part Portland cement and three parts masonry sand to which shall

be added lime putty in the amount of 50% of the volume of cement. Special commercial mortar mixes may be used if approved by the Engineer. All masonry materials shall conform to the latest applicable ASTM specifications. Set all masonry units in full beds of mortar, with full joints and strike all joints flush. Masonry reinforcements shall be galvanized Dur-O-Wal, or approved equal, and shall be installed at every other bed joint. Hollow block shall be poured solid with re-bar as designed.

2.4 CURING MATERIALS

Conform to FDOT Standard Specification 520-8.

2.5 BEDDING STONE

Subbase or base materials meeting requirements of FDOT Standard Specification 530-2.3.

PART 3 - EXECUTION

3.1 EXCAVATIONS FOR MANHOLES, INLETS, AND PIPE

Excavations shall be sufficient enough to leave at least 12 inches in the clear between their outer surfaces and the embankment. Excavation for all structures shall be made to the dimensions and elevations indicated on the drawings. Where the excavation is made below the indicated elevations, the excavation shall be restored to the proper elevation with compacted suitable material without extra compensation.

3.2 PREPARATION OF FOUNDATION FOR BURIED STORMWATER SYSTEMS

- A. Grade trench bottom to provide a smooth, firm, stable, and rock-free foundation, throughout the length of the pipe.
- B. Remove unstable, soft, and unsuitable materials at the surface upon which pipes are to be laid, and backfill with bedding stone per FDOT Standard Specification 530-2.3 to indicated level.
- C. Shape bottom of trench to fit bottom of pipe. Fill unevenness with tamped sand backfill. Dig bell holes at each pipe joint to relieve the bells of all loads and to ensure continuous bearing of the pipe barrel on the foundation.

3.3 PIPE INSTALLATION

- A. Drawings (plans and details) indicate the general location and arrangement of the underground stormwater system piping. Location and arrangement of piping layout takes into account many design considerations. Install the piping as indicated, to the extent practical.

Deviations shall be approved by the County.

- B. Install piping beginning at low point of systems, true to grades and alignment indicated with unbroken continuity of invert. Place bell ends of piping facing upstream. When installing gaskets, seals, sleeves, and couplings, follow manufacturer's recommendations for use of lubricants, cements, and other installation requirements. Maintain swab or drag in line and pull past each joint as it is completed.

The pipe shall be carefully examined for defects and the inside cleaned. After placing pipe in the ditch, the ends shall be wiped free from all dirt, sand and foreign material. All pipe and joints shall be made, handled, and installed in strict accordance with the manufacturer's recommendations and instructions. Install pipe in accordance with FDOT Standard Specification 430.

- C. Install piping pitched down in direction of flow, at minimum slope per plans and in accordance with manufacturer's recommendations, specifications, and design plans.
- D. Boring: Install pipe under streets or other obstructions that cannot be disturbed, by boring, jacking, or a combination of both. These methods of installation are not allowed for newly paved roadways. Utility conduit should be installed prior to paving.
- E. All RCP joints shall be sock/filter wrapped prior to backfilling unless a manufacturer recommended coupling is used.
- F. Field repairs of pipeline shall be in strict accordance with manufacturer's recommendations and specifications.
- G. Only conventional concrete pipe shall be allowed under dedicated County roads.
- H. Pipe Cover: Cover shall be a minimum of 12", unless approved by the County.
- I. Pipe Size: Minimum Pipe size shall be 18" diameter or equivalent, unless approved by the County.

3.4 MANHOLES

- A. General: Install manholes complete with accessories as indicated. Form continuous concrete or split pipe section channel and benches between inlets and outlet. Set tops of frames and covers flush with finish surface where manholes occur in pavements. Elsewhere, set tops 3 inches above finished grade, unless otherwise indicated.

- B. Place precast concrete manhole sections as indicated, and install in accordance with ASTM C 891.
- C. Construct cast-in-place manholes as indicated.
- D. Provide rubber joint gasket complying with ASTM C 443 at joints of sections; or apply bituminous mastic coating at joints of sections.

3.5 INLETS

- A. Construct inlets to sizes and shapes indicated per FDOT Standard Specification 425-6, or as modified in the plans.
- B. Set frames and grates to elevations indicated.

3.6 OUTFALL STRUCTURES

- A. Pipe systems shall be utilized for primary outfall of retention/detention areas.
- B. Weirs and flumes will not be acceptable for use as primary pond outfall structures or to primarily route stormwater to retention/detention areas at the end of down-gradient roadways.

3.7 END TREATMENT

Construct End Treatment per FDOT Standard Specification 430-4.6.

3.8 STORMWATER SYSTEM BACKFILL

Place and compact backfill material in accordance with Section 02300 and FDOT specification 125-8.

3.9 CLOSING OUT-OF-SERVICE STORMWATER SYSTEMS

- A. Out-of-Service Piping: Close open ends of out of service underground piping that is indicated to remain in place. Provide sufficiently strong closures to withstand hydrostatic or earth pressure that may result after pipe ends have been closed and grout filled with non-shrink grout.
 - 1. Close open ends of concrete pipe or structures with not less than 8-inch-thick brick masonry bulkheads and grout fill.
 - 2. Close open ends of other piping with plastic plugs, or other acceptable methods suitable for size and type of material being closed. Wood plugs are not acceptable.
- B. Out-of-Service Structures: Remove structure and close open ends of the

remaining piping or remove top of structure down to not less than 3 feet below final grade; fill structure with stone, rubble, gravel, compacted dirt, or flowable fill to within 1 foot of top of structure remaining, and fill with concrete.

3.10 FIELD QUALITY CONTROL

- A. Refer to Section 03300 for Concrete Testing and 02300 for Earthwork Testing.
- B. Cleaning: Interior of piping and structures shall be cleared of dirt and other superfluous material as work progresses. Maintain swab or drag in piping and pull past each joint as it is completed.
 - 1. In large, accessible piping, brushes and brooms may be used for cleaning.
 - 2. Place plugs in ends of uncompleted pipe at end of day or whenever work stops.
 - 3. Flush piping between manholes, to remove collected debris.
- C. Interior Inspection: Inspect piping to determine whether line displacement or other damage has occurred.
 - 1. Make inspections after pipe between manholes has been installed, cleaned and approximately 2 feet of backfill is in place, and again at completion of project. Each section of pipe between structures is to show from either end on examination, a full circle of light. Each appurtenance to the system shall be of the specified size and form, to be neatly and substantially constructed, with the top set permanently to exact position and grade.
 - 2. If inspection indicates poor alignment, debris, displaced pipe, infiltration, or other defects, correct such defects and re-inspect. All repairs shown necessary by the inspections are to be made, broken, cracked, or punctured pipe replaced, all deposits removed and the pipe left true to line and grade as herein specified, or shown on the plans, entirely clean and free from abnormalities and ready for use at no additional expense to the County.
 - 3. All storm pipes will be subject to video camera inspection by County staff.
- D. Trench Backfill Around and Above Pipe:
 - 1. In each compacted backfill layer, perform density test as specified in Section 02300.

2. Other tests may be required at County's discretion.
- E. Clean Up: Before final inspection and acceptance, the Contractor shall clean ditches, shape shoulders and restore all disturbed areas, including street crossings, grass plots, to as good as condition as existed before work started. All trenches shall be leveled and loose material removed from pavement gutters, sidewalks, pipelines, and inlet sediment traps, employing hand labor, if necessary.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The quantities to be paid for will be (1) the number of inlets, manholes, end walls, mitered end sections, flared end sections, junction boxes, and yard drains, including fittings and appurtenances, completed and accepted; (2) length of pipe to the nearest foot of type specified; and (3) the number of structures of these types (including also valve boxes and monument boxes) satisfactorily adjusted.

4.2 BASIS OF PAYMENT

Price and payment will be full compensation for finishing all materials and completing all work described herein or shown in the plans, including all clearing and grubbing outside the limits of clearing and grubbing as shown in the plans, all excavation except the volume included in the measurement designated to be paid for under the items for the grading work on the project, all backfilling around the structures, the disposal of surplus material, and the furnishing and placing of all the gratings, frames, covers, and any other necessary fittings.

END OF SECTION 02600

SECTION 02800 - FENCING

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of the Contract, including General and Supplementary Conditions, apply to this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Section 550 and Design Standard, Index 802, Latest Editions*

1.2 SUMMARY

- A. This Section includes, but is not limited to, the following:
 - 1. Chain link fence
 - 2. Farm Fence
 - 3. Wood privacy fence
- B. Where existing fences are to be relocated, but existing materials are deteriorated or damaged, fencing shall be replaced in kind or as specified by the County.

1.3 PROJECT CONDITIONS

- A. Traffic: Conduct fencing operations to ensure minimum interference with roads, streets, walks, and other adjacent occupied or used facilities and to minimize disturbance of the activities of adjacent property owners. Do not close or obstruct streets, walks, or other occupied or used facilities without prior approval.
- B. Security: Do not leave any fence unfinished or incomplete which might allow the escape of livestock or household pets, access to a private/public pool or pond, etc without temporary measures in place during construction.

1.4 PRODUCT DELIVERY, STORAGE AND HANDLING

- A. Deliver material in manufacturer's original packaging with all tags and labels intact and legible.
- B. Handle and store material in such a manner as to avoid damage.

PART 2 - PRODUCTS

2.1 CHAIN LINK FENCING:

Chain link fence shall meet the requirements of FDOT Standard Index 802.

2.2 GATES

A. Swing Gates: Per FDOT Standard Index 802 as modified herein, construct of 1.625" o.d. steel pipe galvanized in accord with ASTM A-53 and weighing 2.27 pounds per lineal foot. Provide gates more than 8 feet wide with either intermediate members or diagonal truss rods. Provide gates less than 8 feet wide with truss rods or intermediate braces. Arrange latches for padlocking to provide accessibility from both sides of the gate. Where a double swing gate is called out, Construct Concrete Anchor rod Base 8" in diameter and 4" deep flush with top of ground. Opening in base for rod shall accommodate standard size in accordance with manufacturer and shall be PVC or galvanized steel pipe.

B. Slide Gates: shall be constructed per FDOT Index 803.

2.3 ACCESSORIES

Post Tops: pressed steel, or malleable iron. Where top rail is used, provide post tops to permit passage of top rail.

2.4 FARM FENCING

Farm Fencing shall meet the requirements of FDOT Index 801.

2.5 WOOD PRIVACY FENCE

A. Where existing fences are to be relocated, but existing materials are deteriorated or damaged, fencing shall be replaced in kind or as specified by the County.

B. Shall be constructed as per industry standard with proper clearance below fence so as not to impede stormwater flow.

PART 3 - EXECUTION

3.1 CHAIN LINK FENCING

Chain link fence shall meet the requirements of FDOT Standard Index 802.

A. Drill holes for post footings in firm, undisturbed or compacted soil.

B. Place concrete around posts in a continuous pour, tamp for

consolidation. Check each post for vertical and top alignment.

- C. Set Keepers, stops, sleeves and other accessories into concrete as required.
- D. Topping of the fence with barbed wire shall not be included unless specifically shown on the plans.

3.2 INSTALLATION

- A. Brace Assemblies: install braces so posts are plumb when diagonal rod is under proper tension.
- B. Tension Wire: install tension wires before stretching fabric and tie to each post with ties or clips.
- C. Fabric: pull fabric taut 2 inches above grade level and tie to posts, rails, and tension wires. Attach fabric to terminal or gateposts by a stretcher bar and clip to other framework so that fabric remains in tension after pulling force is released.
- D. Hinge gates to swing through 180 degrees from closed to open.

3.3 FARM FENCING

- A. General installation shall be in accordance with F DOT Index 801 as modified herein.
- B. Fence shall be installed with wire side to the private property side.
- C. Topping of the fence with barbed wire shall not be included unless existing farm fence includes barbed wire topping.

3.4 WOOD PRIVACY FENCING:

Shall be constructed as per industry standard with proper clearance below fence so as not to impede stormwater flow.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

A. GENERAL

The quantities to be paid for will be either the number of gates, the length of each type of fence, the number of corner post assemblies, constructed and accepted or the length of each type of fence with all other items necessary for construction as incidental. In addition, extra payment will be

made, for additional lengths of post approved by the County.

B. MEASUREMENT OF FENCE LENGTH

The length of fence to be paid for will be measured along the bottom of the fabric, out-to-out of end posts, in the completed and accepted fence. Measurement for Resetting Fence will be the actual length of existing fence reset, including gates when applicable.

C. CORNER POST ASSEMBLIES, PULL, AND END POST ASSEMBLIES

The number of corner post assemblies and of pull and end post assemblies to be paid for will be the number of such post assemblies constructed and accepted.

4.2 BASIS OF PAYMENT

A. BASIC ITEMS OF FENCING

The contract unit price will be full compensation for all work and materials necessary for the complete installation, including line posts, but not including the corner, end, and pull posts and the assemblies thereof.

B. ITEMS OF POST ASSEMBLIES

The Contract unit prices for the items of Corner Post Assemblies and Pull and End Post Assemblies will include the posts and the complete assemblies therewith for each such item. Approach posts and brace posts will be considered as part of the assembly of the corner, end, or pull post serves as a brace in more than one horizontal line.

C. PAYMENT RATES FOR EXTRA-LENGTH POSTS

For any length of posts in excess of the standard length for each particular type of post, approved by the Engineer as provided above, payment will be made for each foot in excess of the standard length at the percentage of the Contract unit price per foot for the item of Fencing, as shown in the following schedule:

Total Post Length	Steel and Aluminum Posts	Recycled Plastic & Timber Posts
Standard up to 14'	50%	60%
Between 14' – 20'	60%	80%
Over 20' *	*	*

*When the length of post exceeds 20 feet, the work of finishing and installing such posts and the costs incidental thereto will be paid for as unforeseeable through a change order.

The standard length of steel, recycled plastic and aluminum posts will be the required length as indicated in the plans for each type and case. The above provisions for extra length payment will apply to end, corner and pull posts.

The payment for additional length of post will include the cost of additional concrete to extend concrete bases, as applicable.

D. GATE PAYMENT

The quantities to be paid for will be full compensation for all labor, materials, posts and associated hardware for the complete installation of the type gate specified in the plans, and accepted by the County.

END OF SECTION 02800

SECTION 02900 - GRASSING

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of Contract, including General and Supplementary Conditions and other Specifications Sections apply to this Section.
- B. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction*, Section 570 and Section 981, *Latest Edition*

1.2 SUMMARY

Extent of grassing work is as specified or shown on the construction plans. Sodded areas disturbed during construction shall be re-sodded to match existing. Areas disturbed beyond specified construction areas shall be sodded, at no additional expense, either to match existing or as per County direction.

1.3 SUBMITTALS

See paragraph 1.9 A *Quality Control/Quality Assurance Submittals*, Section 1300.

1.4 DELIVERY AND STORAGE

- A. General: Seed, fertilizer, sod and other grassing materials shall be stored under cover and protected from damage which would make them unacceptable for use.
- B. Seed: All seed shall be labeled in accordance with U.S. Department of Agriculture Rules and Regulations under the Federal Seed Act in effect on the date of invitation for bids. All seed shall be furnished in sealed standard containers, unless exception is granted in writing. Seed, which has become wet, moldy, or otherwise damaged in transit or in storage, shall not be used.
- C. Fertilizer: Fertilizer shall be delivered to the site in the original, unopened containers, each bearing the manufacturer's guaranteed analysis. Any fertilizer, which becomes caked or otherwise damaged, making it unsuitable for use, shall not be used.
- D. Sod: Do not use sod which has been cut (stripped) for more than 48 hours. Stack all sod that is not planted 24 hours after cutting and maintain proper moist condition.

PART 2 - PRODUCTS

2.1 MATERIALS

- A. Lime: Lime shall be ground limestone (Dolomite) containing not less than 85 percent of total carbonates, and shall be ground to such a fineness that 50-percent will pass a 100 -mesh sieve and 90 -percent will pass a 20 - mesh sieve.
- B. Fertilizer: Apply fertilizer at the following rates:
 10-10-10 1000 lbs/acre=0.2 lbs/sq yd
 13-13-13 770 lbs/acre=0.16 lbs/sq yd
- C. Seed: Apply seed at the rate as specified:

GRASS SEEDING RATES (Lbs/Ac)								
TYPE OF SEED	ZONE I				ZONE II			
	COASTAL*		INLAND		COASTAL*		INLAND	
	Mar.- Nov.	Nov.- Mar.	Mar.- Nov.	Nov.- Mar.	Mar.- Nov.	Nov.- Mar.	Mar.- Nov.	Nov.- Mar.
PERMANENT GRASSES								
Unhulled Bermuda**		90		20		90		20
Hulled Bermuda**	60		15		60		15	
Bahia (Argentine or Pensacola)			180	180			180	180
QUICK GROWING GRASS								
Annual Rye Grass		90		90		90		90
TOTAL POUNDS PER ACRE	60	180	195	290	60	180	195	290
* Locations where salt sensitive plants may be adversely affected by high concentrations of salt in soils, water, or air. This may include seaside locations, low-lying areas subjected to periodic saltwater inundation from storms or high tides, or where salt intrusion into groundwater supply has occurred.								
** Bermuda shall not be used in areas adjacent to existing or proposed landscaping.								
NOTE: All seeding shall be performed meeting the requirements of Section 570 of the Standard Specifications								

Activities such as clearing, grading, and excavating that will disturb one or more acres of land require coverage under the Generic Permit for Stormwater Discharge from Large and Small Construction Activities from the Florida Department of Environmental Protection, and implementation

of appropriate pollution prevention measures to minimize erosion and sedimentation. Please refer to the National Pollutant Discharge Elimination System (NPDES) Permit.

- E. Mulch: The mulch material shall be dry straw or hay, consisting of oat, rye, or wheat straw, or of pangola, peanut, coastal Bermuda or Bahia grass, hay or compost; and shall be free from noxious weeds and plants. Any plant officially listed, as being noxious or undesirable by any Federal Agency, any agency of the State of Florida or any Local jurisdiction in which the project is being constructed shall not be used. Furnish to the engineer, prior to incorporation onto the project, a certification from the Florida Department of Agriculture and Consumer Services, Division of Plant Industry, stating that the Mulch materials are free of noxious weeds. Any such noxious plant or plant part found to be delivered shall be removed by the Contractor at his expense. Only undeteriorated mulch, which can readily be cut into the soil, shall be used. The "air-dry" weight (as defined by the Technical Association of the Pulp and Paper Industry, for wood cellulose) shall be marked on each package by the producer. Apply mulch at a rate of 2 ton/acre or 1 lb/sq yd.

- E. Sod: All sod shall be healthy Centipede Sod unless otherwise required. Sod shall be strongly rooted, free of weeds and undesirable grasses and capable of providing vigorous growth and development when planted. Sod shall match existing species where restoration is required as a result of the Contractor's work.

PART 3 - EXECUTION

3.1 REQUIREMENTS

All areas disturbed by the Contractor's operations, shall be grassed, unless otherwise noted.

3.2 PLANTING SEED

- A. Grading: Areas to be grassed shall be graded to remove depressions, undulations, and irregularities in the surface before grassing. Adhere to grades as shown on plans.

- B. Tillage: The area to be grassed shall be thoroughly tilled to a depth of four inches using a plow and disc harrow or rotary tilling machinery until a suitable bed has been prepared and no clods or clumps remain larger than 1½ inches in diameter. Remove sticks, roots, and rubbish.

- C. Applying Lime: The pH of the soil shall be determined. If the pH is below 5.0, sufficient lime shall be added to provide a pH between 5.5 and 6.5. The lime shall be thoroughly incorporated into the top three to four inches

of the soil. Lime and fertilizer may be applied in one operation.

- D. Applying Fertilizer: Fertilizer shall be applied in accordance with the rates specified in Part 2, and shall be thoroughly incorporated into the top three to four inches of soil before sod is installed. FDOT Section 982.
- E. Seed and Mulch: Apply in accordance with the rates specified in Part 2.
- F. Maintenance: Maintenance shall begin immediately following the last operation of grassing and continue until final acceptance. Maintenance shall include watering, mowing, replanting, and all other work necessary to produce a uniform stand of grass, all at the contractor's expense.

3.3 PLACING SOD

- A. Use Centipede sod (*Eremochloa ophiuroides*) unless otherwise required. The sod shall have a thick mat of roots (minimum 2") with enough adhering soil to assure growth. Apply sod within 48 hours of stripping. Protect sod against drying and breaking of rolled strips.
- B. Placement: Prepare the ground by loosening the soil. Place sod perpendicular to the slope. Place sod on the prepared soil to form a solid mass with tightly fitted joints. Ensure the butt ends and sides of sod strips do not overlap. The seam should have a flush tight transition from new to existing sod with no overlap. Stagger strips to avoid a continuous downhill seam. Tamp or roll lightly to ensure contact with subgrade. Tamp the outer edges of the sodded area to produce a smooth contour. Work sifted soil into minor cracks between pieces of sod; remove excess to avoid smothering of adjacent grass. Water sod thoroughly with a fine spray immediately after planting.
- C. Pinning: All sod placed on a slope steeper than 3:1 shall be pinned, at the top of the sod, at a rate listed in the table below:

Sod Size	Pins Required
Square Sod	2 pins per sod square
Mini Roll	3 pins per roll
Standard Rolls	1 pin per linear foot

- C. Watering: Keep sod continuously moist to a depth below the root zone for three weeks after placement. If there is no water available to the site, the Contractor shall provide the water. Do not water in excess of 1" (one inch) per square yard per week for establishment.
- D. Clean-Up: All excess soil, excess grass materials, stones, pallets and other wastes shall be removed from the site daily and not allowed to accumulate. All paved areas shall be kept clean at all times.

- E. Maintenance: Maintain sod by watering, fertilizing, weeding, mowing, trimming and other operations such as rolling, re-grading, and re-planting as required to establish a lawn free of eroded or bare areas and acceptable to the County. Where inspected work and materials do not comply with requirements, replace rejected work and continue maintenance until re-inspected by County and found to be acceptable. Remove rejected materials promptly from the project site. FDOT Section 570-4.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The quantities to be paid for will be for the following items, completed and accepted: square yards of seeding, square yards of seeding and mulching, and square yards of sodding.

4.2 BASIS OF PAYMENT

Prices and payments will be full compensation for all work and materials specified in this Section.

END OF SECTION 02900

SECTION 03300 – PORTLAND CEMENT CONCRETE

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of the Contract, including General and Supplementary Conditions and other Specification Sections, apply to this Section.
- B. Florida Department of Transportation (FDOT), *FDOT Material's Manual, Chapter 9.2, Volume II, FDOT Standard Specifications for Road and Bridge Construction, Section 346, 347, 350, 400, 522, & 925, Latest Edition.*

1.2 SUMMARY

This Section includes concrete work for the following:

1. Roadways
2. Parking lots
3. Curbs and gutters
4. Walkways
5. Pads
6. Flumes
7. Curb Ramps
8. Cast in Place Structures

1.3 SUBMITTALS

- A. Product data for proprietary materials and items, including reinforcement and forming accessories, admixtures, joint systems, curing compounds, dry-shake finish materials, and others if requested by the County.
- B. Design mixes for each class of concrete. Include revised mix proportions when characteristics of materials, project conditions, weather, test results, or other circumstances warrant adjustments.
- C. Material certificates in lieu of material laboratory test reports when permitted by the County. Material certificates shall be signed by manufacturer and Contractor certifying that each material item complies with or exceeds requirements. Provide certification from admixture manufacturers that chloride content complies with requirements.

1.4 PROJECT CONDITIONS

- A. Traffic Control: Comply with requirements of Escambia County Specification, Section 04060, "Maintenance of Traffic."

- B. Utilize flagmen, barricades, warning signs and warning lights as required, as shown on plans, or as directed by the County.

PART 2 - PRODUCTS

2.1 GENERAL REQUIREMENTS

- A. Concrete shall conform to requirements of FDOT Standard Specification, Sections 346, 347, & 522 for curbs, gutters, sidewalks, structures and miscellaneous concrete.
- B. Concrete for pavement shall conform to requirements of FDOT Standard Specification, Section 350.
- C. Curb Ramps shall conform to FDOT Standard Index 304.

2.2 REINFORCING MATERIALS

- A. Reinforcing Bars and Tie Bars: ASTM A 615, Grade 60, deformed.
- B. Welded Steel Wire Fabric: ASTM A 185.
 - 1. Furnish in flat sheets, not rolls.
- C. Deformed-Steel Welded Wire Fabric: ASTM A 497.
- D. Fabricated Bar Mats: Welded or clip-assembled steel bar mats, ASTM A 184. Use ASTM A 615, Grade 60 steel bars, unless otherwise indicated.
- E. Joint Dowel Bars: Plain steel bars, ASTM A 615, Grade 60. Cut bars true to length with ends square and free of burrs.
- F. Hook Bolts: ASTM A 307, Grade A bolts, internally and externally threaded. Design hook bolt joint assembly to hold coupling against pavement form and in position during concreting operations, and to permit removal without damage to concrete or hook bolt.
- G. Supports for Reinforcement: Chairs, spacers, dowel bar supports and other devices for spacing, supporting, and fastening reinforcing bars, welded wire fabric, and dowels in place. Use wire bar-type supports complying with CRSI specifications. Use supports with sand plates or horizontal runners where base material will not support chair legs.

2.3 CONCRETE MATERIALS

- A. Portland Cement: Type I, Type IP, Type IS, Type IP (MS), Type II, or Type III.

1. Use one brand of cement throughout Project.
 2. All concrete shall develop a 28-day compressive strength of 3000 psi for non-structural (NS). If any concrete should fail to meet the strength requirement the structure shall be removed as necessary to remove the defective concrete and shall then be rebuilt at the Contractor's expense.
- B. Fly Ash: ASTM C 618, Class C or Class F.
- C. Normal-Weight Aggregates: ASTM C 33, Class 4, and as follows. Provide aggregates from a single source.
1. Maximum Aggregate Size: 1-1/2 inches.
 2. Do not use fine or coarse aggregates that contain substances that cause spalling.
 3. Local aggregates not complying with ASTM C 33 that have been shown to produce concrete of adequate strength and durability by special tests or actual service may be used when acceptable to Engineer.
- D. Water: Potable.
- E. Fiber Reinforcement: Synthetic fibers engineered and designed for secondary reinforcement of concrete slabs, complying with ASTM C 1116, Type III.

2.4 ADMIXTURES

- A. Provide concrete admixtures that contain not more than 0.01 per cent chloride ions.
- B. Air-Entraining Admixture: ASTM C 260, certified by manufacturer to be compatible with other required admixtures.
- C. Water-Reducing Admixture: ASTM C 494, Type A.
- D. High-Range Water-Reducing Admixture: ASTM C 494, Type F or Type G.
- E. Water-Reducing and Accelerating Admixture: ASTM C 494, Type E.
- F. Water-Reducing and Retarding Admixture: ASTM C 494, Type D.

2.5 CONCRETE MIX

- A. Prepare design mixes for each type and strength of normal-weight concrete

per FDOT Standard Specification, Section 346-6.2 and FDOT Material's Manual, Chapter 9.2, Volume II. Use a qualified independent testing laboratory for preparing and reporting proposed mix designs. Do not use the Owner's field quality-control testing laboratory as the independent testing laboratory.

- B. Fiber Reinforcement: Add to mix at rate of 1.5 lb per cu. yd., unless manufacturer recommends otherwise.
- C. Adjustment to Concrete Mixes: Mix design adjustments may be requested by Contractor when characteristics of materials, project conditions, weather, test results, or other circumstances warrant.

2.6 CONCRETE MIXING

Ready-Mixed Concrete: Comply with requirements of FDOT Standard Specification, Section 346-7 and FDOT Material's Manual, Chapter 9.2, Volume II.

PART 3 - EXECUTION

3.1 SURFACE PREPARATION FOR CONCRETE PAVEMENT

- A. Proof-roll prepared base or subgrade surface to check for unstable areas and verify need for additional compaction. Do not begin concrete work until such conditions have been corrected and are ready to receive paving.
- B. Remove loose material from compacted subbase surface immediately before placing concrete.

3.2 EDGE FORMS AND SCREED CONSTRUCTION

- A. Set, brace, and secure edge forms, bulkheads, and intermediate screed guides to required lines, grades, and elevations. Install sufficient forms to allow continuous progress of work and so that forms can remain in place at least 24 hours after concrete placement.
- B. Check completed formwork and screeds for grade and alignment to following tolerances:
 - 1. Top of Forms: Not more than 1/8 inch in 10 feet.
 - 2. Vertical Face on Longitudinal Axis: Not more than 1/4 inch in 10 feet.
- C. Clean forms after each use and coat with form release agent as required ensuring separation from concrete without damage.

3.3 PLACING REINFORCEMENT

- A. General: Comply with Concrete Reinforcing Steel Institute's recommended practice for "Placing Reinforcing Bars" for placing and supporting reinforcement. Comply with FDOT Standard Specification, Section 350-7.
- B. Clean reinforcement of loose rust and mill scale, earth, ice, or other bond-reducing materials.
- C. Arrange, space, and securely tie bars and bar supports to hold reinforcement in position during concrete placement. Secure reinforcement against displacement by formwork, construction, or concrete placement operations. Locate and support reinforcing by metal chairs, runners, bolsters, spacers and hangers, as required. Set wire ties so ends are directed into concrete, not toward exposed concrete surfaces. Maintain minimum cover to reinforcement.
- D. Install welded wire fabric in lengths as long as practicable. Lap adjoining pieces at least one full mesh and lace splices with wire. Offset laps of adjoining widths to prevent continuous laps in either direction. Use of chairs is required. Welded wire fabric shall not be "pulled" to center of slab.
- E. Install fabricated bar mats in lengths as long as practicable. Handle units to keep them flat and free of distortions. Straighten bends, kinks, and other irregularities or replace units as required before placement. Set mats for a minimum 2-inch overlap to adjacent mats.

3.4 JOINTS

- A. General: Construct control (contraction) joints, construction, and isolation joints true to line with faces perpendicular to surface plane of concrete. Construct transverse joints at right angles to the centerline, unless indicated otherwise. When joining existing paving, place transverse joints to align with previously placed joints, unless indicated otherwise.
- B. Control (Contraction) Joints: Control joints are grooved, formed, or sawed into sidewalks, driveways and concrete pavements so that cracking will occur in these joints randomly. If not specified on drawings, intervals shall be not greater than 10 feet or less than 5 feet. Construct control joints for a depth equal to at least 1/4 of the concrete thickness, as follows:
 - 1. Tooled Joints: Form contraction joints in fresh concrete by grooving and finishing each edge of joint with a radiused jointer tool.
 - 2. Sawed Joints: Form contraction joints with power saws equipped with shatterproof abrasive or diamond-rimmed blades. Cut 1/8-inch-wide joints into hardened concrete when cutting action will not tear, abrade, spall or otherwise damage surface and before development of

random contraction cracks.

3. Inserts: Form contraction joints by inserting preformed plastic, hardboard, or fiberboard strips into fresh concrete until top surface of strip is flush with paving surface. Radius each joint edge with a jointer tool. Carefully remove strips or caps of two-piece assemblies after concrete has hardened. Clean groove of loose debris.
- C. Construction Joints: Set construction joints at side and end terminations of paving and at locations where paving operations are stopped for more than ½ hour, unless paving terminates at isolation joints.
1. Provide preformed galvanized steel or plastic keyway-section forms or bulkhead forms with keys, unless indicated otherwise. Embed keys at least 1-1/2 inches into concrete.
 2. Continue reinforcement across construction joints unless indicated otherwise.
- D. Expansion Joints: Form expansion joints of preformed joint filler strips abutting concrete curbs, catch basins, manholes, inlets, structures, walks, other fixed objects, and where indicated.
1. Locate expansion joints at intervals of 30 feet, unless indicated otherwise or directed by County.
 2. Extend joint fillers full width and depth of joint, not less than ½ inch or more than 1 inch below finished surface where joint sealant is indicated. Place top of joint filler flush with finished concrete surface when no joint sealant is required.
 3. Furnish joint fillers in one-piece lengths for full width being placed wherever possible. Where more than one length is required, lace or clip joint filler sections together.
 4. Protect top edge of joint filler during concrete placement with a metal, plastic, or other temporary preformed cap. Remove protective cap after concrete has been placed on both sides of joint.
- E. Filler and Sealants: Submit specifications to Engineer for approval.
- F. Install dowel bars and support assemblies at joints where indicated. Lubricate or asphalt-coat one half of dowel length to prevent concrete bonding to one side of joint.

3.5 CONCRETE PLACEMENT

- A. Comply with requirements of FDOT Standard Specification, Sections 350-8

and 400-7 for placing concrete.

- B. Deposit and spread concrete in a continuous operation between transverse joints. Do not push or drag concrete into place or use vibrators to move concrete into place. No concrete will be placed on concrete which has hardened sufficiently to cause the formation of seams or planes of weakness. Deposit concrete as nearly as practical to its final location to avoid segregation. When concrete placing is interrupted for more than ½ hour, place a construction joint.
- C. Use a bonding agent at locations where fresh concrete is placed against hardened or partially hardened concrete surfaces.
- D. Consolidate concrete by mechanical vibrating equipment supplemented by hand-spading, rodding, floating, or tamping. Use equipment and procedures to consolidate concrete complying with FDOT Standard Specification, Section 350-9.
- E. Screed paved surfaces with a straightedge and strike off. Use bull floats or darbies to form a smooth surface plane before excess moisture or bleed water appears on the surface. Do not further disturb concrete surfaces prior to beginning finishing operations.
- F. Place concrete in two operations; strike off initial pour for entire width of placement and to the required depth below finish surface. Lay welded wire fabric or fabricated bar mats immediately in final position. Place top layer of concrete, strike off, and screed. Remove and replace portions of bottom layer of concrete that have been placed more than 15 minutes without being covered by top layer or use bonding agent if acceptable to County.
- G. Curbs and Gutters: Shall be constructed in accordance with FDOT Specs. When automatic machine placement is used for curb and gutter placement, submit revised mix design and laboratory test results that meet or exceed requirements. Produce curbs and gutters to required cross section, lines, grades, finish, and jointing as specified for formed concrete. If results are not acceptable, remove and replace with formed concrete.
- H. Slip-Form Pavers: When automatic machine placement is used for paving, submit revised mix design and laboratory test results that meet or exceed requirements. Produce paving to required thickness, lines, grades, finish, and jointing as required for formed paving. Compact subgrade of sufficient width to prevent displacement of paver machine during operations.
- I. When adjoining pavement lanes are placed in separate pours, do not operate equipment on concrete until pavement has attained 85 percent of its 28-day compressive strength, or sufficient strength to carry loads without damage or injury. Maturity Method Testing, as outlined in FDOT Standard Specification, Section 353-10.2, should be used to determine concrete

strength.

- J. Cold-Weather Placement: Comply with provisions of FDOT Standard Specification, Sections 346-7.4 and 400-7.1.1. Protect concrete work from physical damage or reduced strength that could be caused by frost, freezing actions, or low temperatures.
- K. Hot-Weather Placement: Place concrete complying with FDOT Standard Specification, Sections 346-7.5 and 400-7.1.2, and as specified when hot weather conditions exist.

3.6 CONCRETE FINISHING

- A. Float Finish: Begin floating when bleed-water sheen has disappeared and the concrete surface has stiffened sufficiently to permit operations. Float surface with power-driven floats or by hand-floating if area is small or inaccessible to power units. Finish surfaces to true planes within a tolerance of 1/8 inch in 10 feet as determined by a 10-foot-long straightedge placed anywhere on the surface in any direction. Cut down high spots and fill low spots. Refloat surface immediately to a uniform granular texture.
 - 1. Medium-to-Fine-Textured Broom Finish: Draw a soft bristle broom across concrete surface perpendicular to line of traffic to provide a uniform fine line texture finish.
 - 2. Tine Finishes: Apply to curb cut ramps and other areas as noted on the drawings. Finish shall be applied by an approved hand method and shall consist of transverse grooves which are 0.03 to 0.12 inch in width and 0.10 to 0.15 inch in depth, spaced at approximately 1/2 inch center to center.
- B. Final Tooling: Tool edges of paving, gutters, curbs, and joints formed in fresh concrete with a jointing tool to the following radius. Repeat tooling of edges and joints after applying surface finishes. Eliminate tool marks on concrete surfaces. Radius: 1/2 inch.

3.7 CONCRETE PROTECTION AND CURING

General: Protect freshly placed concrete from premature drying and excessive cold or hot temperatures. Comply with the recommendations of FDOT Standard Specification, Sections 350-11 and 925.

3.8 QUALITY CONTROL TESTING

- A. A qualified, accredited testing and inspection laboratory, under the direction of a Professional Engineer, licensed in the State of Florida, shall sample materials, perform tests, and submit test reports during concrete placement as follows:

1. Sampling Fresh Concrete: ASTM C 172, except modified for slump to comply with ASTM C 94. All concrete should be sampled by ACI certified technicians.
 - a. Slump: ASTM C 143; one test at point of placement for each compressive-strength test but no less than one test for each day's pour of each type of concrete. Additional tests will be required when concrete consistency changes.
 - b. Air Content: ASTM C 231, pressure method; one test for each compressive-strength test but no less than one test for each day's pour of each type of air-entrained concrete.
 - c. Concrete Temperature: ASTM C 1064; one test hourly when air temperature is 40 deg F (4 deg C) and below and when 80 deg F (27 deg C) and above, and one test for each set of compressive-strength specimens.
 - d. Compression Test Specimens: ASTM C 31; one set of four standard cylinders for each compressive-strength test, unless directed otherwise. Mold and store cylinders for laboratory-cured test specimens except when field-cured test specimens are required.
 - e. Compressive-Strength Tests: ASTM C 39; one set for each day's pour of each concrete class, plus one set for each additional 50 cu. yd. Test one specimen at 7 days, two specimens at 28 days, and retain one specimen in reserve for earlier or later testing if required. Class I Concrete NS compression test specimens cylinders are not required, except as directed by County.
 - f. Contractor shall repair the area to the satisfaction of the Engineer where material was removed for testing purposes. Should any work or materials fail to meet the requirements set forth in the plans and specifications, contractor shall pay for retesting of same.
 2. Basis for acceptance of concrete will be per FDOT Standard Specification, Sections 346-8 through 346-11.
- B. Test results will be reported in writing to the County, within 24 hours of testing. Reports of compressive strength tests shall contain the Project identification name and number, date and location of concrete placement, name of concrete testing laboratory, concrete type and class, design compressive strength at 28 days, concrete mix proportions and materials, compressive breaking strength, and type of break for both 7-day and 28-day tests.

- C. Nondestructive Testing: Non-destructive test methods may be used with approval of the Engineer, but shall not be used as the sole basis for acceptance or rejection.
- D. Additional Tests: The testing laboratory will make additional tests of the concrete when test results indicate slump, air entrainment, concrete strengths, or other requirements have not been met, as directed by Engineer. Testing laboratory may conduct tests to determine adequacy of concrete by cored cylinders complying with ASTM C 42, or by other methods as directed.

3.9 REPAIRS AND PROTECTION

- A. Remove and replace concrete work that is broken, damaged, or defective, or does not meet the requirements of this Section.
- B. Drill test cores where directed by the County when necessary to determine magnitude of cracks or defective areas. Fill drilled core holes in satisfactory concrete areas with Portland cement concrete bonded to paving with epoxy adhesive.
- C. Protect concrete from damage. Exclude traffic from concrete pavement for at least 14 days after placement. When construction traffic is permitted, maintain concrete as clean as possible by removing surface stains and spillage of materials as they occur.
- D. Maintain concrete work free of stains, discoloration, dirt, and other foreign material. Sweep concrete paving not more than 2 days prior to date scheduled for Substantial Completion inspections.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The quantities to be paid for will be the plan quantity, in square yards, of Plain Cement Concrete Pavement, Reinforced Cement Concrete Pavement, square yards of sidewalk, and linear feet of curb and/or gutter.

4.2 JOINTS AND CRACKS

The Contractor shall include the cost for Cleaning and Sealing Joints in the cost of the newly constructed pavement for: (1) transverse and longitudinal joint construction for new pavement; and (2) abutting joints between existing pavement and new pavement.

For replacing joint seals and sealing random cracks in existing Portland cement concrete pavement, the quantity to be paid for will be as specified below:

- A. The length of pavement joint that has been satisfactorily cleaned and sealed in existing Portland cement concrete pavement, as determined by field measurement along the joints, will be paid for at the Contract unit price per foot for Cleaning and Resealing Joints.
- B. The length of random cracks in existing Portland cement concrete pavement that have been satisfactorily cut, cleaned, and sealed, as determined by field measurement along the joints, will be paid for at the Contract unit price per foot for Cleaning and Sealing Random Cracks.

4.3 BASIS OF PAYMENT

Prices and payment will be full compensation for all work specified in this Section, including any preparation of the subgrade not included in the work to be paid for under another Contract item; all transverse and longitudinal joint construction, including tie-bars and dowel bars; the furnishing of test specimens; repair of core holes; and all incidentals necessary to complete the work.

END OF SECTION 03300

SECTION 03310 – TIED CONCRETE BLOCK

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of the Contract, including General and Supplementary Conditions and other Specification Sections, apply to this Section.

1.2 SCOPE OF WORK

- A. Scope of Work The Contractor shall furnish all labor, materials, equipment, and incidentals required and perform all operations in connection with the installation of tied concrete erosion control mats in accordance with the lines, grades, design and dimensions shown on the Contract Drawings and as specified herein.

1.3 SUBMITTALS

- A. The Contractor shall submit to the Engineer all manufacturer's performance research results and calculations in support of the tied concrete block mat system. Calculations and shop drawings shall be provided by the manufacturer for the means and methods necessary to place the mats in accordance with PART 2. A and in accordance with the plans. The shop drawings shall indicate the size and location of mats and placement along with providing the details and how the mats are tied together and are to be moved.
- B. The Contractor shall furnish to the Engineer all manufacturers' specifications, literature, shop drawings for the installation of the mats, and any recommendations, if applicable, that are specifically related to this project.

PART 2 – PRODUCTS

2.1 PRODUCTS

- A. General Tied concrete block mats shall be manufactured or field fabricated from individual concrete blocks tied together with a high strength geogrid.

Each block shall be tapered, beveled and interlocked. Each block shall incorporate interlocking surfaces or connections that prevent lateral displacement of the blocks within the mats when they are lifted for placement.

- B. Tied Concrete Block Mat

- 1. Scope This specification covers concrete blocks for erosion control

mats used for stabilizing channels.

2. Materials C ementitious M aterials -Materials shall conform t o t he following applicable ASTM specifications:
 - a. Portland Cements -Specification C 150, for Portland Cement.
 - b. Blended Cements -Specification C 595, for Blended Hydraulic Cements.
 - c. Hydrated Lime Types -Specification C 207, for Hydrated Lime Types.
 - d. Pozzolans -Specification C 618, f or F ly A sh and R aw or Calcined Natural P ozzolans f or us e i n P ortland C ement Concrete.

Aggregates shall conform to the following ASTM specifications, except that grading requirements shall not necessarily apply:
Normal Weight -Specification C 33, for Concrete Aggregates.

3. Physical Requirements Durability. The manufacturer shall satisfy the purchaser by proven field performance that the concrete units have adequate durability even if they are to be subjected to a freeze-thaw environment.

TABLE 1. PHYSICAL REQUIREMENTS			
Compressive Strength Net Area Min. psi (mPa)		Water Absorption Max., lb/ft ³ (kg/m ³)	
Avg. of 3 units	Individual Unit	Avg. of 3 units	Individual Unit
4,000 (27.6)	3,500 (24.)	10 (160)	12 (192)

4. Visual Inspection All units shall be sound and free of defects that would interfere with the proper placing of the unit or impair the strength or permanence of the construction. Surface cracks incidental to the usual methods of manufacture, or surface chipping resulting from customary methods of handling in shipment and delivery, shall not be deemed grounds for rejection.
5. Sampling and Testing The purchaser or his authorized representative shall inspect the units upon delivery. Units missing more than 4 blocks per 80 square feet section shall be deemed grounds for rejection.
6. The tied concrete block mats shall have one or more of the following nominal characteristics: Minimum open area of 10%

The tied concrete block mats shall exhibit resistance to mild concentrations of acids, alkalis, and solvents.

Polypropylene Geogrid Revetment mat shall be constructed of high tenacity, low elongating, and continuous filament polypropylene fibers. Interlocking geogrid shall have the following physical characteristics:

Mass/Unit Area ASTM D-5261 7.0 oz/yd² 240 g/m²

Aperture Size Measured 1.6 x 1.6 inch 40 x 40 mm

Wide Width Tensile Strength

Machine Direction (MD) ASTM D-6637 2,055 lb/ft 30 k N/m Cross

Machine Direction (CMD) ASTM D-6637 2,055 lb/ft 30 k N/m

Elongation at Break ASTM D-6637 6 % 6 %

Tensile Strength @ 2%

Machine Direction (MD) ASTM D-6637 822 lb/ft 12 kN/m

Cross Machine Direction (CMD) ASTM D-6637 822 lb/ft 12 kN/m

Tensile Strength @ 5%

Machine Direction (MD) ASTM D-6637 1,640 lb/ft 24 k N/m Cross

Machine Direction (CMD) ASTM D-6637 1,640 lb/ft 24 kN/m

Tensile Modulus @ 2%

Machine Direction (MD) ASTM D-6637 41,100 lb/ft 600 kN/m Cross

Machine Direction (CMD) ASTM D-6637 41,100 lb/ft 600 kN/m

Tensile Modulus @ 5%

Machine Direction (MD) ASTM D-6637 32,900 lb/ft 480 kN/m Cross

Machine Direction (CMD) ASTM D-6637 32,900 lb/ft 480 kN/m

NOTE: Polypropylene geogrids shall be determined by the manufacturer.

Tied concrete block mats are packaged in rolls. These are packaged with high strength lifting straps for moving material into place with an excavator.

PART 3 – CONSTRUCTION

- A. Prior to placing the tied concrete block mats, prepare the sub grade as detailed on the plans. All subgrade surfaces prepared for placement of mats shall be smooth and free of all rocks, stones, sticks, roots, other protrusions, or debris of any kind.
- B. The prepared surface shall provide a firm unyielding foundation for the mats with no sharp or abrupt changes or breaks in the grade.
- C. Apply seed directly to the prepared soil prior to installation of the Tied Concrete Block Mat. Use seed per project specifications.
- D. Install mats to the line and grade shown on the plans and according to the manufacturer's installation guidelines.
- E. The manufacturer will provide technical assistance during the slope preparation and installation of the tied concrete block mats as needed.
- C. Clean forms after each use and coat with form release agent as required ensuring separation from concrete without damage.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The completed work as described shall be measured and paid for at the contract unit price per square yard.

4.2 BASIS OF PAYMENT

Prices and payment for Tied Concrete Block Material will be full compensation for all work (including but not limited to labor, equipment, and materials) specified in this Section, including any preparation of the Subgrade not included in the work to be paid for under another Contract item, and all incidentals necessary to complete the work.

END OF SECTION 03310

SECTION 04000 - TRAFFIC CONTROL SIGNS

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of the Contract, including General and Supplementary Conditions and other Specifications Sections, apply to work of this section.
- B. Unless otherwise specified on the work orders, plan sheets, or in other sections of this contract, all materials and work shall conform to the applicable requirements in the following document:
 - 1. USDOT, Federal Highway Administration, *Manual on Uniform Traffic Control Devices for Streets and Highways, Latest Edition.*
 - 2. USDOT, Federal Highway Administration, *Standard Alphabets for Highway Signs and Pavement Markings, Latest Edition.*
 - 3. Florida Department of Transportation, *Design Standards for Design, Construction, Maintenance and Utility Operations on the State Highway System, Latest Edition.*
 - 4. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, section 700, Latest Edition.*

1.2 DESCRIPTION OF WORK

The work under this section includes the fabrication and installation of standard and special traffic control signs (warning, regulatory, and guide). The Contractor shall furnish all labor, materials, tools, supplies, equipment, and machinery necessary to fully complete the work shown in the plans and in these specifications.

PART 2 - PRODUCTS

2.1 MATERIALS

All materials shall be new and of good quality unless otherwise specified. The Contractor, at his own expense and if requested by the County, shall furnish samples of material and/ or shall certify that the material meets all FDOT requirements. All material or work that has been rejected shall be remedied by the Contractor at his own expense and without delay. If the Contractor fails to promptly remove and/or dispose of rejected material and replace the same, the County may remove and replace the same and deduct the cost of the work from the contract amount.

If the Contractor chooses to use material other than specified herein, a sample of the material with supporting manufacturer's literature and specifications must be submitted to the County for prior approval.

PART 3 - EXECUTION

3.1 UTILITY SPOTS

All street name signs shall be fabricated and installed in accordance with the plans and related documents. Contractor shall contact Sunshine State One Call of Florida (811 or 800-432-4770) at least 48 hours prior to digging or driving posts.

3.2 SIGN INSTALLATION

- A. Signs shall be placed at the locations illustrated and/or specified in the plans or related documents. The soil around the posts shall be solidly tamped so that the sign will stand vertically.
- B. If a sign cannot be placed where indicated due to a conflict, the Contractor shall immediately notify the County for an alternate location.
- C. The date when each sign is installed shall be marked in permanent ink on the rear side of each sign.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The quantity to be paid for will be plan quantity, unless otherwise provided.

4.2 BASIS OF PAYMENT

Price and payment will constitute full compensation for all work specified in this section. Payment for all items relating to traffic control signs will be included in the lump sum Maintenance of Traffic pay item.

END OF SECTION 04000

SECTION 04020 - POST MOUNTED STREET NAME SIGNS

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of the Contract, including General and Supplementary Conditions and other Specifications Sections, apply to work of this section.
- B. Unless otherwise specified on the plan sheets or in other sections of this contract, all materials and work shall conform to the applicable requirements in the following document:
 - 1. USDOT, Federal Highway Administration *Manual on Uniform Traffic Control Devices for Streets and Highways, Latest Edition.*
 - 2. USDOT, Federal Highway Administration *Standard Alphabets for Highway Signs and Pavement Markings, Latest Edition.*
 - 3. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Section 700, Latest Edition.*
 - 4. FDOT *Design Standards for design, Construction, Maintenance, and utility operations on the State Highway System, Latest Edition.*
 - 5. Escambia County *Standard Details for Street Name Signs, Latest Edition.*

1.2 DESCRIPTION OF WORK

The work under this section includes the fabrication and installation of post mounted street name signs as shown or noted on plans. The Contractor shall furnish all labor, materials, tools, supplies, equipment, and machinery necessary to fully complete the work shown in the work order and in these specifications.

PART 2 - PRODUCTS

2.1 MATERIALS

All materials shall be new and of good quality unless otherwise specified. The Contractor, at his own expense, shall, if requested by the County, furnish samples of material and/or shall certify that the material meets all FDOT requirements. All material or work that has been rejected shall be remedied by the Contractor at his own expense and without delay. If the Contractor fails to promptly remove and/or dispose of rejected material and replace the same, the County may remove and replace the same and deduct the cost of the work from the contract amount.

If the Contractor chooses to use material other than specified herein, a sample of the material with supporting manufacturer's literature and specifications must be submitted to the County Contract Administrator for prior approval.

Sign-blades reflective sheeting and posts shall conform to the details for street name signs.

PART 3 - EXECUTION

3.1 UTILITY SPOTS

All street name signs shall be fabricated and installed in accordance with the plans and related documents. Contractor shall contact Sunshine State One Call of Florida (811 or 800-432-4770) at least 48 hours prior to digging or driving posts.

3.2 SIGN LAYOUT AND LEGEND

Letter shape and width of stroke shall comply with FHWA & MUTCD standards. For street name signs, lettering, border and blade dimensions shall be consistent with the County's standard detail for street name signs.

3.3 SIGN INSTALLATION

- A. Signs shall be placed at the typical locations shown in the plans. The soil around the post shall be solidly tamped so that the sign will stand vertically.
- B. If a sign cannot be placed where indicated due to a conflict, the Contractor shall immediately notify the County for an alternate location.
- C. The Contractor shall submit a *Fabricate, Install, and Removal Daily Report Sheet* (Exhibit D) of each sign installation placed for inspection by the County. Contractor shall repair or replace signs deemed unacceptable by the County, at no expense to the County.

3.4 REMOVAL OF SIGNS AND MARKERS

- A. Existing metal street name signs and painted concrete street name markers specified for removal shall be removed from the site, delivered, and unloaded, as directed by the County.
- B. Holes created by the removal of the signs and markers shall be filled with clean soil, which shall be firmly hand tamped to match the level of the surrounding ground.

PART 4 – MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The quantities to be paid for will be:

1. The number and type of street name sign assemblies plus the number and type of auxiliary signs of each designated class complete.
2. The number of existing metal street name signs and concrete markers removed, relocated, modified, and placed on specified supports, of each designated class of assembly complete.
3. The number of each existing sign panel removed, complete.

4.2 BASIS OF PAYMENT

Price and payment will be full compensation for furnishing and installation of all materials necessary to complete the signs in accordance with the details shown in the plans; including sign panels complete with sheeting, painting, and message; sign posts and supports, footings, excavation, etc.; and all other work specified in this Section, including all incidentals necessary for the complete item.

END OF SECTION 04020

SECTION 04030 – SPAN MOUNTED STREET NAME SIGNS

PART 1 – GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of the Contract, including General and Supplementary Conditions and other Specifications Sections, apply to work of this section.
- B. Unless otherwise specified on the plan sheets or in other sections of this contract, all materials and works shall conform to the applicable requirements in the following document.
 - 1. USDOT, Federal Highway Administration *Manual on Uniform Traffic Control Devices for Streets and Highways, Latest Edition.*
 - 2. USDOT, Federal Highway Administration *Standard Alphabets for Highway Signs and Pavement Markings, Latest Edition.*
 - 3. Florida Department of Transportation, *Standard Specifications for Road and Bridge Construction, Section 700, Latest Edition.*
 - 4. FDOT *Design Standards for Design, Construction, Maintenance, and Utility Operations on the State Highway System, Latest Edition.*

1.2 DESCRIPTION OF WORK

The work under this section includes the fabrication of span mounted signs. The Contractor shall furnish all labor, materials, tools, supplies, equipment, and machinery necessary to fully complete the work shown in the work order and in these specifications.

PART 2 – PRODUCTS

2.1 MATERIALS

All materials shall be new and of good quality unless otherwise specified. The Contractor, at his own expense and if requested by the County Contract, shall furnish samples of material and/or shall certify that the material meets all FDOT requirements except as defined herein. All material or work that has been rejected shall be remedied by the Contractor at his own expense and without delay. If the Contractor fails to promptly remove and/or dispose of rejected material, the County may remove and replace the same and deduct the cost of the work from the contract amount.

If the Contractor chooses to use material other than specified herein, a sample of the material with supporting manufacturer's literature and specifications must be submitted to the County for prior approval.

2.2 SIGN BLADES

1. 0.125 gauge, 5052-H38 domestic aluminum alloy, 18" in height by various lengths.
2. 0.50' radius rounded corners free of sharp edges.
3. Color and corrosion resistance per Aodine 1200 F t treatment or approved equivalent.

2.3 SIGN FACING

The sign facing and legend may be fabricated by any of the following methods and materials:

- A. Green retro-reflectivity ink silk-screened onto white Diamond grade material.
- B. Green electronic cuttable prismatic sheeting film over white Diamond grade material.

PART 3 – EXECUTION

3.1 GENERAL

All street name signs shall be fabricated in accordance with the plans and related documents.

3.2 SIGN LAYOUT & LEGEND

Letter shape and width of stroke shall comply with FHWA & MUTCD standards except as modified below for street names:

A. SIGN LAYOUT

1. Left and right margins shall be at least 2 inches.
2. Border width shall be 1" with 2" radius at all corners.
3. Arrows shall be 4" in height and 8" in length and placed 2" above the lower border.
4. Prefixes and suffixes shall be placed 3" below the upper border.

5. Sign lengths shall be in 6" increments as determined by the legend. Minimum length shall be 48 inches.

B. LETTERS

1. Letters shall be FHWA Series "C", upper and lower case. However, Clearview font should be available upon request.
2. Street Names: Initial letters shall be 12" upper case and subsequent letters shall be 9" lower case. Names shall be centered between the upper and lower borders.
3. Prefixes and suffixes: Initial letters shall be 4" upper case and subsequent letters shall be 3" lower case.
4. Suffixes "nd", "rd", "st", and "th" as associated with numbered street names shall be 4" in height and positioned in the upper portion of the primary street name field.

3.3 SIGN INSTALLATION

- A. Signs shall be placed at the typical locations shown in the plans.
- B. If a sign cannot be placed where indicated on the plans due to a conflict, the Contractor shall immediately notify the County for an alternate location.

3.4 REMOVAL OF SIGNS

Existing metal street name signs specified for removal shall be removed from the site, delivered, and unloaded, as directed by the Engineer.

PART 4 – MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The quantities to be paid for will be the number of square- feet of overhead signs span wire mounted, complete.

4.2 BASIS OF PAYMENT

Price and payment will be full compensation for furnishing and installation of all materials necessary to complete the signs in accordance with the details shown in the plans; including sign panels complete with sheeting, painting, and message; and all other works specified in this Section, including all incidentals necessary for the complete item.

END OF SECTION 04030

SECTION 04040 – PAVEMENT MARKINGS

PART 1 – GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and General Provisions of the Contract, including General and Supplementary Conditions and other Specifications Sections, apply to work of this section.
- B. Unless otherwise specified on the plan sheets or in other sections of this contract, all materials and work shall conform to the applicable requirements in the following documents:
 - 1. Florida Department of Transportation *Roadway and Traffic Design Standards*, Indices 17344 through 17359, *Latest Edition*.
 - 2. Florida Department of Transportation *Standard Specifications for Road and Bridge Construction*, Sections 701, 705, 706, 710, 711, 970, 971, and 993, *Latest Edition*.
 - 3. USDOT, Federal Highway Administration *Manual on Uniform Traffic Control Devices for Streets and Highways*, *Latest Edition*.

1.2 DESCRIPTION OF WORK

The work under this section includes the installation and removal of temporary and permanent pavement markings, textured pavement, reflective markers, galvanized posts, flex posts, delineators, wheel stops, and audible and vibratory pavement markings. The Contractor shall furnish all labor, materials, tools, supplies, equipment, and machinery necessary to fully complete the work shown in the plans and in these specifications. Pavement marking notes on plan sheets shall take precedence over and modify conflicting Technical Specifications.

PART 2 – PRODUCTS

2.1 MATERIALS

All materials shall be new and of good quality unless otherwise specified. The Contractor, at his own expense and if requested by the County, shall furnish samples of material and/ or shall certify that the material meets all FDOT requirements. All material or work that has been rejected shall be remedied by the Contractor at his own expense and without delay. If the Contractor fails to promptly remove and/or dispose of rejected material and replace the same, the County may remove and replace the same and deduct the cost of the work from the contract amount.

2.2 TEMPORARY PAVEMENT MARKINGS

Materials for temporary pavement marking shall meet all requirements of FDOT Specs, Section 710, *Latest Edition*.

2.3 PERMANENT PAVEMENT MARKINGS

Materials for permanent pavement markings shall meet all requirements of FDOT Specs, Section 711, *Latest Edition*.

2.4 REFLECTIVE PAVEMENT MARKERS

Materials for reflective pavement markers shall meet all requirements of FDOT Specifications, Sections 706, *Latest Edition*.

2.5 OBJECT MARKERS AND DELINEATORS

Materials for object markers shall meet all requirements of FDOT Specifications, Sections 705, *Latest Edition*.

2.6 AUDIBLE AND VIBRATORY PAVEMENT MARKINGS

Materials for audible and vibratory pavement markings shall meet all requirements of FDOT Specifications, Sections 701, *Latest Edition*.

PART 3 – EXECUTION

3.1 GENERAL

All pavement markings shall be applied in accordance with FDOT requirements.

3.2 TEMPORARY PAVEMENT MARKINGS

Temporary pavement markings shall be installed at the end of each day on new pavement surfaces and shall be maintained until permanent markings are installed.

3.3 PERMANENT PAVEMENT MARKINGS

Permanent pavement markings, including painted stripes, thermoplastic stripes, and reflective pavement markers, shall be installed as shown in the plans. Materials and installation shall conform to applicable standards in the documents referenced in Section 1.1. Installation of permanent markings on all final asphaltic concrete surfaces shall not be accomplished prior to 14 calendar days, nor later than 30 calendar days, after placement of the final surfaces.

3.4 RETROREFLECTIVITY

The Contractor shall, within thirty days of completion, furnish retroreflectivity

readings certifying the materials meet all FDOT requirements as per Part I, 1.1.B.2, Sections 710 and 711.

PART 4 – MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

The engineer or project manager may specify a lump sum or measurement of quantities.

The quantities to be paid for under this Section will be the length in feet or gross mile of Skip Traffic Stripes, the length in feet or gross mile of Solid Traffic Stripes, the number of directional arrows and pavement messages, painted, the area in square feet or of Reflective Paint (Island Nose), and the area in square feet or the length in feet to Remove Existing Markings. Measurement will be taken as the distance from the beginning of the first painted stripe to the end of the last painted stripe with proper deductions made for unpainted intervals will not be included in pay quantity.

4.2 BASIS OF PAYMENT

Prices and payment will be full compensation for all work specified in this Section, including, all cleaning and preparing of surfaces, furnishing all materials, application, curing and protection of all items, protection of traffic, furnishing of all tools, machines and equipment, and all incidentals necessary to complete the work. Final payment will be withheld until all deficiencies are corrected.

END OF SECTION - 04040

SECTION 04060 - MAINTENANCE OF TRAFFIC

PART 1 - GENERAL

1.1 RELATED DOCUMENTS

- A. Drawings and general provisions of the Contract, including General and Supplementary Conditions and other Specifications Sections, apply to work of this section.
- B. Unless otherwise specified on the plan sheets or in other sections of the specifications, all materials and works shall conform to the applicable requirements in the following documents:
 - 1. *Florida Department of Transportation Design Standards, Latest Edition.*
 - 2. *Florida Department of Transportation Standard Specifications for Road and Bridge Construction, Section 102, Latest Edition.*
 - 3. USDOT, Federal Highway Administration *Manual on Uniform Traffic Control Devices for Streets and Highways, Latest Edition, Part 6 Temporary Traffic Controls.*
 - 4. *FDOT Minimum Specifications for Traffic control and Devices, Latest Edition.*

1.2 SUMMARY OF WORK

The work under this section includes the maintenance of traffic within the limits of the project for the duration of construction.

PART 2 – PRODUCTS - Not Used.

PART 3 - EXECUTION

3.1 RESPONSIBILITIES OF CONTRACTOR

- A. Control and maintain traffic and provide for the safety of the work area in accordance with Maintenance of Traffic (MOT) Plan included in the contract documents. Contractor shall comply with all aspects of said plan. Conduct operations in a manner that will not interrupt pedestrian and vehicle traffic except as approved by the County Engineer/Traffic Division. Confine the work area to the smallest area practical to allow the maximum use of the street and sidewalk and to reduce any hazard to vehicles and pedestrians to a minimum.
- B. Maintain access to properties that adjoin the work. Contact property owners

and assure that access is coordinated prior to commencing work that may block access.

- C. Furnish all labor, materials, tools, supplies, equipment, and machinery needed to fully comply with the specifications described on the plan sheets and in this Section. At all times, the Contractor shall use workers and traffic control devices necessary to comply with all applicable provisions contained in the reference documents listed in Section 1.1.
- D. The Contractor shall notify the agencies and media listed below in writing, 48 hours in advance, of any work within the road right-of-way that may interfere with vehicle and/or pedestrian traffic.
 - 1. WCOA Radio – Tel: 478-6011; Fax: 478-3971
 - 2. Pensacola News Journal Tel: 435-8500; Fax: 435-8633; Email: news@pensacolanewsjournal.com
 - 3. Escambia County Emergency Management Tel: 471-6315; Fax: 471-6322; Email: bob_boschen@co.escambia.fl.us
 - 4. Escambia County Engineering Tel: 595-3440
 - 5. Escambia County Sheriff Tel: 436-9630; Fax: 436-9128; Email: traffic@escambiaso.com
 - 6. Florida Highway Patrol Tel: 484-5000; Fax: 393-3405; Email: stevepreston@flhsmv.gov
 - 7. Escambia County School District Tel: 469-5591; Fax: 469-5661; Email: transportation@escambia.k12.fl.us and rdoss@escambia.k12.fl.us
 - 8. Escambia County Administration Tel: 595-4900; Fax: 595-4908; Email: Cheryl_Lively@co.escambia.fl.us
 - 9. Escambia County Area Transit Tel: 595-3228; Fax: 595-3222; Email: Ted_Woolcock@co.escambia.fl.us

3.2 PENALTIES AND SUSPENSION OF WORK

The County may verbally direct the Contractor to immediately suspend work if appearance of violation of safety regulations is found. In such an event, Contractor shall immediately stop work and secure any potential hazards from the public until the potential violation is confirmed and/or corrected to satisfaction of the County. Law enforcement officers may be called to assist the County in suspending work if the Contractor is not responsive. Suspension of work for violation of safety

regulations shall not be grounds for a contract time extension or additional payment.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

- A. Maintenance of Traffic: Where the plans require the use of trucks and truck mounted impact attenuators, these items will not be paid for separately but shall be included in the cost of Maintenance of Traffic. Only use those attenuators that have been tested by a facility approved by the Engineer and certified as meeting the requirements as specified in NCHRP 350 and that have been properly maintained.
- B. Law Enforcement Services: The quantity to be paid for will be at the Contract unit price per hour for the actual number of officers on the project site. Payment will be made only for those off-duty law enforcement officers specified in the MOT and authorized by the County.
- C. When the plans show more than one detour facility is included in the proposal, payment will be made under Maintenance of Traffic.
- D. Materials for Driveway Maintenance: The quantity to be paid for will be, in square yards, of all materials authorized by the County, acceptably placed and maintained for driveway maintenance. The quantity will be determined by in place measurement.

4.2 BASIS OF PAYMENT

- A. MAINTENANCE OF TRAFFIC (GENERAL WORK): Price and payment will be full compensation for all work and costs specified under this Section except as may be specifically covered for payment under other items.
- B. LAW ENFORCEMENT: Prices and payment will be considered full compensation for the services of the off-duty law enforcement officer, including a marked law enforcement vehicle and all other direct and indirect costs.
- C. SPECIAL DETOURS: Price and payment will be full compensation for providing all detour facilities shown on the plans and all costs incurred in carrying out all requirements of this Section for general maintenance of traffic within the limits of the detour, as shown on the plans.

END OF SECTION 04060

SECTION 04090 – CONSTRUCTION OF TRAFFIC SIGNALS

PART 1 – GENERAL

1.1 RELATED DOCUMENTS

- A. Unless otherwise specified by the County, or in other sections of this specification, all work shall conform with the applicable requirements in the following documents:
1. Florida Department of Transportation, *Design Standards for Design, Construction, Maintenance and Utility Operations on the State Highway System, Latest Edition.*
 2. FDOT, *Standard Specifications for Road and Bridge Construction, Latest Edition.*
 3. FDOT, *Minimum Specifications for Traffic Control Signals and Devices, Latest Edition.*
 4. United States Department of Transportation (USDOT), Federal Highway Administration, *Manual on Uniform Traffic Control Devices for Streets and Highways, Millennium Edition.*
 5. *National Electric Code*, including latest revisions.

1.2 DESCRIPTION OF WORK

The work under this section involves the installation and modification of traffic signal equipment. Work will be initiated through the issuance of Work Orders that will identify a specific scope and location.

PART 2 - PRODUCTS

2.1 CONTRACTOR-FURNISHED PARTS AND EQUIPMENT

- A. The Contractor shall furnish all transportation, plant, labor, materials, safety signs, supplies, equipment, and other facilities and things necessary to fully complete the work described in this specification.
- B. The requirements and procedure described in Sections 603-2, 603-3, 603-5, 603-6, 603-7, and 603-8 of the FDOT *Standard Specifications for Road and Bridge Construction, Latest Edition* shall apply.

2.2 COUNTY-FURNISHED EQUIPMENT INSTALLED BY CONTRACTOR

Where the plans include installation of County-furnished equipment, the County will turn over such equipment to the Contractor when the construction progress allows or as designated in the plans. The County will bear the costs of correcting any defects in the equipment found by the Contractor. The Contractor will maintain the equipment in proper operational condition after pick-up at no cost to the County until either final acceptance or the equipment is returned to the County.

2.3 REMOVED PARTS AND EQUIPMENT

- A. Equipment that is removed and suitable for reuse shall be delivered to the County as indicated on the construction plans. Such equipment shall be tagged as to the location from which it was removed.
- B. Parts and equipment that are removed and not suitable for reuse, but have salvage value, shall be delivered to the Road Department facility, 601 North Hwy 297A, Cantonment.
- C. Parts and equipment that are removed, not suitable for reuse, and without salvage value, shall be properly disposed by the Contractor, at his expense.

PART 3 - EXECUTION

3.1 QUALIFICATIONS

- A. The Contractor shall have a sufficient amount of prior satisfactory experience in the construction of all traffic signal components including closed-loop systems and video detection systems.
- B. All persons operating and maintaining signal equipments shall be fully trained and qualified. The Contractor shall have all work performed under the direct, on-site, supervision of a person certified at the "Traffic Signal, Level II" level, or higher, by the International Municipal Signal Association (I.M.S.A.). The Contractor shall furnish a copy of the certificate issued by the I.M.S.A. for each technician to the Contract Manager before execution of the contract.

3.2 OPERATIONS

- A. The Contractor shall replace entire sidewalk slabs and driveway slabs, at the Contractor's expense, if they are damaged.
- B. All public land corners and monuments encountered shall be protected by the Contractor. Corners and monuments which conflict with the work and in danger of disturbance shall be properly referenced by a Florida registered surveyor prior to beginning work at the site. The Contractor

shall assume all costs associated with restoration of corners and monuments.

- C. The Contractor shall coordinate and perform service transfers and adjustments with Gulf Power Company.
- D. The Contractor shall remove all surplus materials from the right-of-way within 24 hours.

3.3 REPORTS

- A. The Contractor shall test each new ground rod and ground rod assembly in accordance with FDOT standards. Record test results and certify accuracy on a Traffic Signal Resistance Data Sheet (re: Appendix "A"). Furnish the original certified data sheet to the Contract Manager.
- B. Contractor shall test each new loop assembly in accordance with FDOT standards. Record test results and certify accuracy on a Traffic Signal Resistance Data Sheet (re: Appendix "A"). Furnish the original certified data sheet to the Contract Manager.

3.4 COMPLETION TIME

The Contractor shall complete work according to the schedule specified in the Work Order. Typically, completion time will be specified according to the representative schedule provided below.

- A. Construct school zone flashing beacon assembly (pedestal-mount): complete within 60 days.
- B. Construct new multi-phase traffic signal: Order equipment from vendors within 10 business days from date of Work Order. Complete installation within 30 days upon receipt of all equipment from vendors.
- C. Install signal head and/or cable to create a left-turn phase: complete within 30 days.
- D. Install new controller assembly: Order equipment from vendor within 5 business days from date of Work Order. Complete installation within 30 days upon receipt from vendor.
- E. Install pedestrian detector station with or without signals: complete within 30 days.
- F. Install new loop assembly: complete within 10 business days.

3.5 INSPECTION AND ACCEPTANCE OF WORK

Acceptance procedures described in Sections 611-2, 611-3, and 611-4 of the FDOT *Standard Specifications for Road and Bridge Construction, Latest Edition*, shall apply unless otherwise specified in the Work Order.

PART 4 - MEASUREMENT/PAYMENT

4.1 METHOD OF MEASUREMENT

Measurement and payment of all items will be made in accordance with the current Construction and Response maintenance contract on file with the Escambia County Office of Purchasing.

END OF SECTION 04090

Appendix "A"

TRAFFIC SIGNAL RESISTANCE MEASUREMENTS DATA SHEET

Intersection: _____

LOOP ASSEMBLY RESISTANCE

Loop Location/No. Resistance Series Resistance Insulation Resistance

1			
2			
3			
4			
5			
6			
7			
8			
9			
10			
11			
12			

GROUND ROD RESISTANCE

D

Rod Location

1		
2		
3		
4		
5		
6		
7		
8		
9		
10		
11		
12		

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Signature of Contractor's Representative
 IMSA Level II-Certified Technician

Date

Appendix B

FDOT / FDEP / ACOE PERMITS



Florida Department of Transportation

RICK SCOTT
GOVERNOR

6025 Old Bagdad Hwy
Milton, FL 32583

Jim Boxold
SECRETARY

Cover Letter

DATE: July 14, 2016

FROM: Justin Jeanneret

TO: Thomas McLendon
Hatch Mott MacDonald
220 West Garden Street, Suite 700
Pensacola FL, 32502

SUBJECT: APPROVED FDOT Permit

PROJECT LOCATION: Detroit Blvd Intersection Improvements at SR 297 (Pine Forest Rd.)

Hello Thomas,

I am sending you one (1) copy of the approved FDOT Driveway permit for Detroit Blvd. Intersection Improvements on Pine Forest Rd. The permit is approved with conditions in the attached addendum which must be met prior to beginning construction on State Right of Way.

Please contact me if you have any questions.

THANK YOU,

Justin Jeanneret
Permits Coordinator II
6025 Old Bagdad Hwy.
Milton, FL 32583
(O) 850-981-2701

Enclosed: FDOT Driveway Permit #16-A-395-0046

Cc: Escambia County

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
**DRIVEWAY/CONNECTION PERMIT
FOR ALL CATEGORIES**

PART 1: PERMIT INFORMATION

Application Number: 16A3950046
Permit Category: Safety Upgrade Access Classification: 3
Project: Detroit Blvd Intersection Improvements at SR 297 (Pine Forest Road)
Permittee: Escambia County, FL
Section/Mile Post: 48190 / 3.570 State Road: 297
Section/Mile Post: _____ State Road: _____

PART 2: PERMITTEE INFORMATION

Permittee Name: Escambia County, Florida
Permittee Mailing Address: 3363 West Park Place
City, State, Zip: Pensacola, Florida, 32505
Telephone: 850-595-3434
Engineer/Consultant/or Project Manager: Thomas McLendon
Engineer responsible for construction inspection: Michael Gund 7941
NAME P.E. #
Mailing Address: 220 West Garden Street, Suite 700
City, State, Zip: Pensacola, Florida, 32502
Telephone: 850-484-6011 Mobile Phone: _____

PART 3: PERMIT APPROVAL

The above application has been reviewed and is hereby approved subject to all Provisions as attached.
Permit Number: 16-A-395-0046
Department of Transportation
Signature: Kelli B Rice Title: Operations Program Engineer
Department Representative's Name: Kelli B. Rice, P.E.
Temporary Permit: YES NO (If temporary, this permit is only valid for 6 months)
Special provisions attached: YES NO
Date of Issuance: 7/14/16

If this is a normal (non-temporary) permit it authorizes construction for one year from the date of issuance. This can only be extended by the Department as specific in 14-96.007(6).

See following pages for General and Special Provisions

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
**DRIVEWAY/CONNECTION PERMIT
FOR ALL CATEGORIES****PART 4: GENERAL PROVISIONS**

1. Notify the Department of Transportation Maintenance Office at least 48 hours in advance of starting proposed work.
Phone: 850-981-3000, Attention: Permits Unit
2. A copy of the approved permit must be displayed in a prominent location in the immediate vicinity of the connection of construction.
3. Comply with Rule 14-96.008(1), F.A.C., Disruption of Traffic.
4. Comply with Rule 14-96.008(7), F.A.C., on Utility Notification Requirements.
5. All work performed in the Department's right of way shall be done in accordance with the most current Department standards, specifications and the permit provisions.
6. The permittee shall not commence use of the connection prior to a final inspection and acceptance by the Department.
7. Comply with Rule 14-96.003(3)(a), F.A.C., Cost of Construction.
8. If a Significant Change of the permittee's land use, as defined in Section 335.182, Florida Statutes, occurs, the Permittee must contact the Department.
9. Medians may be added and median openings may be changed by the Department as part of a Construction Project or Safety Project. The provision for a median might change the operation of the connection to be for right turns only.
10. All conditions in NOTICE OF INTENT WILL APPLY unless specifically changed by the Department.
11. All approved connection(s) and turning movements are subject to the Department's continuing authority to modify such connection(s) or turning movements in order to protect safety and traffic operations on the state highway or State Highway System.
12. **Transportation Control Features and Devices in the State Right of Way.** Transportation control features and devices in the Department's right of way, including, but not limited to, traffic signals, medians, median openings, or any other transportation control features or devices in the state right of way, are operational and safety characteristics of the State Highway and are not means of access. The Department may install, remove or modify any present or future transportation control feature or device in the state right of way to make changes to promote safety in the right of way or efficient traffic operations on the highway.
13. The Permittee for him/herself, his/her heirs, his/her assigns and successors in interest, binds and is bound and obligated to save and hold the State of Florida, and the Department, its agents and employees harmless from any and all damages, claims, expense, or injuries arising out of any act, neglect, or omission by the applicant, his/her heirs, assigns and successors in interest that may occur by reason of this facility design, construction, maintenance, or continuing existence of the connection facility, except that the applicant shall not be liable under this provision for damages arising from the sole negligence of the Department.
14. The Permittee shall be responsible for determining and notify all other users of the right of way.
15. Starting work on the State Right of Way means that I am accepting all conditions on the Permit.

16 A 3950046

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
**DRIVEWAY/CONNECTION PERMIT
 FOR ALL CATEGORIES**

PART 5: SPECIAL PROVISIONS

NON-CONFORMING CONNECTIONS: YES NO

If this is a non-conforming connection permit, as defined in Rule Chapters 14-96 and 14-97, then the following shall be a part of this permit.

1. The non-conforming connection(s) described in this permit is (are) not permitted for traffic volumes exceeding the Permit Category on page 1 of this permit, or as specified in "Other Special Provisions" below.
2. All non-conforming connections will be subject to closure or relocation when reasonable access becomes available in the future.

OTHER SPECIAL PROVISIONS:

SEE ATTACHED ADDENDUM

PART 6: APPEAL PROCEDURES

You may petition for an administrative hearing pursuant to sections 120.569 and 120.57, Florida Statutes. If you dispute the facts stated in the foregoing Notice of Intended Department Action (hereinafter Notice), you may petition for a formal administrative hearing pursuant to section 120.57(1), Florida Statutes. If you agree with the facts stated in the Notice, you may petition for an informal administrative hearing pursuant to section 120.57(2), Florida Statutes. You must file the petition with:

Clerk of Agency Proceedings
 Department of Transportation
 Haydon Burns Building
 605 Suwannee Street, M.S. 58
 Tallahassee, Florida 32399-0458

The petition for an administrative hearing must conform to the requirements of Rule 28-106.201(2) or Rule 28-106.301(2), Florida Administrative Code, and be filed with the Clerk of Agency Proceedings by 5:00 p.m. no later than 21 days after you received the Notice. The petition must include a copy of the Notice, be legible, on 8 1/2 by 11 inch white paper, and contain:

1. Your name, address, telephone number, any Department of Transportation identifying number on the Notice, if known, the name and identification number of each agency affected, if known, and the name, address, and telephone number of your representative, if any, which shall be the address for service purposes during the course of the proceeding.
2. An explanation of how your substantial interests will be affected by the action described in the Notice;
3. A statement of when and how you received the Notice;
4. A statement of all disputed issues of material fact. If there are none, you must so indicate;
5. A concise statement of the ultimate facts alleged, including the specific facts you contend warrant reversal or modification of the agency's proposed action, as well as an explanation of how the alleged facts relate to the specific rules and statutes you contend require reversal or modification of the agency's proposed action;
6. A statement of the relief sought, stating precisely the desired action you wish the agency to take in respect to the agency's proposed action.

If there are disputed issues of material fact a formal hearing will be held, where you may present evidence and argument on all issues involved and conduct cross-examination. If there are no disputed issues of material fact an informal hearing will be held, where you may present evidence or a written statement for consideration by the Department.

Mediation, pursuant to section 120.573, Florida Statutes, may be available if agreed to by all parties, and on such terms as may be agreed upon by all parties. The right to an administrative hearing is not affected when mediation does not result in a settlement.

Your petition for an administrative hearing shall be dismissed if it is not in substantial compliance with the above requirements of Rule 28-106.201(2) or Rule 28-106.301(2), Florida Administrative Code. If you fail to timely file your petition in accordance with the above requirements, you will have waived your right to have the intended action reviewed pursuant to chapter 120, Florida Statutes, and the action set forth in the Notice shall be conclusive and final.

16A3950046

ADDENDUM TO PERMIT

****The Following Special Conditions Apply*

- 1) Revise new construction pavement section for Pine Forest Rd right turn lane to use Type SP 12.5 Structure Course at 3".
- 2) Revise site plan to include 2016-17 FDOT Design Standard Index Sheets relevant to work to be performed in the right-of-way. (104, 105, 301, 302, 515, 613, 11860, 17302, 17346, etc...)
- 3) Revise site plan to label Right of Way line on Pine Forest Rd.
- 4) Revise site plan to place a bicycle pavement marking in new bike lane.
- 5) Revise site plan to label a minimum 30" strip of sod to be placed behind edge of pavement within the right-of-way.
- 6) Remove plans sheets #9 and 11-15 not pertaining to work to be performed in FDOT R/W.
- 7) Revise site plan to locate new "Begin Right Turn Lane Yield to Bikes" sign before turn lane taper and closer to the travel lane of Pine Forest Rd.
- 8) Label Pine Forest Rd. also as State Road 297 on all plan sheets.
- 9) Revise site plan to label total length of new right turn lane (including taper) on Pine Forest Rd.
- 10) Revise lane closure note to read: There are no lane closures associated with proposed work in this permit. If a lane closure is needed, submit a written request with proposed dates/times. Request will need to be sent to the office issuing the permit. Allow at least 2 weeks in your scheduling for this special review.
- 11) The applicant will provide documentation by a Professional Engineer register in the State of Florida that construction was accomplished in accordance with the requirements set out in the permit. This documentation shall include a statement that necessary inspections, test, and physical measurements have been made, that construction was accomplished in accordance with the design information included with the connection permit in accordance with Rule Chapter 14-96, F.A.C., and that all materials entering into the work conform to the specifications in the connection permit, conform to the applicable specifications contained in the Standard Specifications for Road and Bridge Construction, or otherwise conform to or meet generally accepted professional practices. The Record Drawings Report by Permittee's Professional Engineer, Form 850-040-19, shall be used for this purpose.
- 12) Permittee and/or contractor shall submit a performance bond and signed/sealed security instrument to the FDOT office issuing the permit prior to beginning work on State Right-of-Way.
- 13) Provide a Quality Control Plan to the office issuing the permit for review and approval prior to beginning Construction.
- 14) A Pre-Construction meeting shall be held prior to construction with contractor, testing laboratory, owner, FDOT, engineer (anyone involved with construction) present.
- 15) Permittee shall be responsible for any fines associated with this connection and Federal, State, or local Environmental Regulations.
- 16) **Permittee is responsible for restoring FDOT right-of-way to original or better condition after construction is complete.** Permittee shall be responsible to clean and grade ditch to the flowline of culverts and/or match adjacent existing conditions.
- 17) Permittee hereby agrees to adhere to FDOT's Utility Accommodation Manual and FDOT's Design Standard Index 104 and **Florida Erosion Control and Sediment Control Manual**, which relate to EROSION CONTROL in the permitted work area and is further amended to the Permittee to:
 - A) Provisions of the Utility Accommodation Manual and Standard Specifications concerning grassing and restoration of FDOT right-of-way will be strictly enforced. **All portions of the Right-of-Way disturbed**

in the construction of this facility will be sodded, and/or seeded and mulched per FDOT's Index 105.

- B) Minimum application rates for seeding:** It is recommended that permanent grass species be a mixture of Bahia and Bermuda at the rate of 30# each or a total of 60# per acre. (A species variation may occur in some areas as a consideration to adjacent land owners.) The recommended temporary seed application from April through September is Brown Top or Japanese Millet while Annual Rye Grass is sufficient from October through March. Depending on soil conditions, this temporary seed rate may vary 20-30#'s. ALL AREAS are to be reseeded regardless of size.
 - C) Minimum application rates for fertilizer:** It is recommended that a granular blend or homogenized fertilizer be applied at the rate of 400# per acre. The minimum analysis (NPK) is 12-8-8 with 8% Sulfur. (An acceptable substitute is 13-13-13.) Fertilizer may need to be applied from the edge of pavement to the Right-of-Way line.
- 18) All seed, sod, and mulch shall be free of noxious weeds and exotic pest plants, as listed in the current Category I "List of Invasive Species" from Florida Exotic Pest Plant Council (**FLEPPC, www.fleppc.org**).
- 19) *All lanes must be open within 24 hours after notice, during an evacuation notice of a hurricane or other catastrophic event and shall remain open for the duration of the evacuation or event as directed by the local FDOT Maintenance Engineer or his designee.*

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES

OFFICE USE ONLY

Application Number: 16A3950046
 Category: Safety Upgrade
 Section/Mile Post: 48190 / 3.570
 Section/Mile Post: _____

Received By: Justin Jeanneret
FDOT STAFF (TYPE OR PRINT)
 Date: 7/1/2016
 State Road: 297
 State Road: _____

Instructions – To Applicant

- Contact the Department of Transportation to determine what plans and other documents you are required to submit with your application.
- Complete this form (some questions may not apply to you) and attach all necessary documents and submit it to the Department of Transportation.
- For help with this form contact your local Maintenance or District Office.
 - Or visit our website at <https://www3.dot.state.fl.us/OneStopPermitting/Home.aspx> for the contact person and phone number in your area.
 - You may also email – driveways@dot.state.fl.us
 - Or call your District or local Florida Department of Transportation Office and ask for Driveway Permits.

Please print or type

APPLICANT:

Check one:

Owner Lessee Contract to Purchase

Name: Escambia County, Florida

Responsible Officer or Person: Joy Jones

If the Applicant is a Company or Organization, Name: Escambia County, Florida

Address: 3363 West Park Place

City, State: Pensacola, Florida

Zip: 32505 Phone: 850-595-3434 Fax: _____

Email: dijones@myescambia.com

LAND OWNER: (If not applicant)

Name: _____

If the Applicant is a Company or Organization, Name: _____

Address: _____

City, State: _____

Zip: _____ Phone: _____ Fax: _____

Email: _____

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
**DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES**

AUTHORIZED REPRESENTATIVE: If specified by Applicant to handle, represent, sign, and file the application --
NOTE: A notarized letter of authorization must be provided with the Application.

Name: _____

Company Name: _____

Address: _____

City, State: _____

Zip: _____ Phone: _____ Fax: _____

Email: _____

Address of property to be served by permit (if known):

If address is not known, provide distance from nearest intersecting public street (such as, 500 feet south of Main St.)
Connection is for Detroit Boulevard at Pine Forest Road

Check here if you are requesting a

new driveway temporary driveway modification to existing driveway safety upgrade

Does the property owner own or have any interests in any adjacent property?

No Yes, if yes – please describe:

Are there other existing or dedicated public streets, roads, highways or access easements bordering or within the property?

No Yes, if yes – list them on our plans and indicate the proposed and existing access points.

Local Government Development Review or Approval Information:

Local Government Contact: _____

Name: _____

Government Agency: _____

Phone #: _____

16A3950046

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRIVEWAY/CONNECTION APPLICATION
FOR ALL CATEGORIES

If you are requesting commercial or industrial access, please indicate the types and number of businesses and provide the floor area square footage of each. Use additional sheets if necessary.

Business (Name and Type)	Square Footage	Business (Name and Type)	Square Footage
1. Widening Right Turn Lane on Detroit Blvd		3.	
2. Extending Right Turn Lane on SR 297 on Detroit Blvd		4.	

If you are requesting a residential development access, what is the type (single family, apartment, townhouse) and number of units?

Type	Number of Units

Provide an estimate of the daily traffic volume anticipated for the entire property at build out. (An individual single family home, duplex, or quad-plex is not required to complete this section).

Daily Traffic Estimate = 6500 (Use the latest Institute of Transportation Engineers (ITE) Trip Generation Report)

If you used the ITE Trip Generation Report, provide the land use code, independent variable, and reference page number.

ITE Land Use Code	Independent Variable	ITE Report page number reference

Check with the Florida DOT Office where you will return this form to determine which of the following documents are required to complete the review of your application.

Plans should be 11" x 17" (scale 1" x 50') Note: No plans larger than 24" x 36" will be accepted a) Highway and driveway plan profile b) Drainage plan showing impact to the highway right-of-way c) Map and letters detailing utility locations before and after Development in and along the right of way d) Subdivision, zoning, or development plans e) Property map indicating other access, bordering roads and streets	f) Proposed access design g) Parcel and ownership maps including easements (Boundary Survey) h) Signing and striping plans i) Traffic Control/Maintenance of Traffic plan j) Proof of liability insurance k) Traffic Impact Study l) Cross section of roadway every 100' if exclusive turn lanes are required
---	---

Important Notices to Applicant Before Signing Application

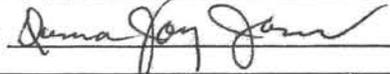
The Department Reserves The Right To Change Traffic Features And Devices In Right Of Way At Any Time
 Proposed traffic control features and devices in the right of way, such as median openings and other traffic control devices, are not part of the connection(s) to be authorized by a connection permit. The Department reserves the right to change these features and devices in the future in order to promote safety in the right of way or efficient traffic operations on the highway. Expenditure by the applicant of monies for installation or maintenance of such features or devices shall not create any interest in the maintenance of such features or devices.

Significant Changes In Property Use Must Undergo Further Review
 If an access permit is issued to you it will state the terms and conditions for its use. Significant changes in the use as defined in Section 335.182(3), Florida Statutes, of the permitted access not consistent with the terms and conditions listed on the permit may be considered a violation of the permit.

All Information I Give Is Accurate
 I certify that I am familiar with the information contained in this application and that to the best of my knowledge and belief, such information is true, complete and accurate.

Starting Work On The Driveway Connection After I Get My Permit Means I Accept All the Conditions In My Permit
 I will not begin work on the connection until I receive my Permit and I understand all the conditions of the Permit. When I begin work on the connection, I am accepting all conditions listed in my Permit.

Applicant Name (Printed): Joy Jones, P.E.

Applicant's signature:  Date: 6/30/16

16 A 3950046



Candace Drive

Sharon Lane

West Detroit Blvd.

Westbound I-10 Ramp

Pine Forest Rd (SR 297)



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION



Hatch Mott MacDonald
 320 West Garden Street, Suite 700
 Pensacola, Florida 32302 | 8501 484-6010
 Engineer of Record: Michael Gund, P.E. #17801



Board of County Commissioners
 Escambia County, Florida

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

16A3950046
660' AERIAL

SHEET NO.

McLendon, Thomas A

From: McLendon, Thomas A
Sent: Thursday, June 30, 2016 3:04 PM
To: 'brandon.knight@ecua.fl.gov'; 'Swails, Chad E.'; 'Diane Moore'; 'troy.young@cox.com'; 'sk1674@att.com'; 'abramblett@slfiber.com'; 'jeff.messer@transcore.com'; 'ggeren@att.com'
Cc: Gund, Michael A; Liz Bush; Hunt, Asa G
Subject: Escambia County Detroit Blvd. Intersection Improvements at SR 297 (Pine Forest Road) - Notification of all known right of way users
Attachments: 100% Escambia County Detroit Blvd Intersection Plans.pdf

All,
Per FDOT's requirement for acquiring a connection permit for this project, please find this as our official notification to utility owners (right of way users) of the proposed improvements for Detroit Blvd. at Pine Forest Rd. Please see the attached 100% plans for the proposed improvements and potential conflict area(s). Based on the improvements, we do not foresee any conflicts and have already coordinated with most of you at our previous meeting. Please let me know if you have any questions, thanks.

Thom

Thomas A. McLendon, PE
Project Manager



Florida Department of Environmental Protection

Northwest District
160 W. Government Street, Suite 308
Pensacola, Florida 32502-5740

Rick Scott
Governor

Carlos Lopez-Cantera
Lt. Governor

Jonathan P. Steverson
Secretary

August 1, 2016

Escambia County Board of County Commissioners
c/o Joy Jones
3363 West Park Place
Pensacola, Florida 32505-5250
Djjones@myescambia.com

File No.: 17-0345394-002-EE, Escambia County

Dear Ms. Jones,

On July 14, 2016 we received your request for verification of exemption to perform the following activities:

To widen the Westbound left turning lane on Detroit Boulevard at the intersection with Pine Forest Road to improve traffic flow and meet current roadway design and safety standards and construct a side walk at the project area, within and existing right -of-way. Projected project will have 0.0683 acres of permanent wetland impacts and 0.0098 temporary impacts on the wetlands in roadside ditch and swale, Class III Florida Waterbody, located at the intersection of Detroit Boulevard and Pine Forest Road, Pensacola, Florida 32534, in Section 12, Township 01 South, Range 31 West, of Escambia County at Latitude 30°31'24" North, Longitude 87°18'46" West.

Your request has been reviewed to determine whether it qualifies for (1) regulatory exemption, (2) proprietary authorization (related to state-owned submerged lands), and (3) federal approval that may be necessary for work in wetlands or waters of the United States.

Your project did not qualify for the federal review portion of this verification request.

Additional authorization must be obtained prior to commencement of the proposed activity. This letter does not relieve you from the responsibility of obtaining other federal, state, or local authorizations that may be required for the activity. Please refer to the specific section dealing with that portion of the review below for advice on how to proceed.

1. Regulatory Review – Verified

Based on the information submitted, the Department has determined that the roadway traffic flow improvements and sidewalk that will have 0.0683 permanent impact and 0.0098 temporary impact on un-named wetlands in roadside ditches and swales is exempt, under Section 373.406(6) of the Florida Statutes, from the need to obtain a regulatory permit under Part IV of Chapter 373 of the Florida Statutes. This determination is made because the activity, in

consideration of its type, size, nature, location, use, and operation, is expected to have only minimal or insignificant individual or cumulative adverse impacts on the water resources.

This exemption verification is based on the information you provided the Department and the statutes and rules in effect when the information was submitted. This verification may not be valid if site conditions materially change, the project design is modified, or the statutes or rules governing the exempt activity are amended. In the event you need to re-verify the exempt status for the activity, a new request and verification fee will be required. Any substantial modifications to the project design should be submitted to the Department for review, as changes may result in a permit being required.

2. Proprietary Review – Not required

The activity does not appear to be located on sovereign submerged lands, and does not require further authorization under Chapter 253 of the Florida Statutes, or Chapters 18-20 or 18-21 of the Florida Administrative Code.

3. Federal Review – SPGP Not Approved

Your proposed activity as outlined on your application and attached drawings **does not qualify** for federal authorization pursuant to the State Programmatic General Permit and a **SEPARATE permit** or authorization **may be required** from the U.S. Army Corps of Engineers (Corps). A copy of your permit application has been forwarded to the Corps for their review. The Corps will issue their authorization directly to you or contact you if additional information is needed. If you have not heard from the Corps within 30 days from the date your application was received at the local FDEP Office, contact the Corps at the Pensacola Regulatory Field Office at 850-439-3474 for status and further information. **Failure to obtain Corps authorization prior to construction could subject you to federal enforcement action by that agency.**

Authority for review – an agreement with the Corps entitled “Coordination Agreement Between the U.S. Army Corps of Engineers (Jacksonville District) and the Florida Department of Environmental Protection, or Duly Authorized Designee, State Programmatic General Permit,” Section 10 of the Rivers and Harbor Act of 1899, and Section 404 of the Clean Water Act.

Additional Information

Please retain this letter. The activities may be inspected by authorized state personnel in the future to ensure compliance with appropriate statutes and administrative codes. If the activities are not in compliance, you may be subject to penalties under Chapter 373, F.S., and Chapter 18-14, F.A.C.

NOTICE OF RIGHTS

This action is final and effective on the date filed with the Clerk of the Department unless a petition for an administrative hearing is timely filed under Sections 120.569 and 120.57, F.S., before the deadline for filing a petition. On the filing of a timely and sufficient petition, this action will not be final and effective until further order of the Department. Because the administrative hearing process is designed to formulate final agency action, the filing of a petition means that the Department's final action may be different from the position taken by it in this notice.

Petition for Administrative Hearing

A person whose substantial interests are affected by the Department's action may petition for an administrative proceeding (hearing) under sections 120.569 and 120.57, Florida Statutes. Pursuant to rule 28-106.201, Florida Administrative Code, a petition for an administrative hearing must contain the following information:

- (a) The name and address of each agency affected and each agency's file or identification number, if known;
- (b) The name, address, any email address, any facsimile number, and telephone number of the petitioner; the name, address, and telephone number of the petitioner's representative, if any, which shall be the address for service purposes during the course of the proceeding; and an explanation of how the petitioner's substantial interests are or will be affected by the agency determination;
- (c) A statement of when and how the petitioner received notice of the agency decision;
- (d) A statement of all disputed issues of material fact. If there are none, the petition must so indicate;
- (e) A concise statement of the ultimate facts alleged, including the specific facts that the petitioner contends warrant reversal or modification of the agency's proposed action;
- (f) A statement of the specific rules or statutes that the petitioner contends require reversal or modification of the agency's proposed action, including an explanation of how the alleged facts relate to the specific rules or statutes; and
- (g) A statement of the relief sought by the petitioner, stating precisely the action that the petitioner wishes the agency to take with respect to the agency's proposed action.

The petition must be filed (received by the Clerk) in the Office of General Counsel of the Department at 3900 Commonwealth Boulevard, Mail Station 35, Tallahassee, Florida 32399-3000 or at Agency_Clerk@dep.state.fl.us. Also, a copy of the petition shall be mailed to the applicant at the address indicated above at the time of filing.

Time Period for Filing a Petition

In accordance with rule 62-110.106(3), Florida Administrative Code, petitions for an administrative hearing by the applicant must be filed within 21 days of receipt of this written notice. Petitions filed by any persons other than the applicant, and other than those entitled to written notice under section 120.60(3), Florida Statutes, must be filed within 21 days of publication of the notice or within 21 days of receipt of the written notice, whichever occurs first.

File Name: Escambia County BOCC – West Detroit Boulevard Traffic Flow Improvements

File No.: 17-0345394-002-EE

Page 3 of 6

Under section 120.60(3), Florida Statutes, however, any person who has asked the Department for notice of agency action may file a petition within 21 days of receipt of such notice, regardless of the date of publication. The failure to file a petition within the appropriate time period shall constitute a waiver of that person's right to request an administrative determination (hearing) under sections 120.569 and 120.57, Florida Statutes, or to intervene in this proceeding and participate as a party to it. Any subsequent intervention (in a proceeding initiated by another party) will be only at the discretion of the presiding officer upon the filing of a motion in compliance with rule 28-106.205, Florida Administrative Code.

Extension of Time

Under Rule 62-110.106(4), Florida Administrative Code, a person whose substantial interests are affected by the Department's action may also request an extension of time to file a petition for an administrative hearing. The Department may, for good cause shown, grant the request for an extension of time. Requests for extension of time must be filed with the Office of General Counsel of the Department at 3900 Commonwealth Boulevard, Mail Station 35, Tallahassee, Florida 32399-3000, before the applicable deadline for filing a petition for an administrative hearing. A timely request for extension of time shall toll the running of the time period for filing a petition until the request is acted upon.

Mediation

Mediation is not available in this proceeding.

FLAWAC Review

The applicant, or any party within the meaning of Section 373.114(1)(a) or 373.4275, F.S., may also seek appellate review of this order before the Land and Water Adjudicatory Commission under Section 373.114(1) or 373.4275, F.S. Requests for review before the Land and Water Adjudicatory Commission must be filed with the Secretary of the Commission and served on the Department within 20 days from the date when the order is filed with the Clerk of the Department.

Judicial Review

Any party to this action has the right to seek judicial review pursuant to Section 120.68, F.S., by filing a Notice of Appeal pursuant to Rules 9.110 and 9.190, Florida Rules of Appellate Procedure, with the Clerk of the Department in the Office of General Counsel, 3900 Commonwealth Boulevard, M.S. 35, Tallahassee, Florida 32399-3000; and by filing a copy of the Notice of Appeal accompanied by the applicable filing fees with the appropriate District Court of Appeal. The Notice of Appeal must be filed within 30 days from the date this action is filed with the Clerk of the Department.

Executed in Escambia County, Florida.

STATE OF FLORIDA DEPARTMENT
OF ENVIRONMENTAL PROTECTION

Thank you for applying to the Submerged Lands and Environmental Resource Permit Program. If you have any questions regarding this matter, please contact Douglas Sprague at the letterhead address, at 850-595-0579, or at Douglas.sprague@dep.stste.fl.us.

Sincerely,



For Andrew Joslyn
Permitting Program Administrator

AJ:ds

Enclosures:

Section 373.406(6), F.S., 1 page.

Project Drawings and Design Specifications

cc:

Cliff Payne, U.S. Army Corps of Engineers, Lyal.c.payne@usace.army.mil

Holly Millsap, U.S. Army Corps of Engineers, Holly.m.millsap@usace.army.mil

Cori Pietrangelo, Agent, Wetland Sciences Inc., Wetlandsciences@gmail.com

CERTIFICATE OF SERVICE

The undersigned duly designated deputy clerk hereby certifies that this determination, including all copies, was mailed before the close of business on August 1, 2016, to the above listed persons.

FILING AND ACKNOWLEDGMENT

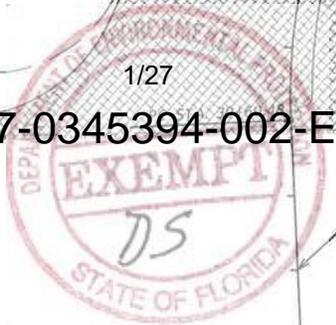
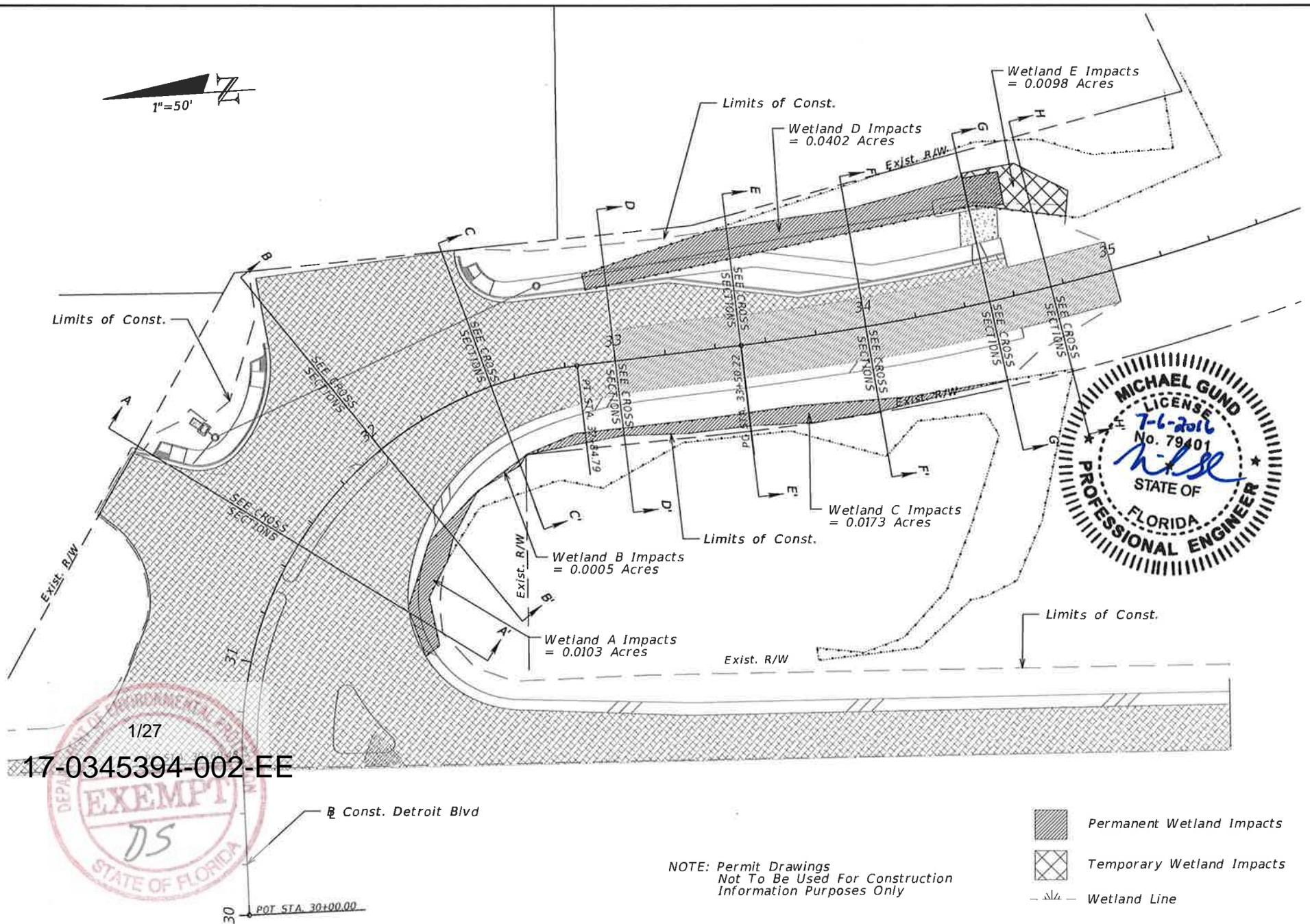
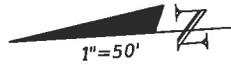
FILED, on this date, under 120.52(7) of the Florida Statutes, with the designated Department Clerk, receipt of which is hereby acknowledged.



August 1, 2016

Clerk

Date



NOTE: Permit Drawings
Not To Be Used For Construction
Information Purposes Only

- Permanent Wetland Impacts
- Temporary Wetland Impacts
- Wetland Line

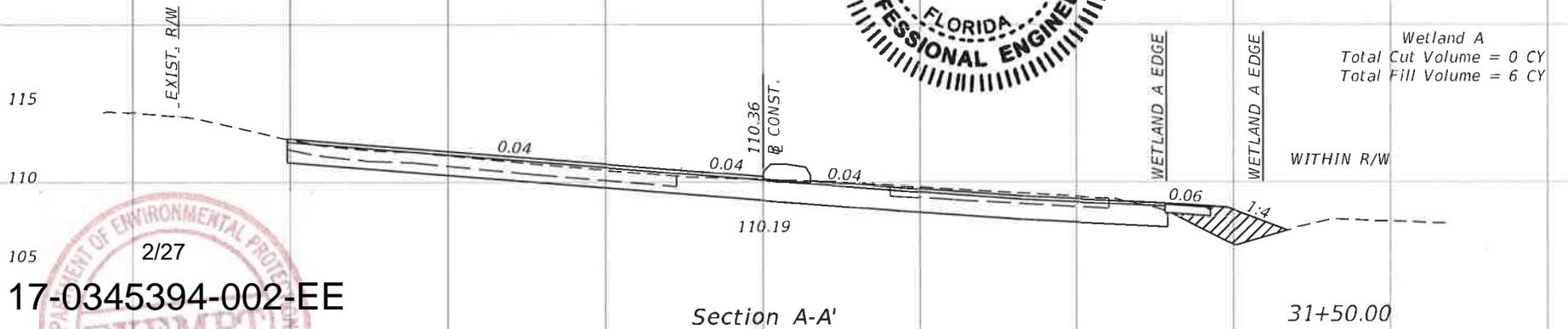
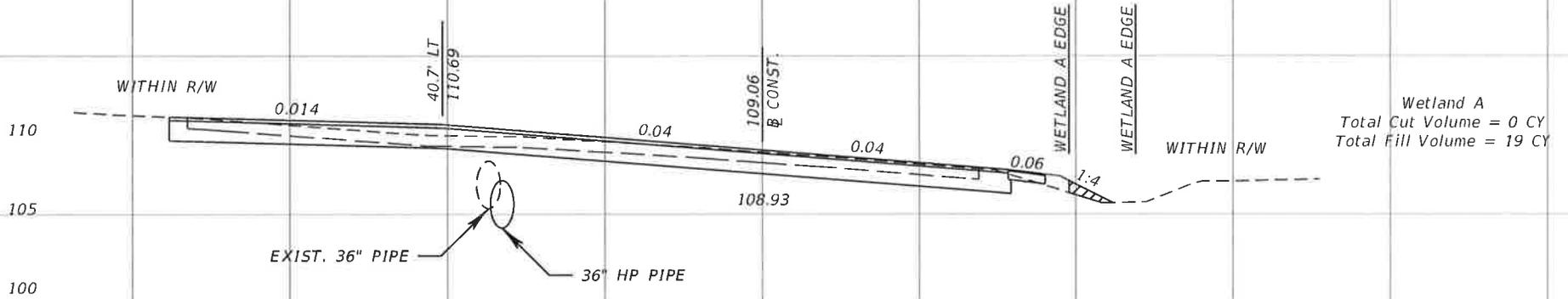
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Hatch Mott MacDonald
220 West Garden Street, Suite 700
Pensacola, Florida 32502
(850) 484-6011
Certificate of Authorization #00000155
E.O.R.: Michael Gund, P.E. #79401

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

WETLAND IMPACTS	SHEET NO.
----------------------------	--------------

 Fill Wetland Impacts
 Cut Wetland Impacts



SCALE
 1" = 20' Horizontal
 1" = 10' Vertical

2/27
 17-0345394-002-EE


REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Hatch Mott MacDonald
 220 West Garden Street, Suite 700
 Pensacola, Florida 32502
 (850) 484-6011
 Certificate of Authorization #00000155
 E.O.R.: Michael Gund, P.E. #79401

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

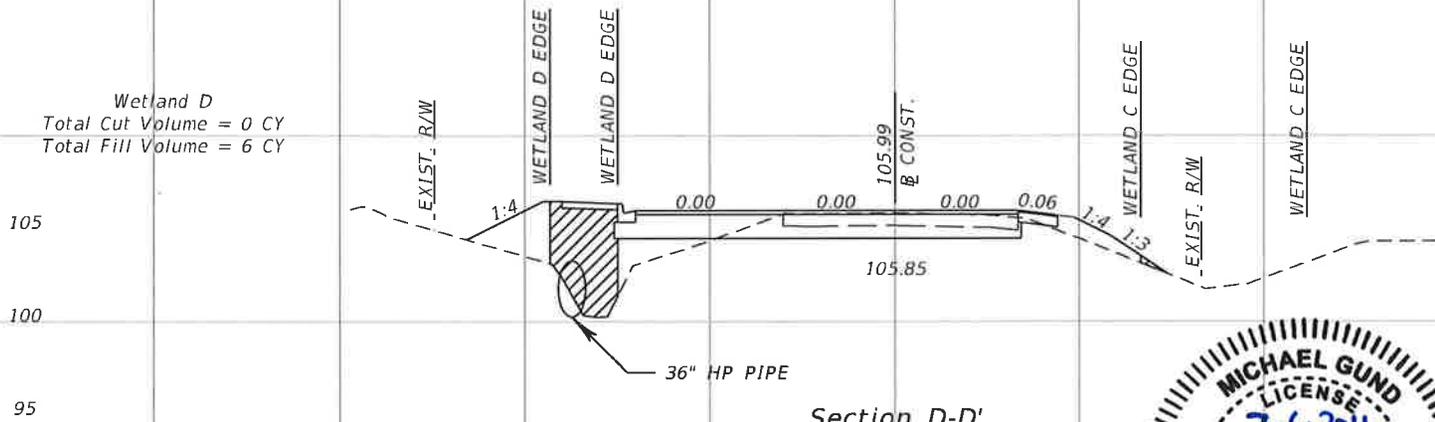
**CROSS
SECTIONS**

SHEET
NO.

 Fill Wetland Impacts
 Cut Wetland Impacts

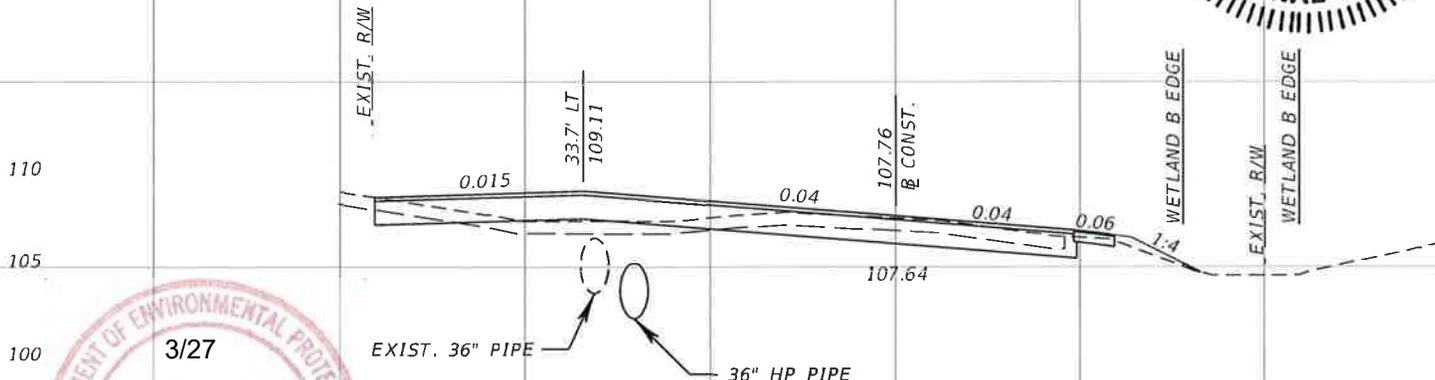
Wetland D
 Total Cut Volume = 0 CY
 Total Fill Volume = 6 CY

Wetland C
 Total Cut Volume = 0 CY
 Total Fill Volume = 1 CY



33+00.00

Wetland B
 Total Cut Volume = 0 CY
 Total Fill Volume = 0 CY



32+50.00

SCALE
 1" = 20' Horizontal
 1" = 10' Vertical

3/27
 17-0345394-002-EE



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Hatch Mott MacDonald
 220 West Garden Street, Suite 700
 Pensacola, Florida 32502
 (850) 484-6011
 Certificate of Authorization #00000155
 E.O.R.: Michael Gund, P.E. #79401

STATE OF FLORIDA
 DEPARTMENT OF TRANSPORTATION

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

CROSS
 SECTIONS

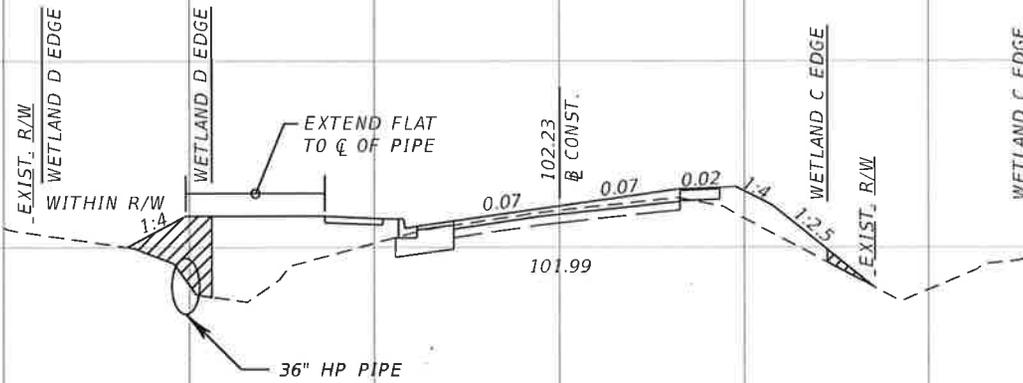
SHEET NO.

 Fill Wetland Impacts
 Cut Wetland Impacts

Wetland D
 Total Cut Volume = 0 CY
 Total Fill Volume = 51 CY

Wetland C
 Total Cut Volume = 0 CY
 Total Fill Volume = 6 CY

100
95
90



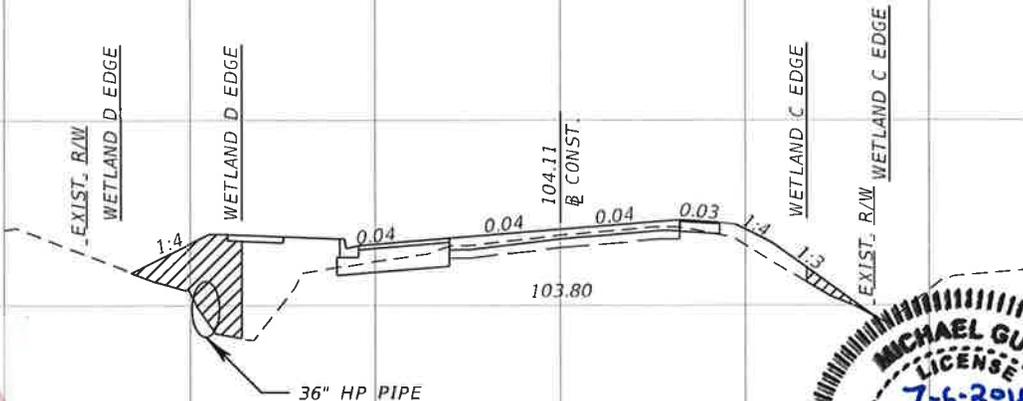
Section F-F'

34+00.00

Wetland D
 Total Cut Volume = 0 CY
 Total Fill Volume = 66 CY

Wetland C
 Total Cut Volume = 0 CY
 Total Fill Volume = 3 CY

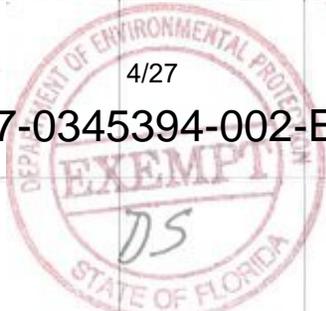
100
95
90



Section E-E'

33+50.00

4/27
 17-0345394-002-EE



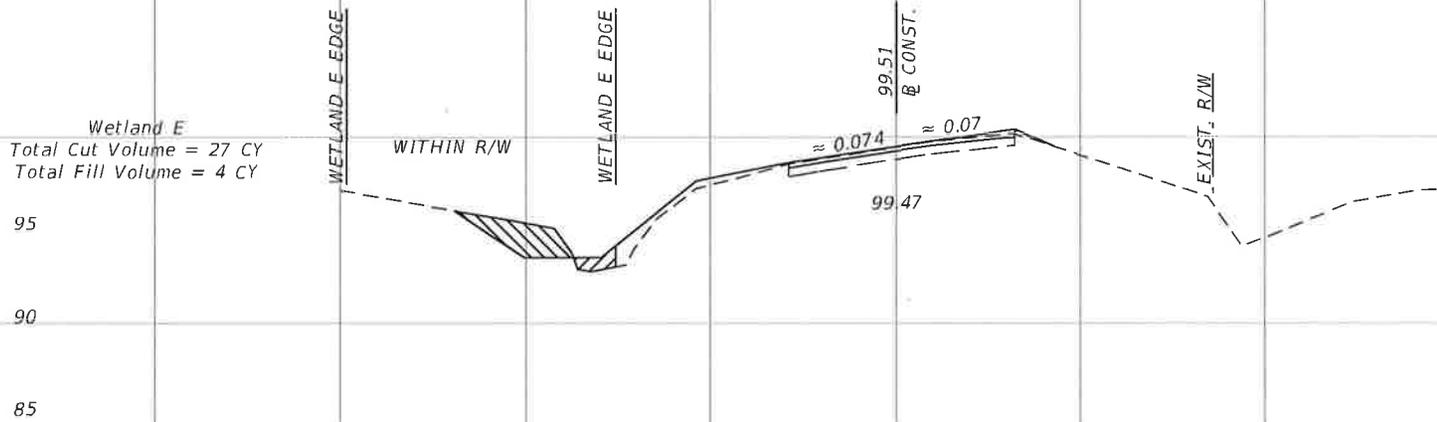
SCALE
 1" = 20' Horizontal
 1" = 10' Vertical

80 60 40 20 0 20 40 60 80

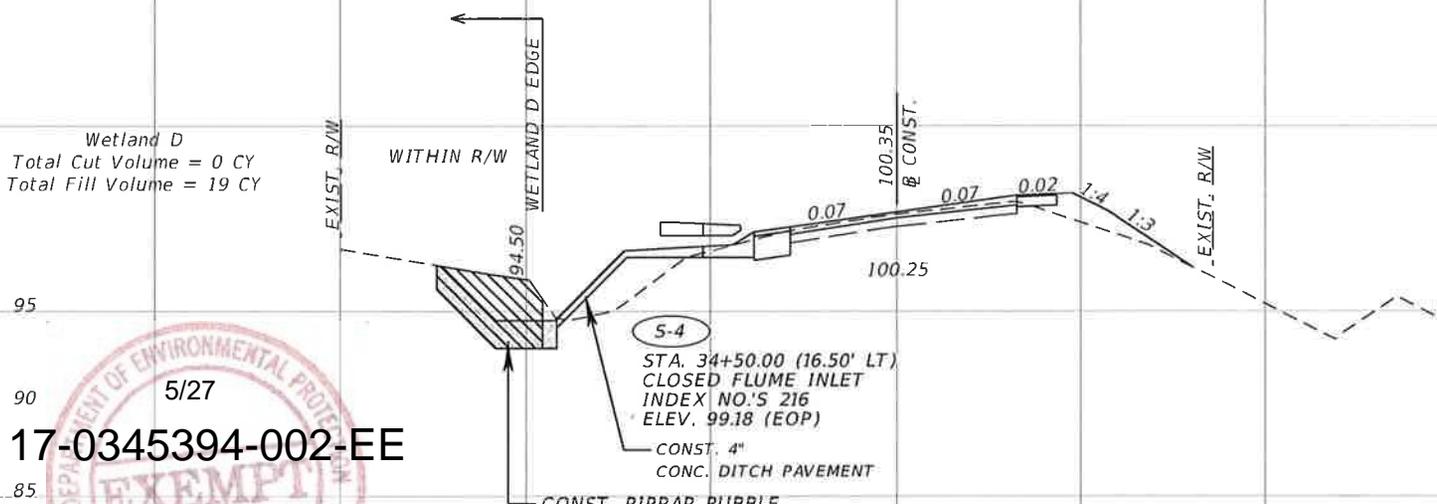
REVISIONS				Hatch Mott MacDonald 220 West Garden Street, Suite 700 Pensacola, Florida 32502 (850) 484-6011 Certificate of Authorization #00000155 E.O.R.: Michael Gund, P.E. #79401	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			CROSS SECTIONS	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID		
					DETROIT	ESCAMBIA			

 Fill Wetland Impacts
 Cut Wetland Impacts

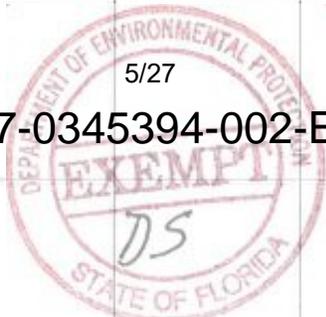
Wetland E
 Total Cut Volume = 27 CY
 Total Fill Volume = 4 CY



Wetland D
 Total Cut Volume = 0 CY
 Total Fill Volume = 19 CY



5/27
 17-0345394-002-EE



SCALE
 1" = 20' Horizontal
 1" = 10' Vertical

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Hatch Mott MacDonald
 220 West Garden Street, Suite 700
 Pensacola, Florida 32502
 (850) 484-6011
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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

**CROSS
SECTIONS**

SHEET
NO.



**BOARD OF COUNTY COMMISSIONERS
 ESCAMBIA COUNTY, FLORIDA
 PUBLIC WORKS BUREAU
 ENGINEERING DIVISION**

**CONSTRUCTION PLANS FOR
 DETROIT BOULEVARD INTERSECTION
 IMPROVEMENTS AT PINE FOREST ROAD**

INDEX OF ROADWAY PLANS

SHEET NO.	SHEET DESCRIPTION
1	KEY SHEET
2	GENERAL NOTES
3	SUMMARY OF PAY ITEMS
4	TYPICAL SECTIONS
5	MISC. DETAILS
6	PROJECT LAYOUT
7	DEMO/UTILITY ADJUSTMENT
8	ROADWAY PLAN
9	ROADWAY PROFILE
10	INTERSECTION DETAILS
11-16	CROSS SECTIONS
17	TRAFFIC CONTROL NOTES
18-20	TRAFFIC CONTROL PLAN
21	SIGNING AND PAVEMENT MARKING PLAN

ESCAMBIA COUNTY COMMISSIONERS

WILSON ROBERTSON, VICE CHAIRMAN	DISTRICT 1
DOUG UNDERHILL	DISTRICT 2
LUMON MAY	DISTRICT 3
GROVER C. ROBINSON, IV, CHAIRMAN	DISTRICT 4
STEVEN BARRY	DISTRICT 5

GOVERNING STANDARDS AND SPECIFICATIONS:

ALL ROADWAY CONSTRUCTION SHALL COMPLY WITH:

THE ESCAMBIA COUNTY TECHNICAL SPECIFICATIONS, LATEST EDITION, AND ALL INTERIM STANDARDS THAT APPLY TO THESE EDITIONS.

FLORIDA DEPARTMENT OF TRANSPORTATION, FY 2016-2017 DESIGN STANDARDS AND REVISED INDEX DRAWINGS AS APPENDED HEREIN, AND JULY 2016 STANDARD SPECIFICATIONS FOR ROADWAY CONSTRUCTION AS AMENDED BY CONTRACT DOCUMENTS.

FOR DESIGN STANDARDS CLICK ON THE "DESIGN STANDARDS" LINK AT THE FOLLOWING WEB SITE:

[HTTP://WWW.DOT.STATE.FL.US/RDDSIGN/](http://www.dot.state.fl.us/rddesign/)

FOR THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION CLICK ON THE "SPECIFICATIONS" LINK AT THE FOLLOWING WEB SITE:

[HTTP://WWW.DOT.STATE.FL.US/SPECIFICATIONSOFFICE/](http://www.dot.state.fl.us/specificationsoffice/)

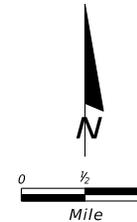
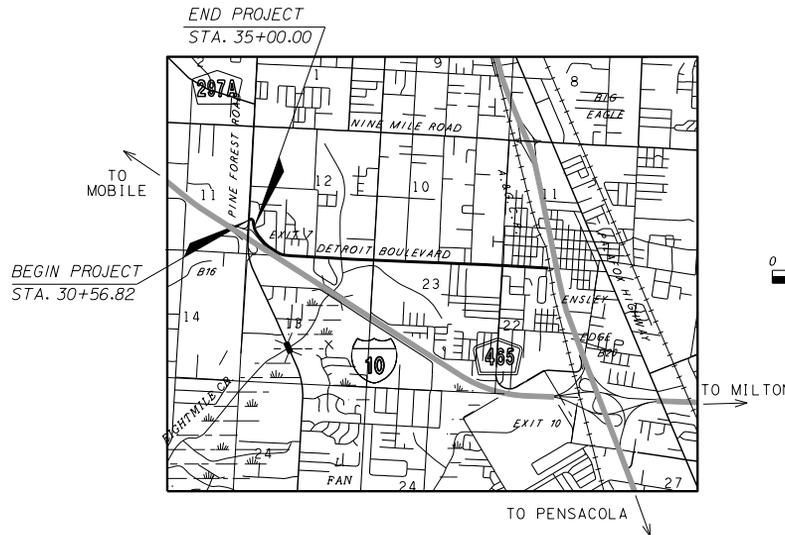
PLANS PREPARED BY:

HATCH MOTT MACDONALD
 220 WEST GARDEN STREET, SUITE 700
 PENSACOLA, FLORIDA 32502
 (850) 484-6011

CERTIFICATE OF AUTHORIZATION #00000155
 E.O.R.: MICHAEL GUND, P.E. #79401

100% PLANS

JUNE 2016



VICINITY MAP

PROJECT LENGTH IS BASED ON $\frac{1}{2}$ OF CONSTRUCTION

	LINEAR FEET	MILES
ROADWAY	443.18	0.084
BRIDGES	00.00	0.000
NET LENGTH OF PROJECT	443.18	0.084
EXCEPTIONS	NA	NA
GROSS LENGTH OF PROJECT	443.18	0.084

ESCAMBIA COUNTY PROJECT MANAGER: ELIZABETH BUSH

DETROIT BOULEVARD INTERSECTION IMPROVEMENTS AT PINE FOREST ROAD

SHEET NO.

1

GENERAL NOTES:

1. THE CONTRACTORS SHALL NOTIFY THE COUNTY DESIGN ENGINEER OR DESIGNEE 48 HOURS PRIOR TO CONSTRUCTION.
2. ALL CONDITIONS AND STIPULATIONS OF THE CONSTRUCTION PERMITS AND THE APPROVALS ISSUED BY THE ESCAMBIA COUNTY ENGINEER SHALL BE COMPLIED WITH IN EVERY DETAIL.
3. THE LOCATION SHOWN FOR EXISTING UNDERGROUND UTILITIES IS APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK IN EACH AREA. THE CONTRACTOR AGREES TO BE COMPLETELY RESPONSIBLE FOR ALL DAMAGES WHICH MIGHT OCCUR BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ALL UTILITIES.
4. ALL ROADS DAMAGED BY CONSTRUCTION OPERATIONS ARE TO BE PATCHED OR RECONSTRUCTED AS DIRECTED BY THE COUNTY ENGINEER OR DESIGNEE. THE CONTRACTOR SHALL TAKE VIDEO DOCUMENTATION OF THE SITE PRIOR TO CONSTRUCTION.
5. THE CONTRACTOR SHALL TAKE STEPS NECESSARY TO PREVENT EROSION AND ANY OFF SITE SEDIMENT TRANSPORT RESULTING FROM INCREASED RUNOFF DURING CONSTRUCTION BY PROVIDING SILT FENCE, AS REQUIRED BY THE FLORIDA STORMWATER, EROSION, AND SEDIMENT CONTROL INSPECTOR'S MANUAL, 2008 EDITION, OR AS INDICATED ON THE PLANS. ALL EROSION CONTROL MEASURES SHALL REMAIN IN PLACE UNTIL ASSOCIATED DISTURBED AREAS ARE STABILIZED AS TO REDUCE SEDIMENT RUNOFF, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR DESIGNEE.
6. ANY NECESSARY PERMITS NOT INCLUDED IN THE CONTRACT DOCUMENTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
7. THE CONTRACTOR IS CAUTIONED TO VISIT THE SITE AND FAMILIARIZE HIMSELF WITH THE PROJECT PRIOR TO BIDDING AND/OR CONSTRUCTION.
8. IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO PRESERVE OR RELOCATE ALL BENCHMARKS AS NEEDED DURING CONSTRUCTION. ANY PUBLIC OR PRIVATE LAND CORNER MONUMENT SHALL BE PROTECTED. IF A PUBLIC OR PRIVATE CORNER MONUMENT IS IN DANGER OF BEING DESTROYED AND HAS NOT BEEN PROPERLY REFERENCED, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OR DESIGNEE IMMEDIATELY. ANY ESCAMBIA COUNTY HARN/GPS NETWORK MONUMENT OR BUREAU OF SURVEY AND MAPPING GPS NETWORK MONUMENT WITHIN THE LIMITS OF CONSTRUCTION SHALL BE PROTECTED. IF A HARN/GPS NETWORK MONUMENT OR BUREAU OF SURVEY AND MAPPING GPS NETWORK MONUMENT IS DISTURBED OR DESTROYED THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACEMENT OF THE MONUMENT AND HAVE THE MONUMENT POSITION DETERMINED BY A FLORIDA LICENSED PROFESSIONAL SURVEYOR AND MAPPER USING GUIDELINES AS ESTABLISHED BY NATIONAL GEODETIC SURVEY FOR BLUE BOOKING AND APPROVAL.
9. EXISTING DRAINAGE FEATURES WITHIN CONSTRUCTION LIMITS ARE TO REMAIN UNLESS OTHERWISE NOTED. THE COSTS FOR REMOVAL OF EXISTING DRAINAGE FEATURES ARE TO BE INCLUDED IN CLEARING AND GRUBBING PAY ITEM.
10. THE CONTRACTOR IS TO USE CAUTION WHEN WORKING IN OR AROUND AREAS OF OVERHEAD TRANSMISSION LINES AND UNDERGROUND UTILITIES.
11. THE CONTRACTOR SHALL MATCH EXISTING CONDITIONS AT THE BEGINNING AND END OF CONSTRUCTION AS DIRECTED BY THE COUNTY ENGINEER OR DESIGNEE.
12. EXISTING STREETS AND DRIVES SHALL BE MAINTAINED TO LOCAL TRAFFIC AND PROPERTY OWNERS.
13. ALL ROADWAY CONSTRUCTION SHALL COMPLY WITH THE ESCAMBIA COUNTY TECHNICAL SPECIFICATIONS, LATEST EDITION.
14. ALL MATERIALS, TESTING AND CONSTRUCTION METHODS SHALL CONFORM TO THE ESCAMBIA COUNTY TECHNICAL SPECIFICATIONS, LATEST EDITION. ANY ITEM NOT COVERED UNDER THESE SPECIFICATIONS SHALL BE GOVERNED BY THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.
15. ANY REFERENCE TO FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION, DIVISION 1, GENERAL REQUIREMENTS AND COVENANTS, SHALL BE EXCLUDED AND NOT APPLICABLE TO ANY SPECIFICATION REFERRED HEREIN OR OTHERWISE LISTED IN THESE PLANS OR RELATED DOCUMENTS OR THE ESCAMBIA COUNTY TECHNICAL SPECIFICATIONS.
16. EXISTING STREET AND ROAD NAME SIGNS ON THE PROJECT SHALL BE KEPT VISIBLE AT ALL TIMES FOR THE FACILITATION OF ACCESS BY EMERGENCY VEHICLES. ALL OTHER EXISTING SIGNS THAT CONFLICT WITH CONSTRUCTION OPERATIONS SHALL BE TAKEN DOWN AND STOCKPILED WITHIN THE R/W LIMITS BY THE CONTRACTOR AS DIRECTED BY THE COUNTY ENGINEER OR DESIGNEE. ANY EXISTING SIGNS THAT ARE TO BE RELOCATED AND ARE DAMAGED BEYOND USE BY THE CONTRACTOR SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
17. THE CONTRACTOR SHALL MAINTAIN AT LEAST ONE 10' OPEN LANE AT ALL TIMES. NO OPEN EXCAVATION SHALL REMAIN OVER NIGHT. CONTRACTOR SHALL RESTORE ROAD TO TWO LANES OF TRAFFIC AT THE END OF EACH WORK DAY.
18. ONLY ACCESS TO THE ROADWAY SHOWN IS GUARANTEED BY THE COUNTY. PRIVATE R/W REQUIRED BY THE CONTRACTOR TO FACILITATE CONSTRUCTION SHALL BE ACQUIRED BY THE CONTRACTOR WITH NO ADDITIONAL COMMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE COUNTY ENGINEER OR DESIGNEE PRIOR TO CONSTRUCTION.
19. IF THE EXISTING UTILITY MONUMENTS OR REFERENCE POINTS ARE MISSING OR HAVE BEEN DESTROYED, PLEASE CONTACT:

DANNY SWAIN, PSM ESCAMBIA COUNTY SURVEYOR 3363 WEST PARK PLACE PENSACOLA, FLORIDA 32503 PH (850) 595-3427	DAVID D. GLAZE, PSM PITTMAN, GLAZE AND ASSOCIATES, INC. 5700 N. DAVIS HIGHWAY, SUITE 3 PENSACOLA, FLORIDA 32503 PH (850) 434-6666
--	--
20. VEGETATION ON R/W AND EASEMENTS SHALL BE RESTORED TO ORIGINAL CONDITION UNLESS OTHERWISE NOTED ON THE PLAN SHEETS. COST OF SAID RESTORATION SHALL BE CONSIDERED INCIDENTAL TO OTHER PAY ITEMS.
21. UTILITIES TO REMAIN AND BE PROTECTED DURING CONSTRUCTION. NECESSARY REPAIRS SHALL BE CONSIDERED INCIDENTAL TO OTHER PAY ITEMS AND SHALL BE TO THE SATISFACTION OF UTILITY OWNERS.
22. ALL TREES WITHIN LIMITS OF CONSTRUCTION SHALL BE REMOVED UNLESS OTHERWISE NOTED IN PLANS.

23. MAINTENANCE OF TRAFFIC AS PER FDOT INDEX 600.
24. ALL EXISTING MAILBOXES INTERFERING WITH NEW CONSTRUCTION SHALL BE RELOCATED OR REPLACED BY THE CONTRACTOR IN ACCORDANCE WITH POSTAL REQUIREMENTS. ALL EXISTING BRICK MAILBOXES WITHIN LIMITS OF CONSTRUCTION OR COUNTY RIGHT OF WAY SHALL BE REMOVED AND PLACED ON THE PROPERTY LINE OF THE OWNER. CONTRACTOR SHALL REPLACE EXISTING BRICK MAILBOX WITH APPROVED PLASTIC BREAK AWAY MAILBOX.
25. WHERE UNSUITABLE MATERIAL, AS DEFINED BY THE COUNTY SPECIFICATIONS SECTION 02300.1.3(1), IS ENCOUNTERED IN THE AREAS PROPOSED FOR PAVING, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE COUNTY ENGINEER OR DESIGNEE PRIOR TO ANY EXCAVATION.
26. THE PIPE LENGTHS SHOWN IN THE PLANS DO NOT INCLUDE THE LENGTH OF PIPE THAT MUST BE INSTALLED WITH THE MITERED END SECTION. THEREFORE, ALL PIPES LENGTHS ASSOCIATED WITH MITERED END SECTIONS SHALL BE PAID FOR IN THE UNIT COST OF THE MITERED END SECTION.
27. UTILITY OWNERS SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY CONSTRUCTION SO THAT THE UTILITY OWNER CAN SPOT VERIFY AND/OR EXPOSE THEIR UTILITIES. KNOWN UTILITIES OWNERS INCLUDE:

EMERALD COAST UTILITY AUTHORITY BRANDON KNIGHT 850-698-4609	GULF POWER CHAD SWALLS 850-429-2446	
PENSACOLA ENERGY DIANE MOORE 850-479-5319	COX CABLE TROY YOUNG 850-232-5044	
AT&T FLORIDA STEPHEN KENNINGTON 850-623-3811	TRANSCORE INC JONATHAN BAILEY 850-462-6030	
SOUTHERN LIGHT ANDRU BRAMBLETT 251-662-1170	ESCAMBIA COUNTY TRAFFIC SIGNAL UTILITY SPOTS JOHNNY COX 850-595-3484	

28. ALL WALKWAY PATHS AND INTERSECTIONS SHALL MEET ADA REQUIREMENTS (2010 ADA STANDARDS FOR ACCESSIBLE DESIGN). SIDEWALK CURB RAMPS AND DETECTABLE WARNING MATS SHALL BE INSTALLED IN ACCORDANCE WITH FDOT STANDARD INDEX 304.
29. ALL DRIVEWAYS AND TURNOUTS SHALL BE CONSTRUCTED ACCORDING TO FDOT STANDARD INDEX 515.
30. GRADE ALL AREAS WITHIN THE CONSTRUCTION LIMITS TO PROVIDE UNIFORM CONTOURS AND GRADES SO THAT DRAINAGE FLOW PATTERNS AND GRASS MOWING OPERATIONS ARE NOT HINDERED BY TERRAIN IRREGULARITY. PERFORM THIS WORK REGARDLESS OF WHETHER THE IRREGULARITIES WERE THE RESULT OF CONSTRUCTION OPERATIONS OR EXISTED ORIGINALLY.

17-0345394-002-EE

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Hatch Mott MacDonald
 220 West Garden Street, Suite 700
 Pensacola, Florida 32502 (850) 484-6001
 Engineer of Record: Michael Gund, P.E. #79401



Board of County Commissioners Escambia County, Florida		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

GENERAL NOTES

SHEET NO.
2

SUMMARY OF PAY ITEMS

SECTION	CATEGORY	ITEM DESCRIPTION	UNIT	QUANTITY	
				P	F
01000-	INSURANCE				
01100-	00100	PERFORMANCE BOND	PER \$1000	279	
02000-	EQUIPMENT				
02100-	00101	MOBILIZATION, 0-15 MILES	EA	1	
02100-	00105	DEMobilIZE	EA	1	
03000-	CLEARING				
03100-	00101	CLEARING AND GRUBBING, PER COUNTY SPEC. 2230	AC	0.5	
04000-	EARTHWORK				
04100-	00101	EARTHWORK EXCAVATION BY MACHINE, COUNTY SPECS 2300	CY	1600	
04100-	00103	EARTHWORK EXCAVATE, HAUL, AND INSTALL, ON-SITE	CY	692	
05000-	ASPHALT				
05200-	00102	1" FDOT TYPE FC 9.5 ASPHALT, OVER 1500 SY	SY	4263	
05200-	00114	1" COUNTY SPEC 2500 TYPE SP 9.5 ASPH. CONC. SURFACE, OVER 1500 SY	SY	7923	
05600-	00103	MILL EXISTING ASPHALT, 1.5"-3" THICKNESS, UNDER 1500 SY	SY	548	
05700-	00109	REMOVE EXISTING ASPHALT, 3" AVERAGE DEPTH	SY	2598	
05700-	00111	SAW CUT EXISTING ASPHALT	LF	969	
06000-	RDWY PREP				
06200-	00104	6" GRADED AGG. BASE "MIN. LBR 100", COUNTY SPEC 2400, > THAN 1000 SY	SY	3980	
06200-	00108	10" GRADED AGG. BASE "MIN. LBR 100", COUNTY SPEC 2400, > THAN 1000 SY	SY	3649	
07000-	TRAFFIC				
07200-	00106	TEMP. 6" SOLID STRIPE, WHITE OR YELLOW	LF	1725	
07200-	00109	TEMP. 6" 2-4 SKIP LINE STRIP, WHITE OR YELLOW	LF	160	
07200-	00110	TEMP. 6" DOUBLE SOLID STRIPE, WHITE OR YELLOW	LF	385	
07200-	00111	TEMP. 8" STRIPE, WHITE OR YELLOW	LF	850	
07200-	00113	TEMP. 18" STRIPE, WHITE OR YELLOW	LF	445	
07200-	00115	TEMP. WHITE PEDESTRIAN CROSSWALK	LF	227	
07200-	00117	TEMP. 24" STOP BAR	LF	115	
07200-	00131	TEMP. DIRECTIONAL ARROW, SINGLE HEAD (TURN LEFT, RIGHT) 16 SF	EA	9	
07200-	00140	TEMP. REFLECTIVE PAVEMENT MARKERS	EA	100	
07300-	00106	THERMOPLASTIC 6" SOLID STRIPE, WHITE OR YELLOW	LF	1725	
07300-	00109	THERMOPLASTIC 6" 2-4 SKIP STRIPE, WHITE OR YELLOW	LF	160	
07300-	00110	THERMOPLASTIC 6" DOUBLE SOLID STRIPE, WHITE OR YELLOW	LF	385	
07300-	00111	THERMOPLASTIC 8" STRIPE, WHITE OR YELLOW	LF	850	
07300-	00113	THERMOPLASTIC 18" WHITE OR YELLOW SOLID STRIPE	LF	445	
07300-	00115	THERMOPLASTIC WHITE PEDESTRIAN CROSSWALK	LF	227	
07300-	00117	THERMOPLASTIC STOP BAR	LF	115	
07300-	00128	THERMOPLASTIC "ONLY" PAVEMENT MESSAGE	EA	3	
07300-	00132	THERMOPLASTIC DIR. ARROW, SIGNLE HEAD (TURN LEFT, RIGHT) 16 SF	EA	9	
07300-	00147	REFLECTIVE PAVEMENT MARKERS	EA	100	
07400-	00101	RELOCATE TRAFFIC SIGNS	EA	9	
07400-	00110	KEEP RIGHT SIGN, R4-7	EA	1	
07600-	00101	DEVELOP AND PROVIDE AN APPROVED NOT SAFETY TRAFFIC PLAN BOTH MAP AND WRITTEN TYPE BY A CERTIFIED WORK ZONE SAFETY TRAFFIC SUPERVISOR	EA	1	
07900-	00106	9/27 MOT (FOR CONTRACTS \$250,000 TO \$300,000)	LS	1	
08000-	CONCRETE				
08100-	00109	6" FIBER REINFORCED CONCRETE DITCH PAVING W/O WEEP HOLES, COUNTY DETAIL, MIN 3" THICK	LF	105	
08100-	00109	6" FIBER REINFORCED CONCRETE FLUME	LF	321	
08300-	00105	6" FIBER REINFORCED CONCRETE SIDEWALK, LESS THAN 500 LF	LF	251	
08300-	00111	CONSTRAINT CURB RAMP (APPROVED MAT, COLOR INCLUDED) FDOT INDEX 304	EA	3	
08400-	00101	FIBER REINF. CONCRETE DITCH PAVING W/O WEEP HOLES, COUNTY DETAIL, MIN 3" THICK	SY	23	
08400-	00103	FIBER REINF. CONCRETE FLUME	SY	2	
08500-	00101	SAWCUT EXISTING CONCRETE	LF	10	
08500-	00103	REMOVE EXISTING CONCRETE, 4" THICK	SY	39	
08500-	00106	REMOVE CURB	LF	145	
08600-	00101	MISC. CONCRETE	CY	22	

SUMMARY OF PAY ITEMS

SECTION	CATEGORY	ITEM DESCRIPTION	UNIT	QUANTITY	
				P	F
09000-	DRAINAGE				
09100-	00105	DITCH BOTTOM INLET, TYPE C, 0'-6" DEPTH	EA	1	
09100-	00501	SINGLE BARREL FLUME	EA	1	
09100-	00801	STORM MANHOLE, 0'-6" DEPTH	EA	2	
09100-	00912	TIE TO EXISTING INLETS, PIPE, MANHOLE	EA	1	
09200-	00211	24" HDPE DW PIPE, 0'-6" DEPTH	LF	17	
09200-	00215	36" HDPE DW PIPE, 0'-6" DEPTH	LF	296	
09300-	00405	36" RCP SIDE DRAIN MES, FDOT INDEX 273, 280	EA	1	
09500-	00105	PIPE REMOVAL, 30" AND LARGER	LF	110	
09500-	00109	REMOVE EXISTING CONCRETE HEADWALL, 30" AND LARGER	EA	1	
13000-	SWPP				
13100-	00102	CENTPEDE SOD, STAKED, OVER 1000 SY	SY	1141	
13200-	00101	18" DEPTH RIP RAP RUBBLE W/ 4" BEDDING STONE AND GEOTEXTILE	SY	21	
13300-	00102	SILT FENCE TYPE III, OVER 500 LF	LF	800	
13300-	00110	ESTABLISH, QUANTIFY, AND SUBMIT AN APPROVED EROSION CONTROL PLAN PREPARED BY A CERTIFIED TECHNICIAN	EA	1	
17000-	MISC.				
17100-	00102	JOBSITE BOARD FOR POSTING PROJECT INFORMATION, PERMITS, ETC.	EA	1	

PAY ITEM NOTES:

ITEM 05200-00114:

PG 76-22 BINDER (5307 SY)

PG 82-22 BINDER (2616 SY)

ITEM 06200-00104:

PAY ITEM IS FOR 6.5" GRADED AGGREGATE BASE

ITEM 06200-00108:

PAY ITEM IS FOR 9" GRADED AGGREGATE BASE

ITEM 07400-00101:

QUANTITY INCLUDES TRAFFIC SIGN REMOVAL (4 EA).

ITEM 07400-00110:

PAY ITEM IS FOR "BEGIN RIGHT TURN LANE/ YIELD FOR BIKES" (R4-4).

ITEM 08600-00101:

INCLUDES ALL COSTS FOR THE CONSTRUCTION OF TRAFFIC SEPARATORS.

ITEM 09100-00501:

PAYMENT DOES NOT INCLUDE CONCRETE DITCH PAVING. CONCRETE DITCH PAVEMENT SHALL BE PAID FOR WITH ITEM 08400-00101.

ITEM 09200-XXXXX:

PAY ITEM IS FOR HP POLYPROPYLENE PIPE.

17-0345394-002-EE

REVISIONS

DATE	DESCRIPTION	DATE	DESCRIPTION



Hatch Mott MacDonald
220 West Garden Street, Suite 700
Pensacola, Florida 32502 (850) 484-6001
Engineer of Record: Michael Gund, P.E. #79401



Board of County Commissioners

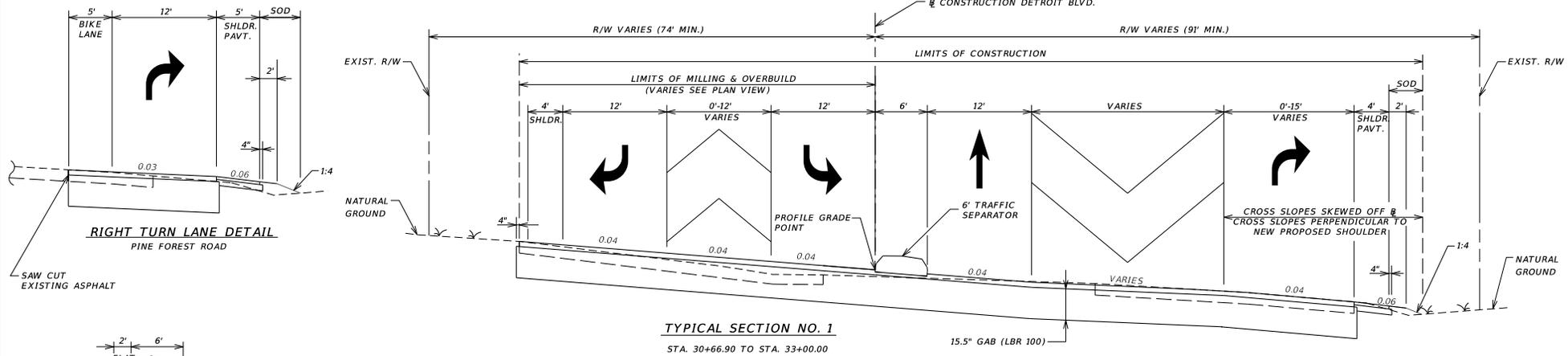
Escambia County, Florida

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

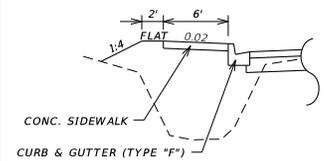
SUMMARY OF PAY ITEMS

SHEET NO.

3



TYPICAL SECTION NO. 1
STA. 30+66.90 TO STA. 33+00.00



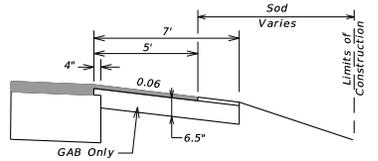
RIGHT TURN LANE DETAIL
PINE FOREST ROAD



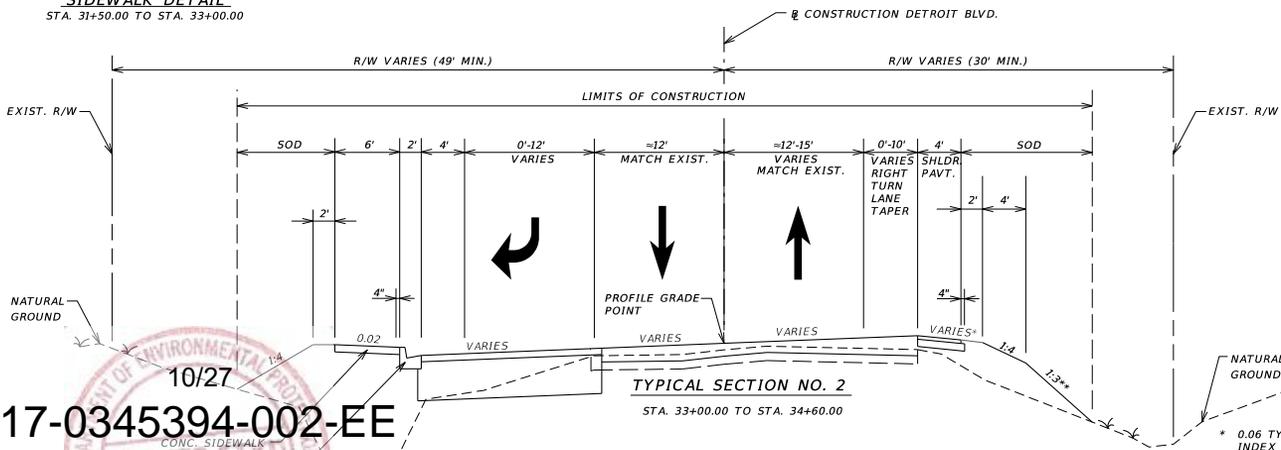
SIDEWALK DETAIL
STA. 31+50.00 TO STA. 33+00.00

NEW CONSTRUCTION
GRADED AGGREGATE BASE ONLY (15.5') WITHOUT STABILIZATION
TYPE SP STRUCTURAL COURSE (1.5") (PG 76-22) PMA
AND FRICTION COURSE FC-9.5 (1") (PG 76-22) PMA

SHOULDER PAVEMENT
GRADED AGGREGATE BASE ONLY (6.5') WITHOUT STABILIZATION
AND FRICTION COURSE FC-9.5 (1") (PG 76-22) PMA
(SEE SHOULDER PAVEMENT DETAIL)



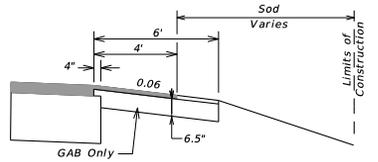
Shoulder Pavement Detail
PINE FOREST RD
STA. 0+70.60 TO STA. 2+84.10



TYPICAL SECTION NO. 2
STA. 33+00.00 TO STA. 34+60.00

CROSS SLOPE CORRECTION
MILLING FOR DEPTH
MILL EXISTING ASPHALT PAVEMENT FOR DEPTH (2.5")
OVERBUILD FOR SLOPE
TYPE SP STRUCTURAL COURSE (PG 82-22) PMA
THICKNESS VARIES (0" TO 4.8")

SHOULDER PAVEMENT
GRADED AGGREGATE BASE ONLY (6.5') WITHOUT STABILIZATION
AND FRICTION COURSE FC-9.5 (1") (PG 76-22) PMA
(SEE SHOULDER PAVEMENT DETAIL)



Shoulder Pavement Detail
DETROIT BLVD
STA. 31+14.64 TO STA. 34+60.00

* 0.06 TYP, VARIES PER FDOT STANDARD INDEX 510 THROUGH SUPERELEVATION TRANSITIONS
** UP TO 1:2 AS NEEDED TO TIE W/N R/W

Traffic Data
Current year = 2014 AADT = 6,500
K = 9.00% D = 55.8% T = 3.7% (24 hour)
Design Hour T = 1.86%
Design Speed = 40 mph

17-0345394-002-EE



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Hatch Mott MacDonald
220 West Garden Street, Suite 700
Pensacola, Florida 32502 (850) 484-6001
Engineer of Record: Michael Gund, P.E. #79401



Board of County Commissioners Escambia County, Florida		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

TYPICAL SECTIONS

SHEET NO.
4

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL EROSION AND SEDIMENT CONTROL PRACTICES TO BE INSTALLED PRIOR TO ANY MAJOR SOIL DISTURBANCE, OR IN THEIR PROPER SEQUENCE, AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
2. ANY DISTURBED AREAS THAT WILL BE LEFT EXPOSED MORE THAN 30 DAYS, AND NOT SUBJECT TO CONSTRUCTION TRAFFIC, WILL IMMEDIATELY RECEIVE A TEMPORARY SEEDING. IF THE SEASON PREVENTS THE ESTABLISHMENT OF A TEMPORARY COVER, THE DISTURBED AREAS WILL BE MULCHED WITH STRAW OR EQUIVALENT MATERIAL, AT A RATE OF TWO (2) TONS PER ACRE, ACCORDING TO STATE STANDARDS.
3. PERMANENT VEGETATION TO BE SEED OR SOODED ON ALL EXPOSED AREAS WITHIN TEN (10) DAYS AFTER GRADING. MULCH TO BE USED AS NECESSARY FOR PROTECTION UNTIL SEEDING IS ESTABLISHED.
4. ALL WORK AND MATERIALS TO BE IN ACCORDANCE WITH THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION, SECTIONS 104, 570, 575 AND 980 TO 986.
5. A BITUMINOUS CONCRETE BASE COURSE WILL BE APPLIED IMMEDIATELY FOLLOWING ROUGH GRADING AND INSTALLATION OF IMPROVEMENTS IN ORDER TO STABILIZE STREETS, ROADS, DRIVEWAYS AND PARKING AREAS. IN AREAS WHERE NO UTILITIES ARE PRESENT, THE BITUMINOUS CONCRETE BASE SHALL BE INSTALLED WITHIN 15 DAYS OF THE PRELIMINARY GRADING.
6. IMMEDIATELY FOLLOWING INITIAL DISTURBANCE OR ROUGH GRADING, ALL CRITICAL AREAS SUBJECT TO EROSION (I.E. STEEP SLOPES AND ROADWAY EMBANKMENTS) WILL RECEIVE A TEMPORARY SEEDING IN COMBINATION WITH STRAW MULCH OR A SUITABLE EQUIVALENT, AT A THICKNESS OF TWO (2) TO FOUR (4) INCHES MIXED WITH THE TOP TWO (2) INCHES OF SOIL, ACCORDING TO STATE STANDARDS.
7. ANY STEEP SLOPES RECEIVING PIPELINE INSTALLATION WILL BE BACKFILLED AND STABILIZED DAILY, AS THE INSTALLATION PROCEEDS (I.E. SLOPES GREATER THAN 3:1).
8. UNFILTERED DEWATERING IS NOT PERMITTED. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS DURING ALL DEWATERING OPERATIONS TO MINIMIZE SEDIMENT TRANSFER.
9. SHOULD THE CONTROL OF DUST AT THE SITE BE NECESSARY, THE SITE WILL BE SPRINKLED UNTIL THE SURFACE IS WET. TEMPORARY VEGETATION COVER SHALL BE ESTABLISHED OR MULCH SHALL BE APPLIED IN ACCORDANCE WITH STATE STANDARDS FOR EROSION CONTROL.
10. ALL SOIL WASHED, DROPPED, SPILLED OR TRACKED OUTSIDE THE LIMIT OF DISTURBANCE OR ONTO PUBLIC RIGHTS-OF-WAY WILL BE REMOVED IMMEDIATELY.
11. ALL SOIL STOCKPILES ARE TO BE TEMPORARILY STABILIZED IN ACCORDANCE WITH SOIL EROSION AND SEDIMENT CONTROL NOTE NUMBER 2 (ABOVE).
12. ALL SEDIMENTATION STRUCTURES SHALL BE INSPECTED AND MAINTAINED REGULARLY.
13. THE CONTRACTOR SHALL PREPARE A PLAN FOR THE PROPER DEWATERING AND DOWNSTREAM SILTATION PROTECTION OF EACH STREAM CROSSING PRIOR TO EXCAVATING THE STREAM BED. PLAN SHALL BE FORWARDED TO THE ENGINEER FOR APPROVAL. THE ENGINEER SHALL BE NOTIFIED FOR INSPECTION PRIOR TO EACH STREAM CROSSING CONSTRUCTION.
14. ANY AREAS USED FOR THE CONTRACTOR'S STAGING, INCLUDING BUT NOT LIMITED TO, TEMPORARY STORAGE OF STOCKPILED MATERIALS (E.G. CRUSHED STONE, QUARRY PROCESS STONE, SELECT FILL, EXCAVATED MATERIALS, ETC.), SHALL BE EXTENSIVELY PROTECTED BY A SILT FENCE ALONG THE LOW ELEVATION SIDE TO CONTROL SEDIMENT RUNOFF.

* WHERE APPLICABLE

TEMPORARY SEEDING DETAILS

SEED BED PREPARATION
SOIL TO BE THOROUGHLY PULVERIZED BY DISK-HARROWING AND BE LOOSE AND REASONABLY SMOOTH. APPLY FERTILIZER AT A RATE OF 260 LBS./ACRE OF 16-16-16 OR EQUIVALENT. APPLY DOLOMITIC LIMESTONE AT A RATE OF 800 TO 1000 LBS./ACRE TO PROVIDE A SOIL pH OF 5.5 TO 6.5. LIME & FERTILIZER TO BE WORKED INTO THE TOPSOIL TO A DEPTH OF 4". ADD SANDY LOAM TOPSOIL TO A MINIMUM OF TWO (2) INCHES WHERE DIRECTED BY ENGINEER.

SEED MIXTURE
CONSISTING OF ANNUAL RYE (LOLIUM MULTIFLORUM) AT A RATE OF 174 LBS./ACRE.

PERMANENT SEEDING DETAILS

SEED BED PREPARATION
SOIL TO BE THOROUGHLY PULVERIZED BY DISK-HARROWING AND BE LOOSE AND REASONABLY SMOOTH. APPLY FERTILIZER AT A RATE OF 260 LBS./ACRE OR EQUIVALENT. APPLY DOLOMITIC LIMESTONE AT A RATE OF 800 TO 1000 LBS./ACRE TO PROVIDE A SOIL pH OF 5.5 TO 6.5. LIME & FERTILIZER TO BE WORKED INTO THE TOPSOIL TO A DEPTH OF 4".

SEED MIXTURE CONSISTING OF	RATE	PURITY	GERMINATION
ARGENTINE BAHIA	260 LBS./AC.	95%	95%
PENSACOLA BAHIA	260 LBS./AC.	95%	40%(MIN.)-80%(TOTAL)

SODDING

SOD SHALL BE WELL ROOT MATED CENTIFERE OR BAHIA GRASS OR SHALL BE LIKE IN KIND TO EXISTING AS DIRECTED BY ENGINEER. COMMERCIALY CUT TO A MINIMUM DIMENSION OF 12" x 24" A MAXIMUM OF 22 HOURS PRIOR TO PLACEMENT. SOD SHALL BE LIVE, FRESH, AND UNHARMED, REASONABLY FREE OF WEEDS AND OTHER GRASSES, WITH A HEAVY SOIL MAT ADHERING TO THE ROOT SYSTEM. SOD SHALL BE GROWN, CUT, AND SUPPLIED BY A STATE CERTIFIED GROWER.

TRAFFIC CONTROL STANDARDS

1. CONSTRUCTION TRAFFIC SHALL BE RESTRICTED TO ONSITE ACCESS BY MEANS SO DESIGNATED BY THE ENGINEER, POLICE /SHERIFF DEPARTMENT, BAY COUNTY, AND/OR THE FLORIDA DEPARTMENT OF TRANSPORTATION.
2. TRAFFIC DURING WET WEATHER SHALL BE MINIMIZED AND APPROPRIATE ROADWAY AND SITE CLEAN-UP SHALL BE PROVIDED BY THE CONTRACTOR AS SOON AS WEATHER CONDITIONS PERMIT.

11/12/27

1. ALL DAMAGED ROOTS ARE TO BE EXPOSED TO SOUND TRIMMING AND SEVERED CLEANLY. (NOT TORN) ROOTS SHALL BE PRUNED TO A MINIMUM OF 18 INCHES BELOW THE EXISTING GRADE OR TO THE DEPTH OF DISTURBANCE IF LESS THAN 18 INCHES FROM THE EXISTING GRADE.
2. THE LIMIT OF DISTURBANCE SHALL BE ESTABLISHED BY THE ENGINEER IN ACCORDANCE WITH STATE STANDARDS.
3. FOR TREES TO BE REMOVED, TRANSPORTATION LEADING BY HEAVY EQUIPMENT SHOULD HAPPEN UNDER THE DIRECTION OF PROTECTED TREES TO BE LEFT ON THE SITE. STORAGE OF HEAVY EQUIPMENT SHOULD NOT OCCUR UNDER THE DRIFLINE OF PROTECTED TREES OR SOIL.

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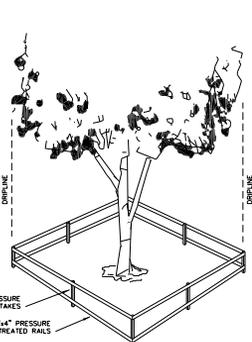
11/12/27

11/12/27

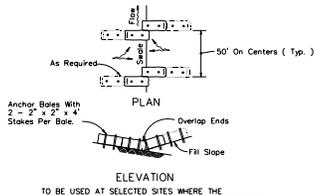
11/12/27

PROPOSED SEQUENCE OF CONSTRUCTION

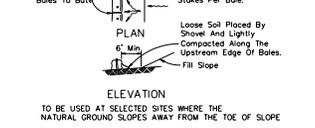
- THIS PROJECT SHALL CONSIST OF THE CONSTRUCTION OF SANITARY SEWERS OR WATER MAINS BY THE BAY COUNTY OR ITS CONTRACTOR WITHIN PUBLIC RIGHTS-OF-WAY. THE CONSTRUCTION SHOULD PROCEED IN THE FOLLOWING MANNER:
1. INSTALLATION OF ALL SEDIMENT AND EROSION CONTROL DEVICES THAT CAN BE PLACED PRIOR TO ANY MAJOR SOIL DISTURBANCES.
 2. CLEAR AND REMOVE ALL EXISTING VEGETATION IN THOSE AREAS WHERE NECESSARY. ALL REMAINING VEGETATION TO BE PROPERLY PROTECTED AND TO REMAIN IN ITS NATURAL STATE.
 3. IMMEDIATE INSTALLATION OF ALL REMAINING SEDIMENT AND EROSION CONTROL DEVICES.
 4. INITIATE CONSTRUCTION OF ROADWAY & STORMWATER COLLECTION/TRANSMISSION SYSTEM.
 5. UPON COMPLETION OF CONSTRUCTION ACTIVITIES, PROVIDE RESTORATION, FINE GRADE REMAINDER OF SITE, AND STABILIZE WITH TEMPORARY SEEDING.
 6. REMOVAL OF APPROPRIATE TEMPORARY SEDIMENT AND EROSION CONTROL DEVICES.



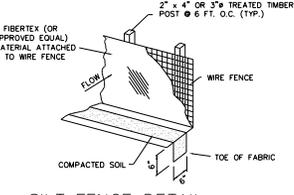
TREE BARRICADE SHALL BE IN PLACE AT THE OUTER EDGE OF THE GRAP LINE OF THE TREE PRIOR TO ANY LANDSCAPE DISTURBANCE.
N.T.S.



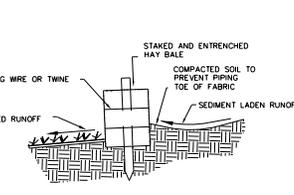
TO BE USED AT SELECTED SITES WHERE THE NATURAL GROUND SLOPES TOWARD THE TOE OF SLOPE.
N.T.S.



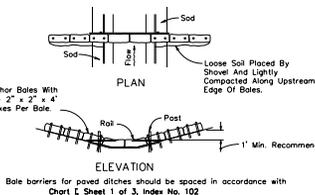
TO BE USED AT SELECTED SITES WHERE THE NATURAL GROUND SLOPES AWAY FROM THE TOE OF SLOPE.
N.T.S.



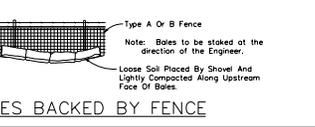
SILT FENCE DETAIL
N.T.S.



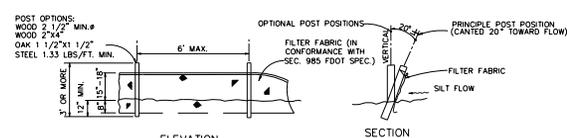
DETAIL OF PROPERLY INSTALLED HAY BALE
N.T.S.



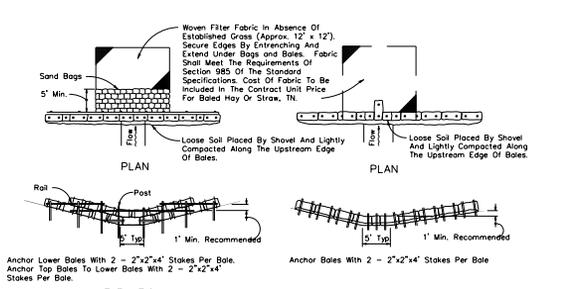
BARRIER FOR PAVED DITCH
N.T.S.



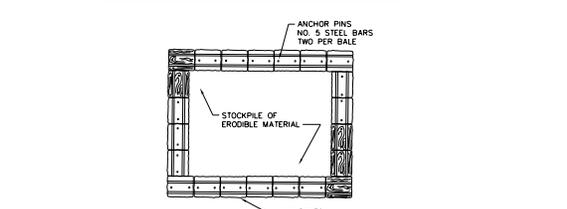
BALES BACKED BY FENCE
N.T.S.



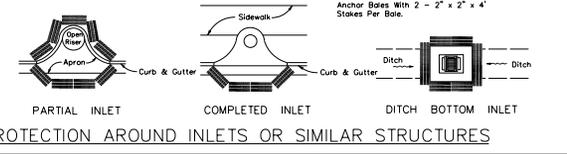
SILT FENCE APPLICATIONS
DO NOT DEPLOY IN A MANNER THAT SILT FENCES WILL ACT AS A DAM ACROSS PERMANENT FLOWING WATERCOURSES. SILT FENCES ARE TO BE USED AT UPLAND LOCATIONS AND TURBIDITY BARRIERS USED AT PERMANENT BODIES OF WATER.



BARRIER FOR UNPAVED DITCHES
The use of Types I & II b.e barriers should be limited to the conditions outlined in Chart C Sheet 1 of 3, Index No. 102.



MATERIAL STOCKPILE DETAIL
N.T.S.



PROTECTION AROUND INLETS OR SIMILAR STRUCTURES
N.T.S.

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Hatch Mott MacDonald
220 West Garden Street, Suite 700
Pensacola, Florida 32502 (850) 424-6001
Engineer of Record: Michael Gund, P.E. #79401

Board of County Commissioners
Escambia County, Florida

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

MISC. DETAILS

SHEET NO. 5

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SURVEYOR'S NOTES:

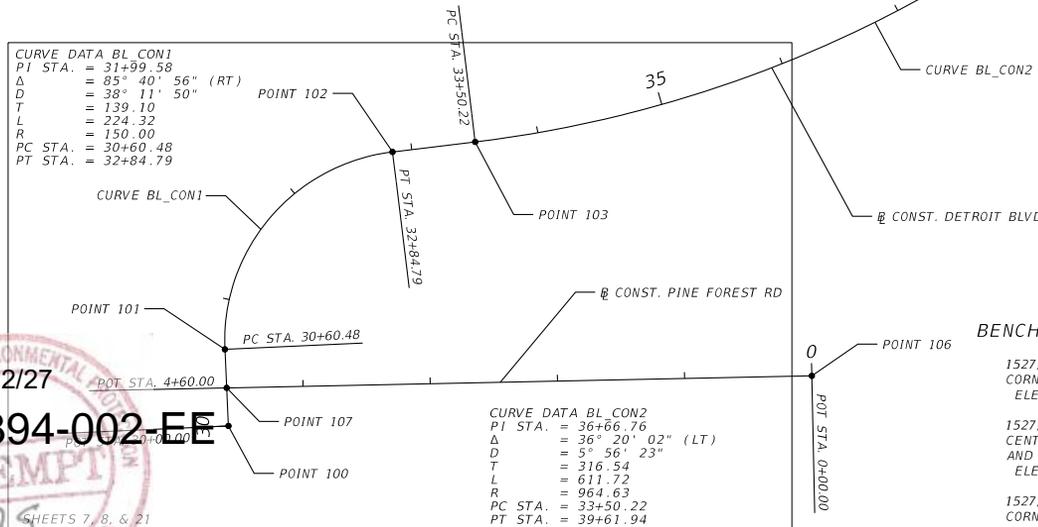
1. THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. THE SURVEYOR DOES NOT CERTIFY THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES OR THAT THEY ARE THE EXACT SIZE, MATERIAL, OR LOCATION AS INDICATED.
2. THIS SURVEY DOES NOT REFLECT OR DETERMINE OWNERSHIP.
3. THIS SURVEY IS SUBJECT TO ANY FACTS THAT MAY BE DISCLOSED BY A FULL AND ACCURATE TITLE SEARCH.
4. THIS SURVEY IS SUBJECT TO SETBACKS, EASEMENTS, AND RESTRICTIONS OF RECORD.
5. TO ASSURE THE CONTRACTOR IS ON THE SAME VERTICAL AND HORIZONTAL DATUM AS THIS SURVEY, IT IS STRONGLY RECOMMENDED THAT VERTICAL CHECKS BE MADE BETWEEN TWO BENCHMARKS AND THAT HORIZONTAL CHECKS BE MADE BETWEEN THREE CONTROL POINTS OR PROPERTY CORNERS.
6. FOOTINGS AND FOUNDATIONS BELOW NATURAL GROUND NOT LOCATED.
7. DUE TO CURRENT SAFETY REGULATIONS, WE ARE UNABLE TO ACCESS ANY SEWER MANHOLES AND/OR STRUCTURES IN ORDER TO VERIFY PIPE SIZE OR MATERIAL. PIPE SIZE AND MATERIAL HAS BEEN SHOWN AS PER PLANS OR BY APPROXIMATE MEASUREMENTS. IT IS STRONGLY RECOMMENDED THAT SIZE AND MATERIAL ARE VERIFIED BEFORE ANY DESIGN WORK BEGINS.
8. COORDINATES SHOWN ARE STATE PLANE COORDINATES REFERENCED TO N.A.D. 83 DATUM, 2011 ADJUSTMENT, FLORIDA NORTH ZONE AND EXPRESSED IN U.S. SURVEY FEET.
9. ELEVATIONS SHOWN ARE REFERENCED TO NAVD 88 DATUM, 1991 ADJUSTMENT, AND ARE EXPRESSED IN U.S. SURVEY FEET.
10. WETLANDS SHOWN AS REFERENCED BY WETLAND SCIENCES.

CONTROL POINTS:

110-77-B24:	N 568371.83	E 1094239.68	Z 138.84
ESC-4066:	N 565921.35	E 1080107.50	Z 105.25
J-302:	N 568618.75	E 1089490.04	Z 125.87
110-73-B23 RM1:	N	E	Z 133.41

SURVEYOR CONTACT INFORMATION:

DAVID D. GLAZE, PSM
 PITTMAN, GLAZE AND ASSOCIATES, INC.
 5700 N. DAVIS HIGHWAY, SUITE 3
 PENSACOLA, FLORIDA 32503 PH: (850) 434-6666



POINT NO.	STATION	NORTHING	EASTING
DETROIT BLVD @ CONST. COORDINATE DATA			
100	30+00.00	565,085.3070	1,082,689.9587
101	30+60.48	565,084.2112	1,082,750.4255
102	32+84.79	564,942.8170	1,082,897.4619
103	33+50.22	564,877.4957	1,082,901.2049
104	39+61.94	564,317.6283	1,083,121.1390
105	42+90.70	564,064.3638	1,083,330.7578

POINT NO.	STATION	NORTHING	EASTING
PINE FOREST RD @ CONST. COORDINATE DATA			
106	0+00.00	564,625.1920	1,082,700.1210
107	4+60.00	565,084.7628	1,082,719.9866

BENCHMARKS:

- 1527/57 - AN "X" IN THE SOUTH BONNET BOLT OF A FIRE HYDRANT LOCATED IN THE NORTHEAST CORNER OF THE INTERSECTION OF DETROIT BOULEVARD AND PINE FOREST ROAD. ELEVATION = 117.40'
- 1527/28 - AN "X" IN THE WEST BONNET BOLT OF A FIRE HYDRANT LOCATED 18' +/- EAST OF THE CENTERLINE OF DETROIT BOULEVARD, 200' +/- SOUTH OF THE CENTERLINE OF CAMDACE DRIVE, AND 70' +/- SOUTH OF THE CENTERLINE OF THE WEST ENTRANCE TO BEST WESTERN PLUS HOTEL. ELEVATION = 92.91'
- 1527/54B - A COTTON GIN SPIKE IN THE SOUTH SIDE OF A POWER POLE LOCATED AT THE NORTHWEST CORNER OF THE INTERSECTION OF SHARON LANE AND DETROIT BOULEVARD. ELEVATION = 93.88'
- 1538/62B - AN "X" IN THE NORTH BONNET BOLT OF A FIRE HYDRANT LOCATED 18' +/- NORTH OF THE CENTERLINE OF DETROIT BOULEVARD AND 48' +/- EAST OF KINGFISHER WAY. ELEVATION = 91.27'

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REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

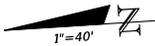
Hatch Mott MacDonald
 220 West Garden Street, Suite 700
 Pensacola, Florida 32502 (850) 484-6001
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Board of County Commissioners Escambia County, Florida		
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DETROIT	ESCAMBIA	

PROJECT LAYOUT

SHEET NO.
6



REMOVE EXIST. CONC. PAVEMENT
REMOVE EXIST. CONC. HEADWALL

EXIST. GULF POWER POLE
LOCATE & PROTECT
ADJUST GUY ANCHORS
FOR PROPOSED
IMPROVEMENTS

SAWCUT ASPHALT 79 LF
REMOVE EXIST. 36" PIPE

SAWCUT ASPHALT 26 LF

REMOVE PORTION OF EXIST.
CONC. FLUME AS NEEDED
TO CONNECT FLUME TO S-1
(SEE CROSS SECTIONS)

REMOVE EXIST. CONC. PAVEMENT

SAWCUT ASPHALT 27 LF

SAWCUT ASPHALT 4 LF

EXIST. ECUA WM
LOCATE & PROTECT

SAWCUT ASPHALT 541 LF

REMOVE EXIST. CURB

SEDIMENT BARRIER (STAKED SILT FENCE)

REMOVE EXIST. PAVEMENT

17-0345394-002-EE



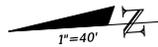
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

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220 West Garden Street, Suite 700
Pensacola, Florida 32502 (850) 484-6001
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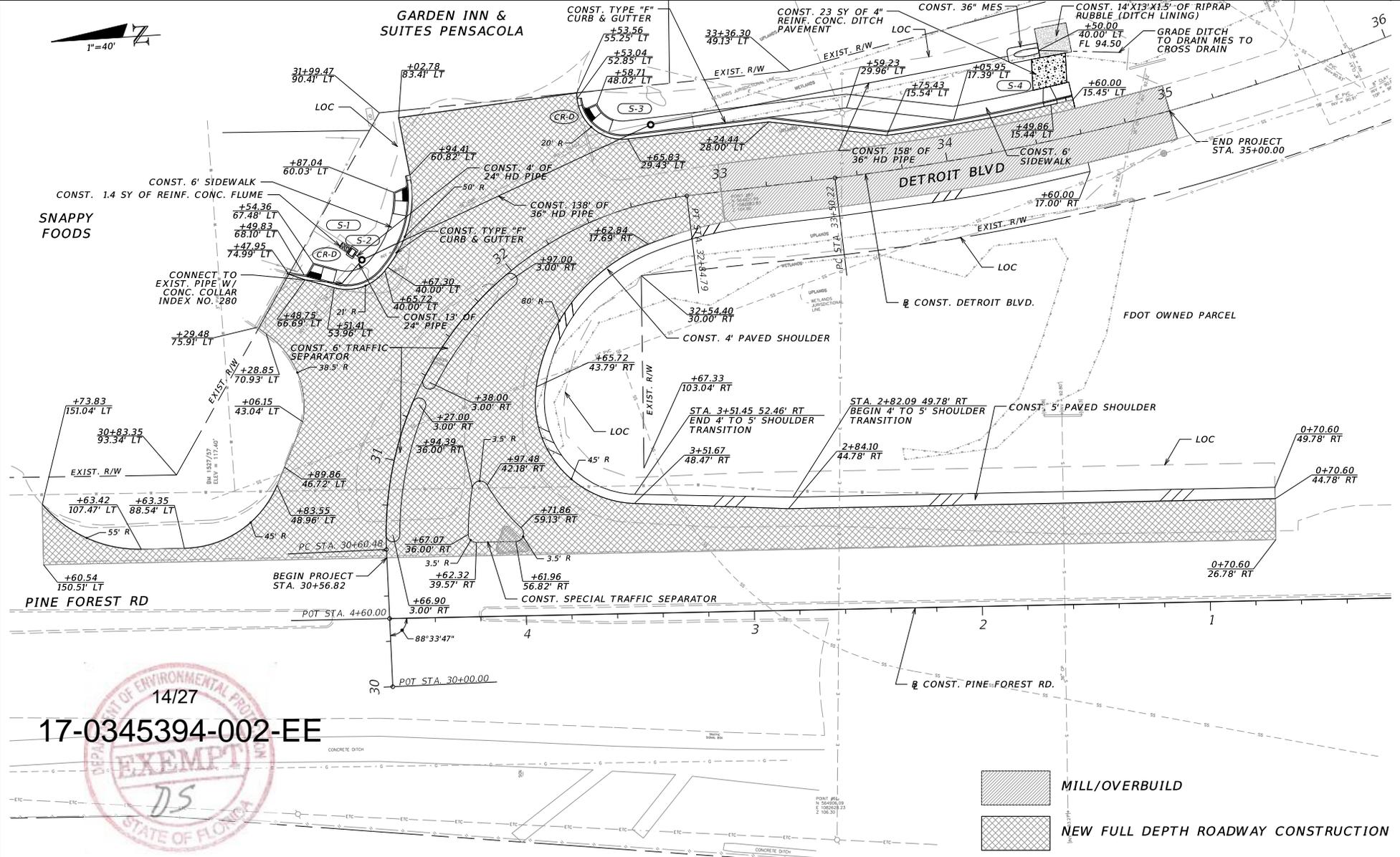


Board of County Commissioners Escambia County, Florida		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

DEMO/UTILITY ADJUSTMENT		SHEET NO. 7
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GARDEN INN & SUITES PENSACOLA

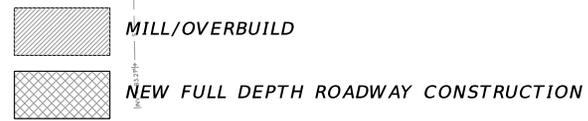


SNAPPY FOODS

PINE FOREST RD

DETROIT BLVD

17-0345394-002-EE



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

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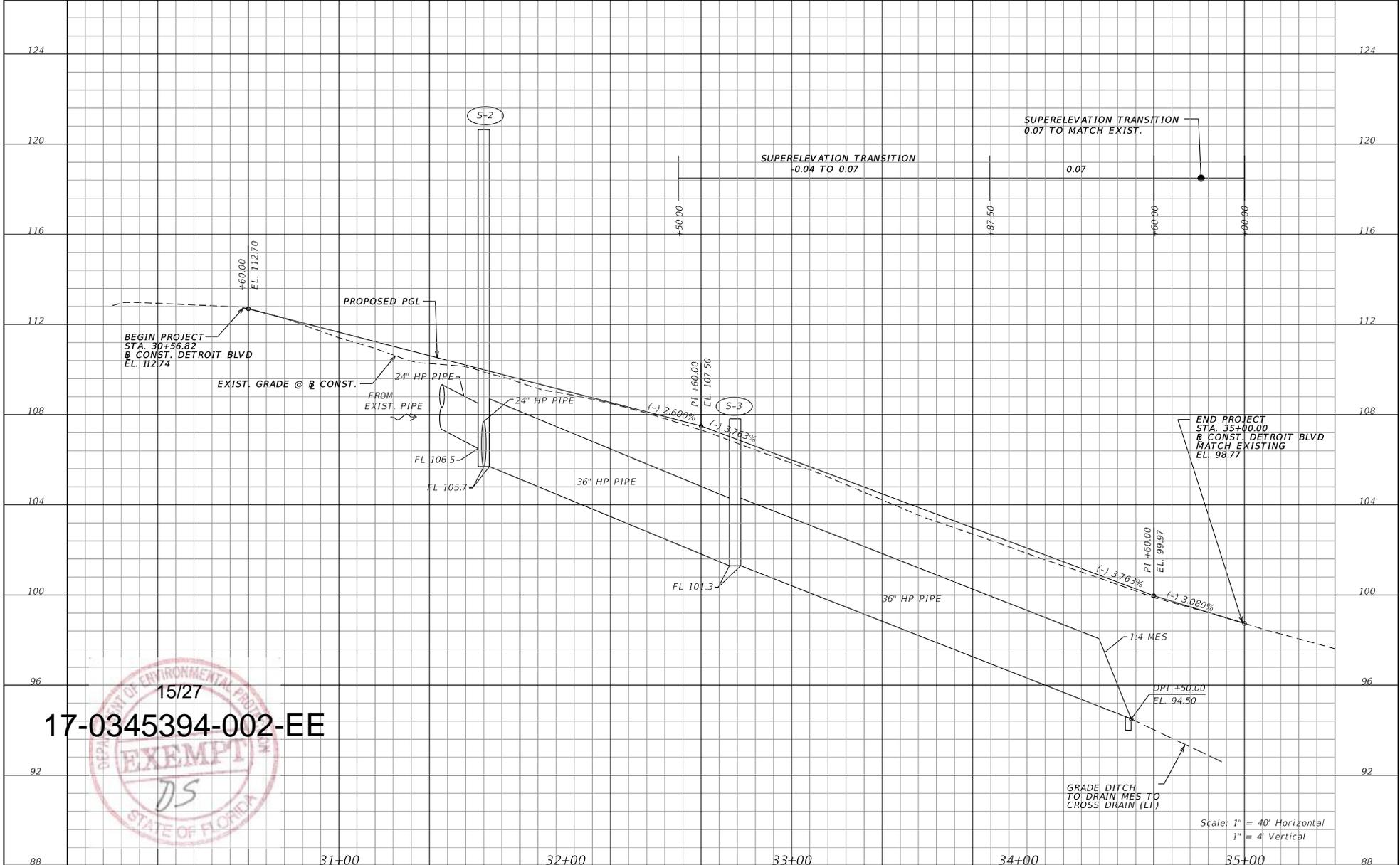


Board of County Commissioners Escambia County, Florida		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

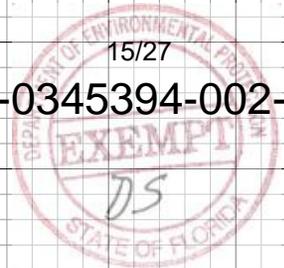
ROADWAY PLAN

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SHEET NO.
8



17-0345394-002-EE



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

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Board of County Commissioners
 Escambia County, Florida

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

ROADWAY PROFILE

Scale: 1" = 40' Horizontal
 1" = 4' Vertical

88 SHEET NO. 9



Regular		Exc.		Embankment	
A	V	A	V	A	V
	200.7	192	0.0	0	0
	214.6	145	0.0	0	0
	214.6	0	0.0	0	0

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Board of County Commissioners
 Escambia County, Florida

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DETROIT	ESCAMBIA	

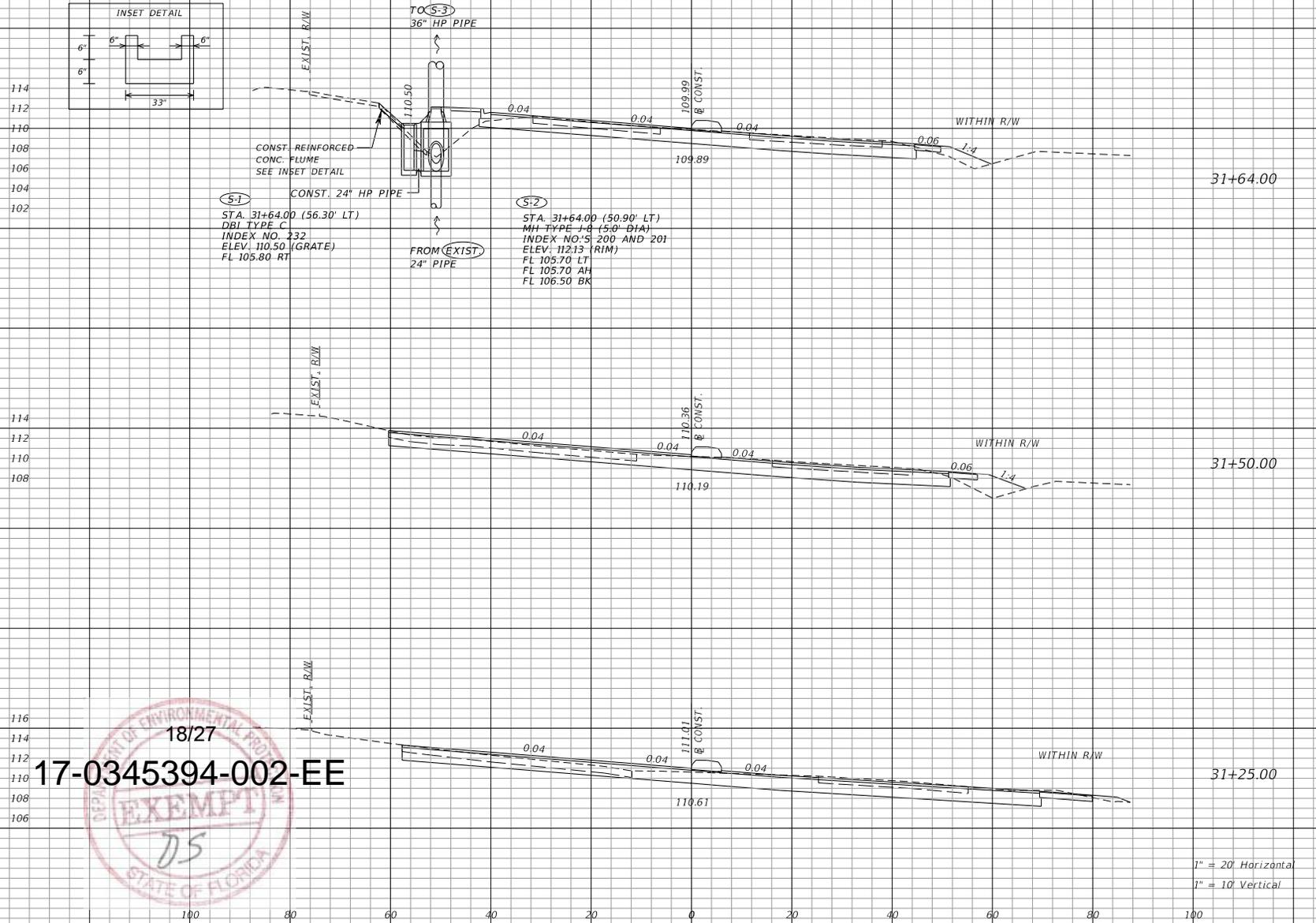
CROSS SECTIONS

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SHEET NO.
11

Regular Exc. Embankment

A V A V



S-1
 STA. 31+64.00 (56.30' LT)
 DBL TYPE C
 INDEX NO. 732
 ELEV. 110.50 (GRATE)
 FL 105.80 RT

S-2
 STA. 31+64.00 (50.90' LT)
 MH TYPE J-8 (50" DIA)
 INDEX NO'S 200 AND 201
 ELEV. 112.15 (RIM)
 FL 105.70 LT
 FL 105.70 AH
 FL 106.50 BK

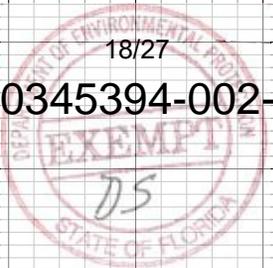
124.6 51.9 72 18

152.8 15.6 153 8

178.5 2.6 176 1

1" = 20' Horizontal
 1" = 10' Vertical

18/27
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 Pensacola, Florida 32502 (850) 434-6001
 Engineer of Record: Michael Gund, P.E. #79401

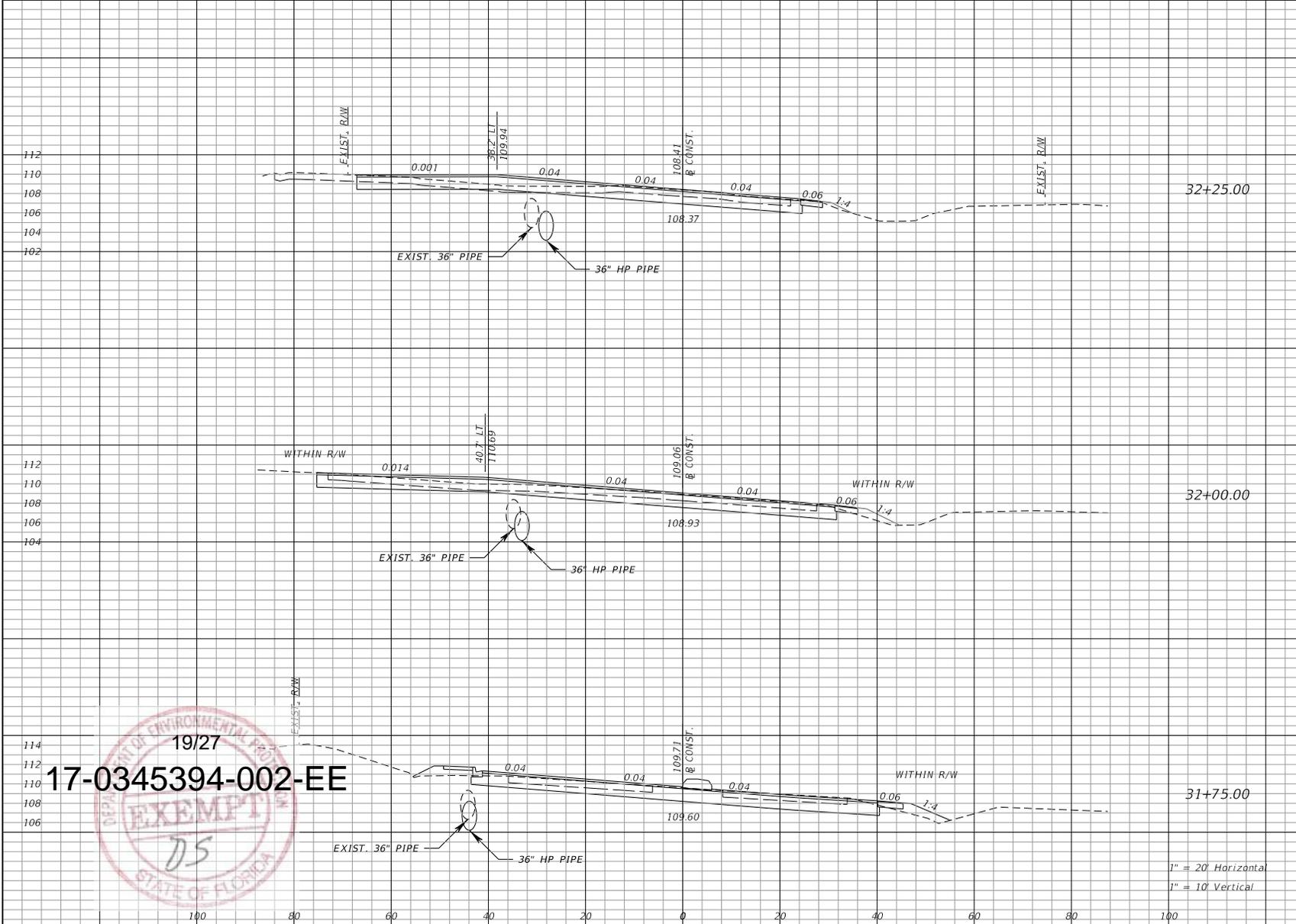


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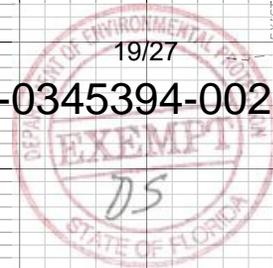
CROSS SECTIONS

SHEET NO.
 12



Regular		Exc.		Embankment	
A	V	A	V	A	V
106.9	111	1.4	3		
133.9	115	5.1	10		
114.0	49	15.6	14		

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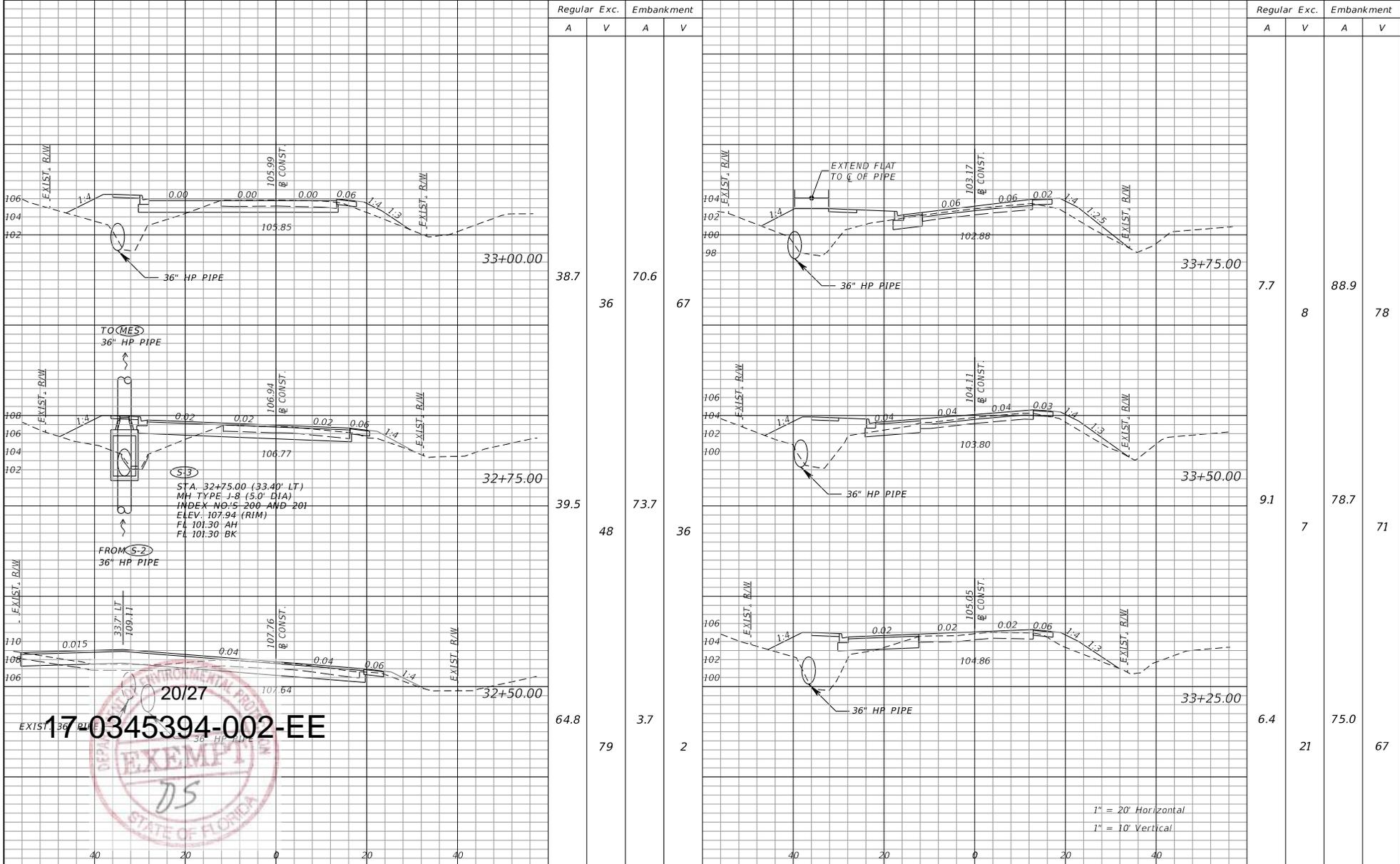
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CROSS SECTIONS

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SHEET NO.
13



Regular Exc. Embankment

A	V	A	V
38.7	36	70.6	67
39.5	48	73.7	36
64.8	79	3.7	2

Regular Exc. Embankment

A	V	A	V
7.7	8	88.9	78
9.1	7	78.7	71
6.4	21	75.0	67

1" = 20' Horizontal
1" = 10' Vertical



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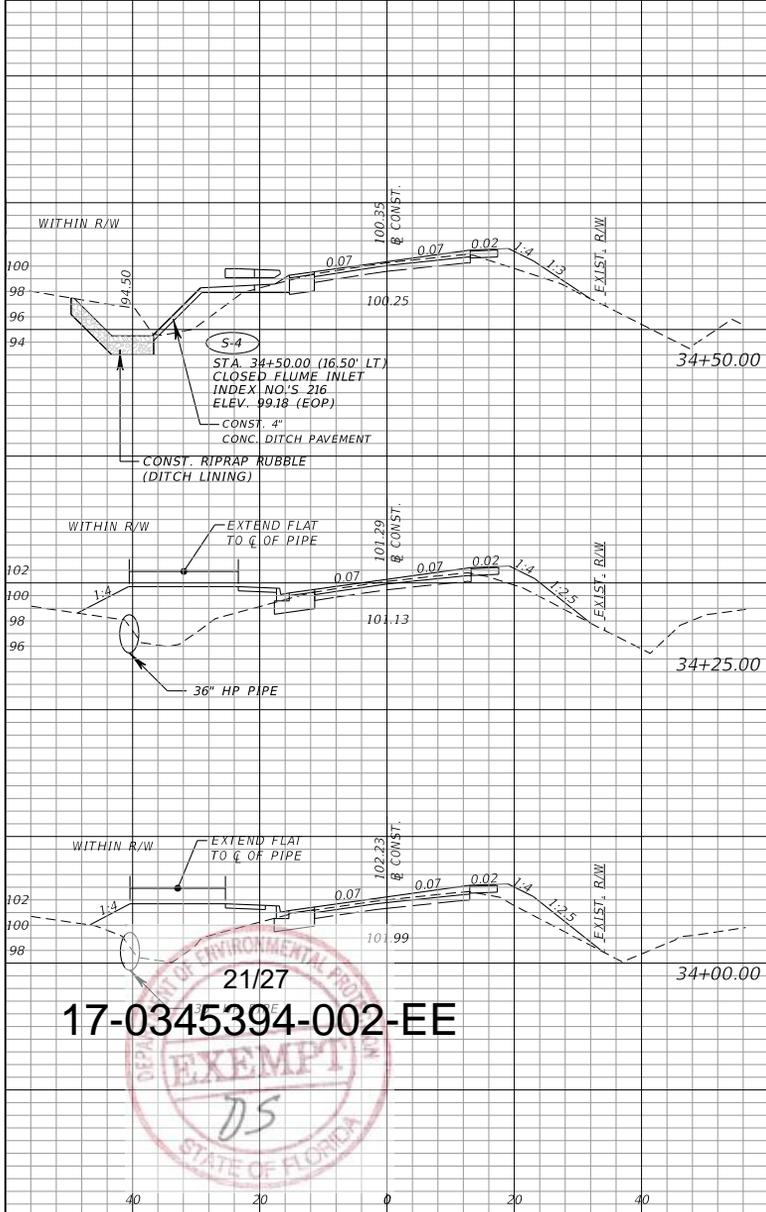


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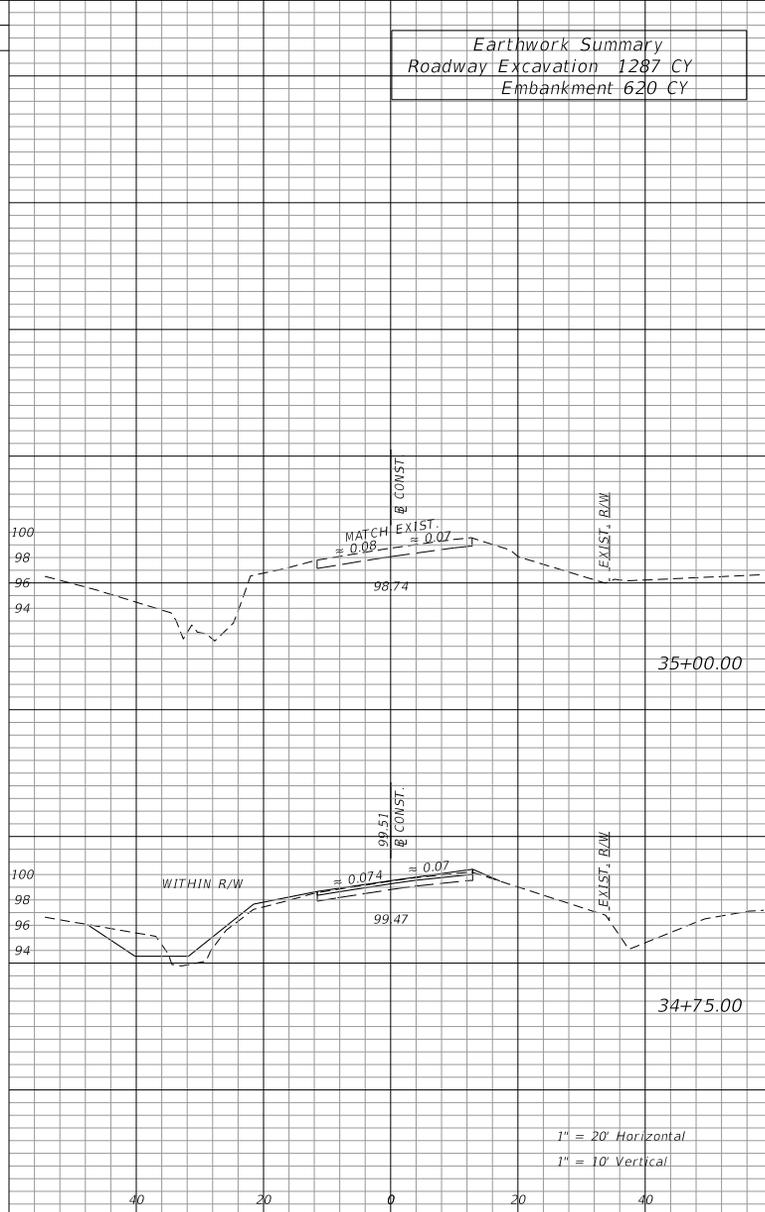
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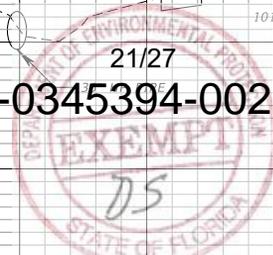
Regular Exc.		Embankment	
A	V	A	V
47.3	25	28.5	56
7.5	7	92.0	83
8.0	7	87.5	82

Earthwork Summary
 Roadway Excavation 1287 CY
 Embankment 620 CY



Regular Exc.		Embankment	
A	V	A	V
0.0	7	0.0	5
14.4	29	11.6	19

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1" = 20' Horizontal
 1" = 10' Vertical

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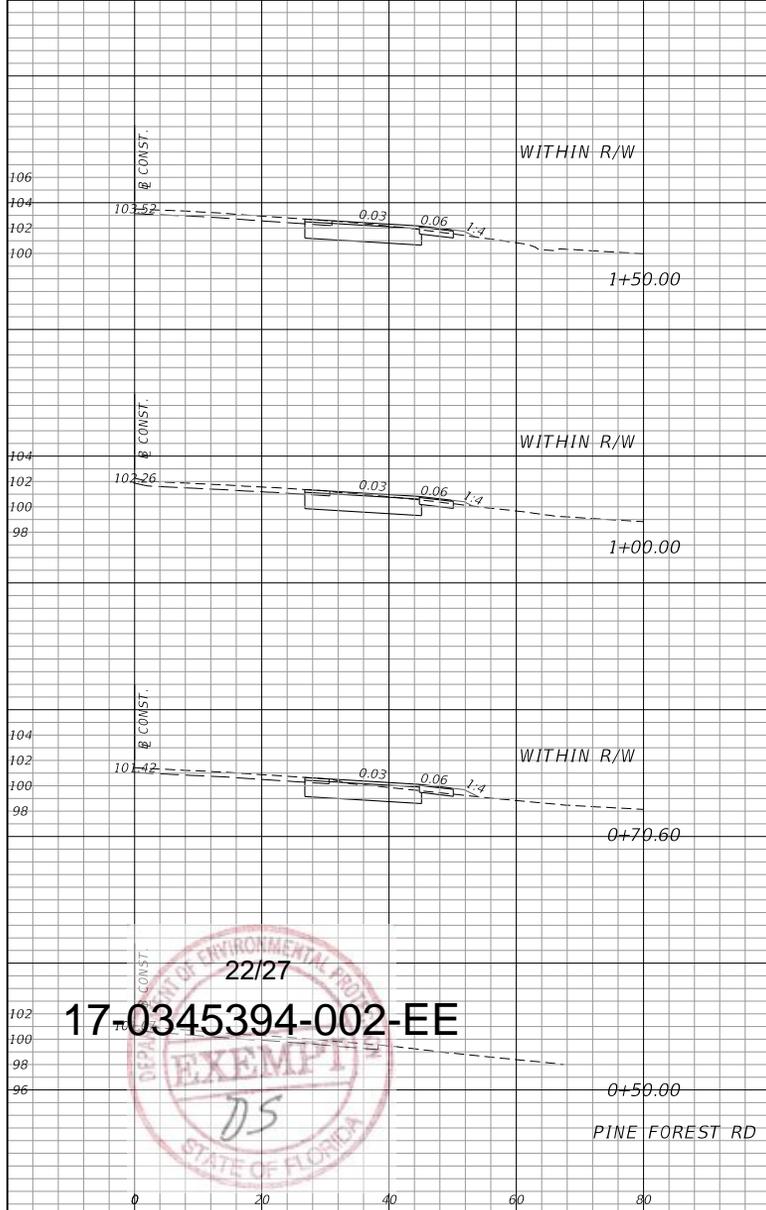


Board of County Commissioners Escambia County, Florida		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

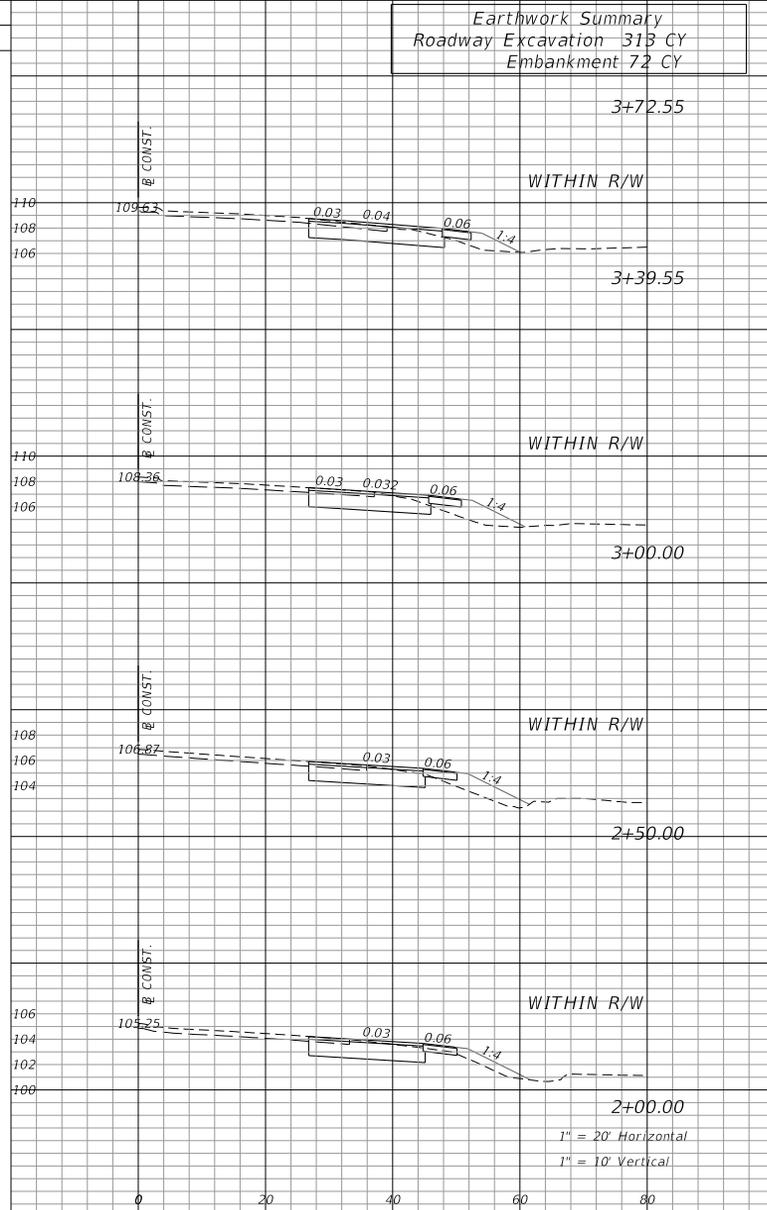
CROSS SECTIONS

SHEET NO.
15

Earthwork Summary
 Roadway Excavation 313 CY
 Embankment 72 CY

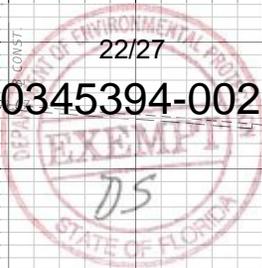


Regular Exc.		Embankment	
A	V	A	V
27.3	50	0.9	1
27.2	28	0.6	1
23.5	9	1.3	0
0.0	0	0.0	0



Regular Exc.		Embankment	
A	V	A	V
45.0	44	2.2	6
27.0	39	7.2	15
25.6	47	12.9	24
25.0	47	12.5	18
26.1	49	6.9	7

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 Engineer of Record: Michael Gund, P.E. #79401



Board of County Commissioners
 Escambia County, Florida

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

CROSS SECTIONS

1" = 20' Horizontal
 1" = 10' Vertical

SHEET NO.
16

TRAFFIC CONTROL GENERAL NOTES:

1. TRAFFIC CONTROLS SHALL BE IN ACCORDANCE WITH THE PROJECT PLANS, THE LATEST EDITION OF THE ESCAMBIA COUNTY TECHNICAL SPECIFICATIONS, THE FDOT DESIGN STANDARDS, THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS A MINIMUM.
2. THE TRAFFIC AND TRAVEL WAYS SHALL NOT BE ALTERED TO CREATE A WORK ZONE UNTIL ALL LABOR AND MATERIAL ARE AVAILABLE FOR THE CONSTRUCTION IN THAT AREA.
3. TRAVEL LANE WIDTHS SHALL NOT BE LESS THAN 10 FEET DURING CONSTRUCTION, UNLESS OTHERWISE SPECIFIED OR APPROVED BY THE ENGINEER. LANES SHALL BE PROPERLY DELINEATED DURING ALL PHASES OF CONSTRUCTION.
4. HEAVY TRAFFIC CONDITIONS, ACCIDENTS, AND ANY OTHER UNFORESEEN EMERGENCIES MAY REQUIRE RESTRICTION OR REMOVAL OF ANY LANE CLOSURE. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS WITHOUT DELAY AT THE DIRECTION OF THE ENGINEER.
5. ALL LANES MUST BE REOPENED TO NORMAL TRAFFIC WITHIN 12 HOURS OF AN EVACUATION NOTICE FOR A HURRICANE OR ANY OTHER EMERGENCY EVENT AND SHALL REMAIN OPEN FOR THE DURATION OF THE EVENT AS DIRECTED BY THE ENGINEER.
6. THE CONTRACTOR IS TO MAINTAIN AND KEEP STREET NAME IDENTIFICATION VISIBLE DURING CONSTRUCTION OPERATIONS IN ORDER TO FACILITATE EMERGENCY VEHICLE TRAFFIC.
7. THE CONTRACTOR SHALL ELIMINATE ALL DROP OFF HAZARDS BETWEEN TRAVEL LANES IN ACCORDANCE WITH INDEX 600.
8. ALL PROJECT INFORMATION SIGNS TO BE LOCATED AS REQUIRED BY STANDARD INDEX NO. 600.
9. ACCESS TO SIDE ROADS AND DRIVEWAYS SHALL BE MAINTAINED TO LOCAL TRAFFIC AND PROPERTY OWNERS FOR THE DURATION OF CONSTRUCTION ACTIVITIES.
10. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAINTAIN CLEAR ZONE REQUIREMENTS FOR EQUIPMENT, MATERIALS STORAGE AND WORK ZONE PROTECTION AS SPECIFIED IN THE INDEX 600 SERIES.
11. ALL DRAINAGE IMPROVEMENTS SHALL BE COMPLETED PRIOR TO THE FINAL STRUCTURAL COURSES. NO OPEN EXCAVATION SHALL REMAIN OVERNIGHT.
12. MILLING OPERATIONS IN TRAVEL LANES ARE RESTRICTED TO ONLY THAT AREA WHICH CAN BE MILLED AND RESURFACED WITHIN THE SAME WORK OPERATION, PRIOR TO OPENING THE ROADWAY BACK TO TRAFFIC.
13. IF EXISTING SIGNS ARE DAMAGED BEYOND USE BY THE CONTRACTOR DURING CONSTRUCTION, AS DETERMINED BY THE ENGINEER, SIGNS SHALL BE REPLACED BY THE CONTRACTOR AT HIS/HER EXPENSE.
14. MAINTENANCE OF TRAFFIC OPERATIONS ON INTERSECTING SIDE STREETS SHALL BE COORDINATED WITH ESCAMBIA COUNTY. COORDINATION WITH FDOT MAY BE REQUIRED FOR SR 297 (PINE FOREST ROAD).
15. CONFLICTING OR MISLEADING SIGNAGE SHALL BE COVERED OR REMOVED FOR THE DURATION OF CONSTRUCTION OR UNTIL THE CONFLICT NO LONGER EXISTS.
16. ANY TEMPORARY PAVEMENT CALLED FOR IN THE PLANS OR PROPOSED BY THE CONTRACTOR SHALL BE COMPRISED OF 2" TYPE 3P STRUCTURAL COURSE CONSTRUCTED ON A FIRMLY COMPACTED AND UNYIELDING SUBGRADE AS A MINIMUM. THE CONTRACTOR IS RESPONSIBLE FOR BOTH CONSTRUCTING AND MAINTAINING ALL TEMPORARY PAVEMENT THROUGHOUT ALL PHASES OF CONSTRUCTION.
17. CONFLICTING OR MISLEADING PAVEMENT MARKINGS SHALL BE REMOVED BY WATER BLASTING OR OTHER METHODS APPROVED BY THE ENGINEER. THE REMOVAL OF EXISTING PAVEMENT MARKINGS OR TEMPORARY PAVEMENT MARKINGS SHALL BE COMPLETED IN ACCORDANCE WITH INDEX 600. ALL EXISTING PAVEMENT MARKINGS WHICH ARE ALTERED BY CONSTRUCTION, SHALL BE REPLACED UPON COMPLETION OF THE PROJECT.



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Hatch Mott MacDonald
 220 West Garden Street, Suite 700
 Pensacola, Florida 32502 (850) 484-6001
 Engineer of Record: Michael Gund, P.E. #79401



Board of County Commissioners Escambia County, Florida		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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TRAFFIC CONTROL NOTES

SHEET NO.
17



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 PHASE I WORK ZONE

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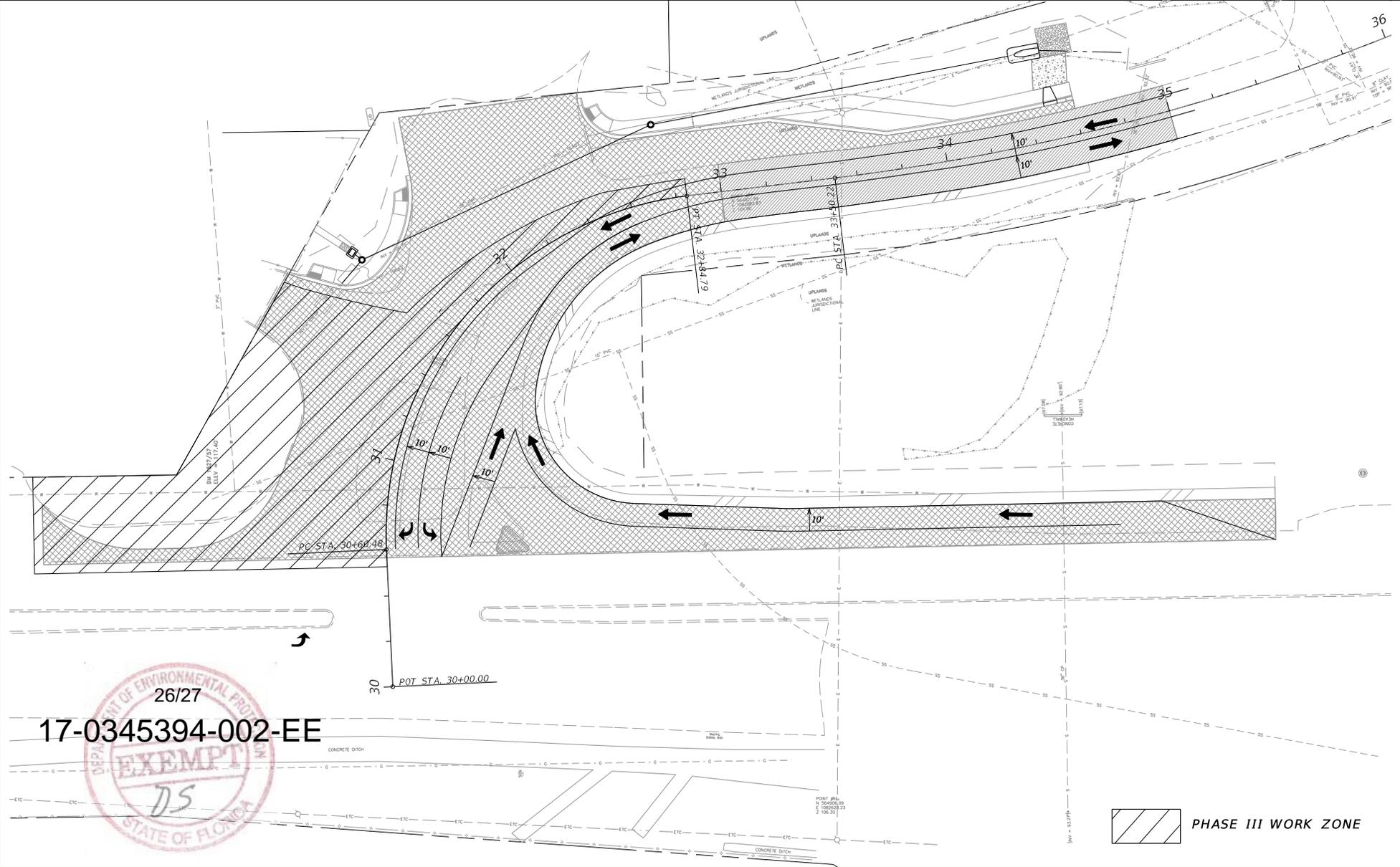


Board of County Commissioners
Escambia County, Florida

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

TRAFFIC CONTROL PLAN
PHASE I

SHEET NO.
18



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26/27



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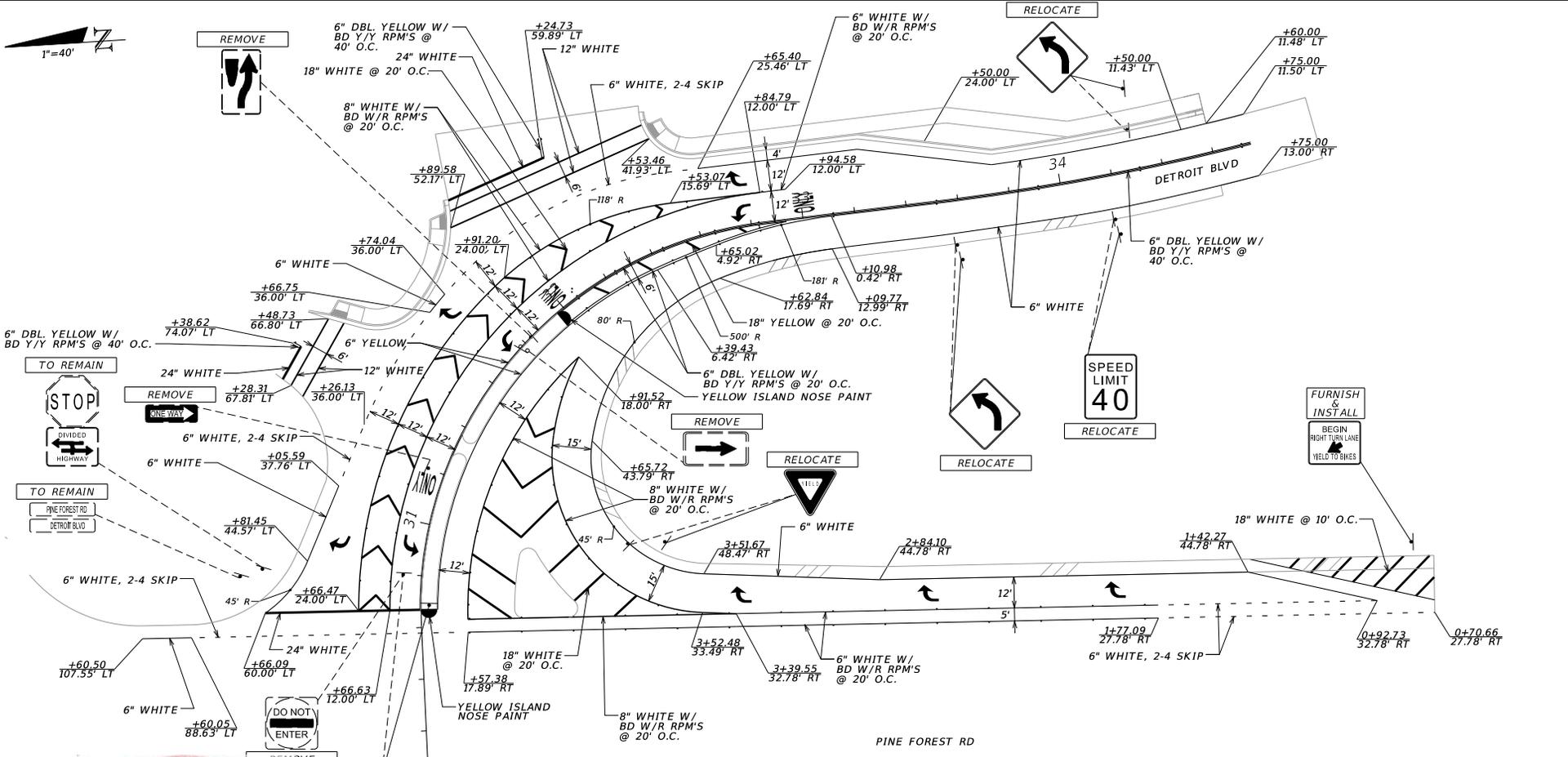
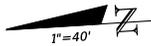


Board of County Commissioners
 Escambia County, Florida

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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TRAFFIC CONTROL PLAN
PHASE III

SHEET NO.
20



27127
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 Engineer of Record: Michael Gund, P.E. #79401



Board of County Commissioners Escambia County, Florida		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
DETROIT	ESCAMBIA	

SIGNING AND PAVEMENT MARKING PLAN	
21	

SHEET NO.	21
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ACOE PERMIT HAS BEEN APPLIED FOR BUT NOT RECEIVED

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Appendix C
GEOTECHNICAL REPORT

3.0 REVIEW OF PUBLISHED DATA

3.1 Regional Geology

Based on the Florida Geological Survey, Open-File Report 80, 2001 and other geologic references, the near surface geologic deposits and formations from youngest to oldest in Escambia County include: Holocene Sediment (Qh), Undifferentiated sediment (Qu), the Citronelle Formation (Tci), Miocene Coarse Clastics and Pensacola Clay.

The Holocene sediments generally occur near the coastline and in river flood plains and include quartz sands, carbonate sand, and muds with organics. The Undifferentiated sediments are light gray, tan, brown to black, unconsolidated to poorly consolidated, clean to clayey silty, unfossiliferous, variably organic-bearing sands to blue green to olive green, poorly to moderately consolidated, sandy silty clays.

The Citronelle Formation is a siliciclastic deltaic deposit that consists of gray to orange, often mottled, unconsolidated to poorly consolidated, very fine to coarse grained, poorly sorted, clean to clayey sands. It contains significant amounts of clay, silt and gravel which may occur as beds and lenses and may vary considerably over short distances. Limonite nodules and limonite cemented beds (hard pan) are common. The Citronelle Formation is highly permeable and forms the upper portions of the Sand and Gravel Aquifer system in the western panhandle of Florida. The Citronelle Formation varies in thickness but averages from 250 feet to greater than 500 feet.

Underlying most of the county is the Miocene Coarse Clastics which is similar to the Citronelle Formation with one exception - the Miocene Coarse Clastics have an abundance of fossils whereas the Citronelle Formation has none. The Miocene Coarse Clastics consists of light-brown to light gray, poorly sorted fine to very coarse sand and granules and small pebbles of quartz with muscovite abundant throughout the deposit.

Underlying the southern portion of the county is the Pensacola Clay which is a dark to light gray silty clay that is micaceous and slightly calcareous.

3.2 USGS Quadrangle Map

Based on review of the “Cantonment” USGS Quadrangle Map, it appears that the natural ground surface elevations within the project limits range from approximately +80 feet to +120 feet North



REPORT OF GEOTECHNICAL EXPLORATION

**Detroit Boulevard Safety Improvements – Phase II
Roadway Soil Survey & Pavement Design
Escambia County, Florida**

Tierra Project No. 4511-14-004

Prepared for:

**Hatch Mott MacDonald of Florida, Inc.
Attn: Mr. Thomas McLendon, PE
5111 North 12th Avenue
Pensacola, Florida 32504**

Prepared by:

**Tierra, Inc.
1300 West Main Street
Pensacola, Florida 32502**

July 14, 2016



**1300 West Main Street • Pensacola, FL 32502
Phone (850) 462-8774 • Fax (850) 607-6953
Florida Certificate No. 6486**

TIERRA

July 14, 2016

Hatch Mott MacDonald of Florida, Inc.
Attn: Mr. Thomas McLendon, PE
5111 North 12th Avenue
Pensacola, Florida 32504

Subject: Detroit Blvd. Safety Improvements – Phase II
Roadway Soil Survey & Pavement Design
Escambia County, Florida
Project No. 4511-14-004

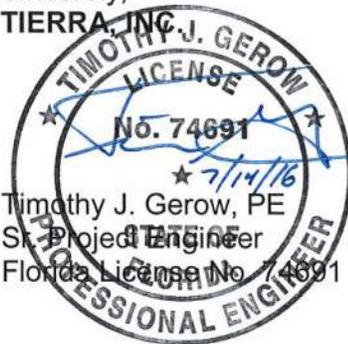
Mr. McLendon:

Thank you for choosing Tierra, Inc. (Tierra) as your Geotechnical consultant. Per your authorization, we have completed the Roadway Soil Survey for the subject project. The results of the survey are discussed in this report, three copies of which are enclosed (two bound copies, one unbound copy).

Tierra appreciates being of service to HMM. Should you have any questions regarding the enclosed report or the project in general, please do not hesitate to contact us at (850) 462-8774.

Sincerely,

TIERRA, INC.



Timothy J. Gerow, PE
Site Project Engineer
Florida License No. 74691

A handwritten signature in blue ink, appearing to read "Mitchell L. Smith".

Mitchell L. Smith, PE
Principal Geotechnical Engineer
Florida License No. 43416



1300 West Main Street • Pensacola, FL 32502
Phone (850) 462-8774 • Fax (850) 607-6953
Florida Certificate No. 6486

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APPENDIX A

Project Location Map
USGS Quadrangle Map
USDA Soil Survey Map
Table 1 – Summary of USDA Soil Survey
Potentiometric Surface Map

APPENDIX B

Roadway Boring Location Plan
Roadway Soil Profiles
Table 2 – Summary of Laboratory Test Results
Table 3 – Summary of Seasonal High Water Table Estimates
Preliminary Plastic Soil Delineation Plan Sheets
LBR Test Results

1.0 PROJECT INFORMATION

1.1 Project Authorization

Authorization to proceed on this project was issued by Hatch Mott MacDonald of Florida, Inc. via acceptance of our Geotechnical Engineering Services Proposal dated February 24, 2016 which was incorporated into Work Order 1.

1.2 Project Description

Based on the information provided by Hatch Mott MacDonald, we understand that Escambia County desires to bring the section of Detroit Blvd. from Pine Forest Road (SR 297) to Pensacola Blvd. (US 29/SR 95) into compliance with AASHTO standards for pavement widths, clear zones, and other safety criteria as well as Americans with Disabilities Act (ADA) standards.

The improvements will include, among other items, milling/resurfacing and reconstructing Detroit Blvd. from Pine Forest Road to Pensacola Blvd. (13,040 feet). The finished roadway cross section will consist of two 11-foot wide traffic lanes, 4-foot wide paved shoulders/bike lanes, and a sidewalk on the north side of the road. From Pine Forest Road to approximately Cove Lane, the roadway will be an undivided rural section and from approximately Cove Lane to Pensacola Blvd. the roadway will be an urban roadway with curb/gutter. To achieve the proposed cross section, approximately 4 feet of pavement will be added to the north and south edges of the existing pavement to accommodate the paved shoulder/bike lane.

If any of the project information noted above is incorrect or has changed, please inform Tierra so that we may amend the recommendations presented in this report, if necessary.

1.3 General Site Conditions

Detroit Boulevard is a 2-lane road oriented in an east/west direction in Escambia County, Florida. The location of the roadway is shown on the Project Location Map included in Appendix A.

Roadside ditches are located on both sides of the existing road throughout most of the project where the proposed paved shoulders will be located. These areas are generally vegetated with grass. Based on the topographic information provided by the project surveyor, existing grades along the alignment ranged from approximately +75 feet to +125 feet NAVD88.

2.0 PURPOSE AND SCOPE OF SERVICES

The purpose of this phase II Roadway Soil Survey was to collect and evaluate the subsurface conditions present in the proposed alignment to assist in development of the 60% Roadway plans. The following services were provided during the course of this study:

1. Reviewed topographic information obtained from the “Cantonment, Florida” quadrangle map published by the United States Geologic Survey (USGS).
2. Reviewed soil information obtained from the Soil Survey of Escambia County, Florida published by the United States Department of Agriculture (USDA) Natural Resources Conservation Service (NRCS).
3. Reviewed the potentiometric surface map of the surficial aquifer within the region.
4. Conducted a visual reconnaissance of the project site and located and coordinated utility clearances via Sunshine One Call.
5. Performed a geotechnical field study consisting of auger borings within areas of the project alignment where embankment fill is planned.
6. Measured the depth to groundwater (where encountered) and estimated “Normal” Seasonal High Water Table (SHWT) levels at each boring location.
7. Visually classified and stratified the recovered soil samples in the laboratory.
8. Analyzed the preliminary data collected and prepared this Roadway Soil Survey report.

American Vertical Datum of 1988 (NAVD 88), as shown on the USGS Vicinity Map (Appendix A). The USGS topographic information is generally consistent with the topographic information provided by the project surveyor.

3.3 USDA Soil Survey

The United States Department of Agriculture (USDA) Natural Resources Conservation Services (NRCS) Web Soil Survey, which details the “Soil Survey of Escambia County, Florida”, published by the USDA Soil Conservation Service, was reviewed for general near-surface soil information within the project limits. The USDA Soil Survey Map along the project alignment is shown in Appendix A.

Based on review of the Soil Survey, there are eleven (11) primary soil-mapping units noted within the project limits (see USDA Vicinity Map, Appendix A). Table 1 (Appendix A) summarizes the soil mapping units identified and provides select engineering and environmental properties for each unit.

To summarize, the soil units with the most coverage within the project limits are (24) Poarch Sandy Loam, (38) Bonifay Loamy Sand, (43) Albany Sand, (49) Dorovan Muck and Fluvaquents, (51) Pelham Loamy Sand and (56) Troup-Poarch Complex. The soil units generally consist of slightly silty sands and silty sands from the ground surface to depths of 5 inches to 72 inches underlain by more silty and clayey sands, and sandy clays. This generally corresponds with the subsurface conditions encountered in the auger borings. Plastic soils (i.e. clayey sands to clays) will be discussed in greater detail in Section 4.4 below.

The USDA soil survey reported the presence of (49) Dorovan-Fluvaquents (soils with elevated organic contents) in isolated areas throughout the project (from approximately Sta 40 to Sta 45, Sta 60 to Sta 62, Sta 76 to Sta 77, and Sta 116 to Sta 118). Organic soils will be discussed in greater detail in Section 4.3 below.

The soil survey identified the potential for perched groundwater as shallow as approximately 10 inches below existing grade during the months of December through April. During periods of increased rainfall, shallow perched groundwater should be expected in areas where more silty and clayey sands are present in the upper portion of the soil horizon.

Note that the information contained in the USDA Soil Survey may not be reflective of actual soil and groundwater conditions, particularly if recent development in the project vicinity has modified soil conditions and/or surface/subsurface drainage conditions.

3.4 Review of Potentiometric Surface Information

Based on review of the “Potentiometric Surface of the Surficial Zone of the Sand-and-Gravel Aquifer, Escambia County, Florida” map (Figure 4, Appendix A), the potentiometric surface elevation of the Sand and Gravel aquifer within the project limits site is estimated to be approximately +30 to +40 feet MSL.

Given that ground surface elevations within the project limits range from approximately +70 feet to +130 feet NAVD 88, artesian conditions are not expected along the alignment of the road. The preliminary auger borings did not encounter artesian conditions during the field exploration.

4.0 SUBSURFACE EXPLORATION

4.1 Boring Location Plan and Utility Clearances

Prior to commencing the subsurface exploration, a Boring Location Plan for the proposed roadway improvements was prepared based on project information provided by HMM, our engineering judgment, and general guidelines provided in the FDOT “Soils and Foundation Handbook”.

The borings were located and staked in the field using a Garmin GPSMap 64ST Global Positioning System (GPS) unit with a reported accuracy of ± 3 meters (when locating a point in the field, ± 1 meter when recording the location of a point in the field). Generally, the borings were performed at the proposed boring locations. When not possible due to access or utility constraints, the borings were offset. The final locations of all borings were recorded after boring completion. The coordinates were then converted by Tierra to station and offset using the Microstation design files provided by HMM. The Boring Location Plan for the project is presented in Appendix B.

Utility clearances were coordinated by Tierra and updated as required prior to performing the soil borings in order to reduce the potential for damage to underground utilities during field testing activities.

4.2 Roadway Borings

To evaluate the subsurface conditions along the proposed roadway alignment, a total of 26 auger borings were performed to a depth of approximately 5 feet below the existing ground surface.

The hand auger borings were performed by manually twisting and advancing a bucket auger into the ground, typically in 4 inch to 6 inch increments. The soil sampling was performed in general accordance with the American Society for Testing and Materials (ASTM) test designation D1452. As each soil type was revealed, representative samples were placed in air-tight containers and returned to our laboratory for testing and visual classification.

The boring locations and the results of the borings are presented on the Boring Location Plan sheets and the Roadway Soil Profile sheets attached in Appendix B.

4.3 Delineation of Organic Materials

Soils with organic contents greater than 5% were not encountered in any of the 26 borings performed for the Roadway Soil Survey. However, if identified during construction, organic soils will need to be removed and replaced in accordance with FDOT Standard Indices 500 and 505 and/or Escambia County specifications.

4.4 Delineation of Plastic Materials

Several of the borings in the western portion of the project encountered Plastic soils (A-2-6, A-4, A-6, and A-7-6 materials) in the upper 5 feet of the soil horizon. The table below summarizes where and at what depth/elevation the Plastic materials were encountered in the borings.

PLASTIC SOIL DELINEATION			
Boring	Station/Off-Set	Approximate Depth of Plastic Soil, ft.	Approximate Elevation of Plastic Soil, ft. NAVD 88
A-35R	34+98, 18' RT	2.0 – 5+	96.7 to <93.7
A-44R	43+98, 23' RT	1.0 – 5+	91.0 to <87.0
A-46L	45+99, 27' LT	3.0 – 5+	86.7 to <84.7
A-54L	54+02, 25' LT	0.5 – 5+	86.0 to <81.5
A-60L	60+00, 21' LT	2.0 – 5+	74.1 to <71.1
A-66L	66+27, 23' LT	3.5 – 5+	91.8 to <90.3

We have prepared Plastic Soil Delineation Plan Sheets (Appendix B) based on the data collected. Plastic soils will need to be managed in accordance with FDOT Standard Indices 500 and 505 and/or Escambia County specifications.

5.0 LABORATORY TESTING

5.1 General

Representative soil samples collected from the borings were classified in general accordance with the AASHTO Soil Classification System. The classifications were based on visual observations and the results of the laboratory soil tests.

5.2 Test Designations

The following tests were performed on select soil samples:

- Grain-Size Analyses - Performed in general accordance with AASHTO T-088 (ASTM D-422).
- Fines Content - Performed in general accordance with AASHTO T-088 (ASTM D-422).
- Natural Moisture Content - Performed in general accordance with AASHTO T-265 (ASTM D-2216).
- Atterberg Limits - The liquid limit and the plastic limit tests ("Atterberg Limits") were performed in general accordance with AASHTO T-089 and T-090, respectively, (ASTM D-4318).

A detailed summary of the laboratory tests performed on samples obtained from the preliminary borings is presented in **Table 2** in **Appendix B**.

5.3 Limerock Bearing Ratio Testing

Limerock Bearing Ratio (LBR) tests were performed on several samples of the subgrade soils encountered in the borings. The results of the LBR tests are summarized in the Table 1 below. The complete LBR test results are attached in Appendix B.

TABLE 1
LIMEROCK BEARING RATIO TEST RESULTS

Soil Strata	Boring	Station	Offset	Sample Depth, ft.	Sample Description	Max. Dry Density, pcf	LBR Value
1	A-61R	61+15	31' RT	1' – 2'	Orange SAND (A-3)	113	52
2	A-3	33+31	19' RT	1' – 2'	Tan Silty SAND (A-2-4)	109	54
3	A-136L	136+00	22' LT	1' – 2'	Brown Silty SAND (A-2-4)	121	107
3	A-88R	88+02	16' RT	1' – 2'	Brown Silty SAND (A-2-4)	125	106
3	A-117R	117+01	17' RT	1' – 2'	Brown Silty SAND (A-2-4)	128	115

6.0 RESULTS OF SUBSURFACE EXPLORATION

6.1 General Soil Conditions

The soil types encountered in the preliminary roadway borings were assigned a stratum number and soil descriptions/AASHTO classifications, which are shown below.

Stratum	Soil Description	AASHTO Classification
1	Light Gray/Gray/Tan/Brown/Orange SAND	A-3
2	Light Gray/Gray/Tan/Brown/Orange silty SAND, 10% - 15% Fines	A-2-4
3	Gray/Tan/Brown/Orange/Red Silty SAND, 15% - 35% Fines	A-2-4
4	Gray/Tan/Brown/Orange/Red Clayey SAND	A-2-6
5	Gray/Tan/Orange SILT	A-4
6	Tan/Orange/Red CLAY	A-6
7	Brown/Orange/Red CLAY	A-7-6

The soil stratification was based on a visual review of the recovered samples, laboratory testing, and interpretation of the field boring logs. The boring stratification lines on the Roadway Soil Profile sheets (Appendix B) represent the approximate boundaries between soil types of significantly different engineering properties; however, the actual transition may be gradual.

In some cases, small variations in properties not considered pertinent to our engineering evaluation may have been abbreviated or omitted for clarity. The boring profiles represent the conditions at the particular boring location and variations do occur among the borings.

6.2 Groundwater Conditions

The depth to groundwater, where encountered, was measured at each boring location. If groundwater was not encountered to the termination depth of the boring, it was reported as GNE (Groundwater Not Encountered) on the soil profiles. Groundwater information at each of the boring

locations is summarized in Table 3 (Appendix B) and is shown on the Roadway Soil Profiles (Appendix B).

Note that groundwater was not encountered in all of the roadway borings at the time of drilling. However, the silty/clayey sands and sandy clays comprising Stratums 3, 4, 5, 6 and 7 (A-2-4, A-2-6, A-4, A-6 and A-7-6 materials) are relatively slow draining, and perched/slowly infiltrating groundwater should be expected in/above these soils during periods of increased rainfall.

Groundwater levels will fluctuate with rainfall and could vary several feet during typical seasonal fluctuations. Larger fluctuations are possible under severe weather conditions. Estimated “normal” seasonal high groundwater elevations are discussed below.

6.3 Estimated “Normal” Seasonal High Groundwater Elevations

Estimated “Normal” Seasonal High Water Table (SHWT) elevations are provided for each boring in which groundwater was encountered at the time of drilling. This information is shown on each boring profile (Soil Profile sheets, Appendix B) and is summarized in Table 3 (Appendix B).

To assist in estimating these elevations, recent rainfall data was obtained from the weather station at Pensacola International Airport (Station KFLPENSA29) located approximately 7 miles southeast of the site. The table below summarizes the data collected.

RAINFALL DATA – PENSACOLA AIRPORT WEATHER STATION KFLPENSA29			
Month	Normal Average (inches)	Actual (inches)	Delta (inches)
January 2016	4.63	3.94	-0.69
February 2016	5.06	4.71	-0.35
March 2016	5.81	7.65	+1.84
April 2016	4.32	6.00	+1.68
May 2016	3.24 <small>(As of 05/24/2016)</small>	1.88 <small>(As of 05/24/2016)</small>	-1.36 <small>(As of 05/24/2016)</small>

As can be seen from the previous Rainfall Data table, the groundwater data collected in late April was during a period of above-normal seasonal rainfall, and the groundwater data collected in May

was during a period of slightly above-normal seasonal rainfall. Therefore, the groundwater levels measured in April are likely more reflective of above-normal seasonal water levels whereas the groundwater levels measured in May are likely more reflective of near-normal to slightly above-normal seasonal water levels.

The estimated “normal” seasonal high water table (SHWT) elevations on the Soil Profiles and in Table 3 (Appendix B) were estimated based on the subsurface conditions encountered in the preliminary borings, the rainfall data collected, the distance to natural and/or man-made drainage features (e.g. stains on culverts and headwalls), and/or natural indicators (e.g. soil color changes within the boring profile). Note that in some cases, the estimated “normal” SHWT elevation is shown above a soil stratum expected to result in perched groundwater.

7.0 ENGINEERING EVALUATION AND RECOMMENDATIONS

7.1 General

In general, the existing shallow subsurface soils encountered in the preliminary roadway borings are suitable for supporting the proposed roadway after subgrade preparation in accordance with FDOT and/or Escambia County specifications. Some plastic soils were identified in the preliminary borings (see Plastic Soil Delineation sheets, Appendix B), and should be removed and used in accordance with FDOT Standard Indices 500 and 505 and/or Escambia County specifications.

Site preparation should consist of normal clearing and grubbing operations followed by compaction of the subgrade soils. Backfill should consist of materials conforming to FDOT Design Standard Index 505 and/or Escambia County specifications. Clearing and grubbing and compaction should be accomplished in accordance with the FDOT and/or Escambia County specifications.

7.2 Embankment Settlement

Based on the subsurface conditions encountered and the planned roadway grades, embankment settlements should be minimal (less than 1 inch) and any settlement experienced should occur during or shortly after placement of the fill.

7.3 Slope Stability

Based on the subsurface conditions encountered, the minimal embankment heights (less than 4 feet), and the planned embankment slopes, slope stability should not be a concern on this project.

7.4 Excavations and Temporary Side Slopes

Temporary side slopes and excavations should comply with the Occupational Safety and Health Administration's (OSHA) trench safety standards, 29 C.F.R., s. 1926.650, Subpart P, all subsequent revisions or updates of OSHA's referenced standard adopted by the Department of Labor and Employment Security and Florida's Trench Safety Act, Section 553.62, Florida Statutes. Excavated materials should not be stockpiled at the top of the slope within a horizontal distance equal to the excavation depth.

7.5 Groundwater Control

Shallow groundwater was identified throughout the project alignment and will require special consideration during construction. Depending upon groundwater levels at the time of construction, some form of dewatering may be required to achieve the required compaction. During subgrade soil preparation, any plastic soils below design grade could become disturbed by construction activities. If this occurs, the Contractor may be directed to remove the disturbed or pumping soils to a depth of 12 inches to 18 inches below design grade and backfill the area with structural fill. In such situations, FDOT Specifications should be followed.

7.6 Pavement Design

Based on discussions with Hatch Mott MacDonald, Inc., the roadway will be classified as an Urban/Rural Connector. The design ESALs will be 830,000 (Traffic Level B). The design speed will be 40 mph. From the LBR test results, the design resilient modulus value will be 10,000 psi.

Two pavement sections are required for this project – a New/Reconstruction section for areas to be widened and where the existing pavement is to be completely removed and replaced, and a Rehabilitation section where the existing pavement will be milled and resurfaced. The pavement design for each section is presented in the following sections. The pavement designs were performed in accordance with the FDOT Flexible Pavement Design Manual.

7.6.1 New/Reconstruction Pavement Section

Shown below is the pavement section for New/Reconstruction Areas to meet the calculated Required Structural Number (SN_R) of 3.42:

- 1 inch Friction Course FC-9.5 (PG 76-22)
- 1.5 inches Type SP Structural Course (PG 76-22)
- Optional Base Group (OBG) 6 (GAB or Asphalt Base Course Type B-12.5)
- 6.5 inches GAB Subbase (LBR 100)

Note that PG 76-22 binder is shown for the Friction Course and Type SP Structural Course. This is recommended in slow moving or standing truck traffic areas to increase the stiffness of the pavement in an attempt to reduce the occurrence/severity of rutting.

7.6.2 Rehabilitation Pavement Section

For the Rehabilitation pavement section, full depth cracking was reported in the Pavement Condition Survey (PCS), and based on the PCS and the conditions observed at the time of this report, the existing pavement is deemed to be in Fair condition.

To reduce/delay the occurrence of reflective cracking and to achieve the Required Structural Number (SN_R) of 3.42 with minimal overbuild, we recommend a 3 inch mill depth and a 3.5 inch overlay. This results in a ½ inch overbuild, which we understand is acceptable. Shown below is the pavement section for Rehabilitation pavement areas to meet the calculated Required Structural Number (SN_R):

- 1 inch new Friction Course FC-9.5 (PG 82-22)
- 2.5 inches new Type SP Structural Course (PG 82-22)
- 1 inch existing Structure Course
- 6 inches existing Sand Clay base
- 12 inches existing Type B Stabilized Subgrade

Note that a PG 82-22 binder is shown for the new Friction Course and Type SP Structural Course. This is recommended to assist in reducing/delaying the occurrence of reflective cracking in the new pavement.

8.0 REPORT LIMITATIONS

The recommendations submitted are based on the available soil information obtained by Tierra, Inc. and design details furnished by HMM, Inc. for the subject project. If there are any revisions to the plans for this project or if deviations from the subsurface conditions noted in this report are encountered during construction, Tierra should be notified immediately to determine if changes in the foundation, or other, recommendations are required. If Tierra is not retained to perform these functions, we cannot be responsible for the impact of such conditions on the performance of the project.

The findings, recommendations, specifications, and professional advice contained herein have been made in accordance with generally accepted professional Geotechnical engineering practices in the local area.

After the plans and specifications are more complete, the Geotechnical engineer should be provided the opportunity to review the final design plans and specifications to assure our engineering recommendations have been properly incorporated into the design documents. At that time, it may be necessary to submit supplementary recommendations. This report has been prepared for the exclusive use of HMM, Inc. for the specific application to the subject project.

APPENDIX A

Project Location Map

USGS Quadrangle Map

USDA Soil Survey Map

Table 1 – Summary of USDA Soil Survey

Potentiometric Surface Map



MOBILE HIGHWAY

WEST NINE MILE ROAD

SR 29 (PENSACOLA BOULEVARD)

PINE FOREST ROAD

I-10

DETROIT BOULEVARD

APPROXIMATE BEGIN PROJECT

APPROXIMATE END PROJECT

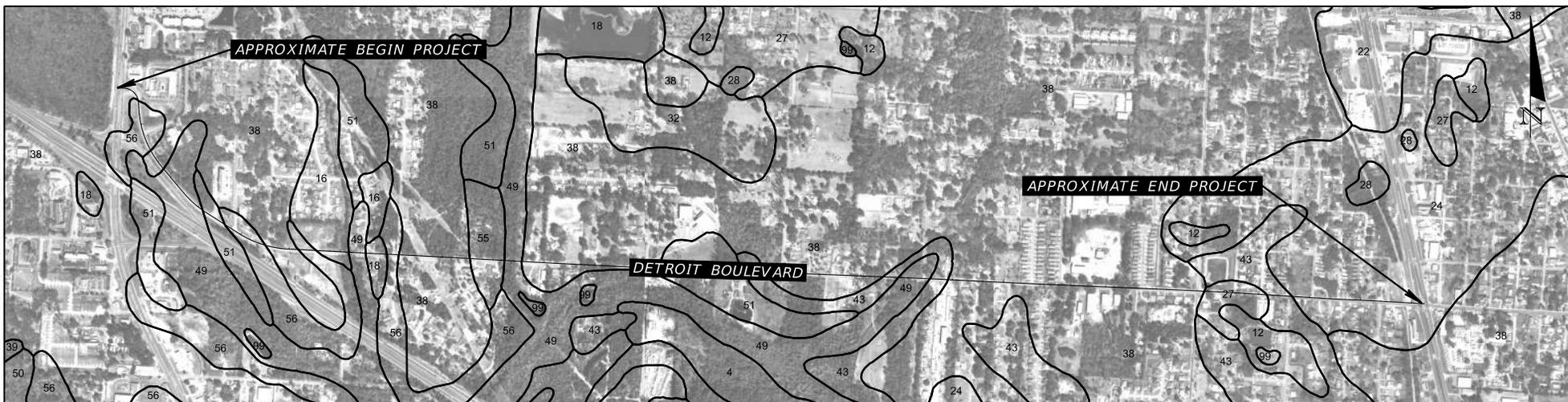
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

MITCHELL L. SMITH, P.E.
 P.E. LICENSE NUMBER 43416
 TIERRA, INC.
 1300 WEST MAIN STREET
 PENSACOLA, FLORIDA 32502
 CERTIFICATE OF AUTHORIZATION NO. 6486

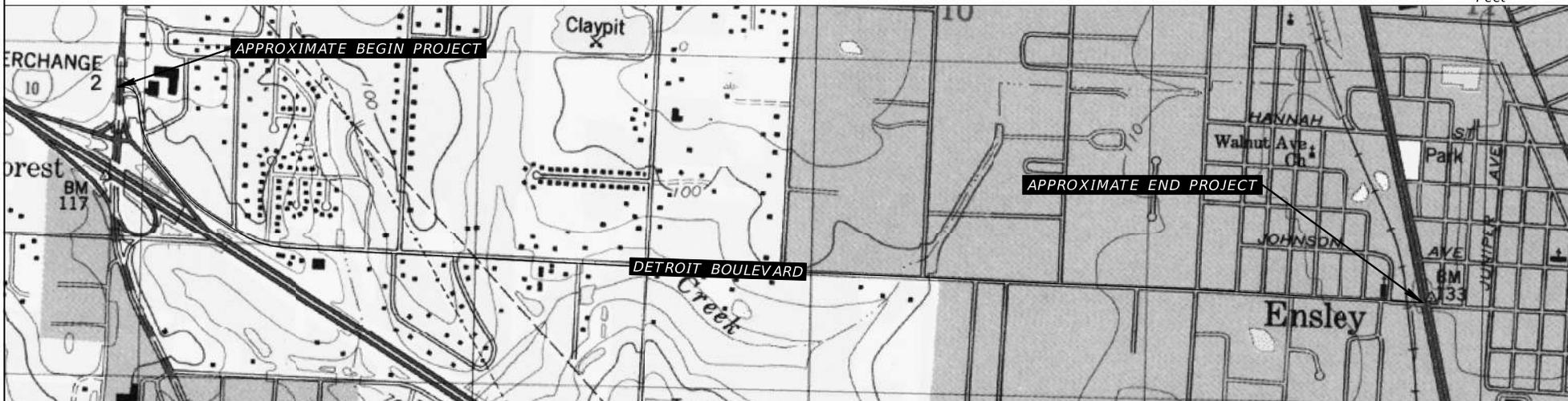
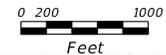
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
	ESCAMBIA	

PROJECT LOCATION MAP

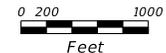
SHEET NO.



REFERENCE: USDA SOIL SURVEY OF ESCAMBIA COUNTY, FLORIDA



REFERENCE: USGS QUADRANGLE MAP OF "CANTONMENT, FLORIDA"



TOWNSHIP: 1S 1S
 RANGE: 30W 31W
 SECTION: 10, 11, 22, & 23 12 & 13

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

MITCHELL L. SMITH, P.E.
 P.E. LICENSE NUMBER 43416
 TIERRA, INC.
 1300 WEST MAIN STREET
 PENSACOLA, FLORIDA 32502
 CERTIFICATE OF AUTHORIZATION NO. 6486

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
	ESCAMBIA	

USDA SOIL SURVEY &
 USGS QUADRANGLE MAPS

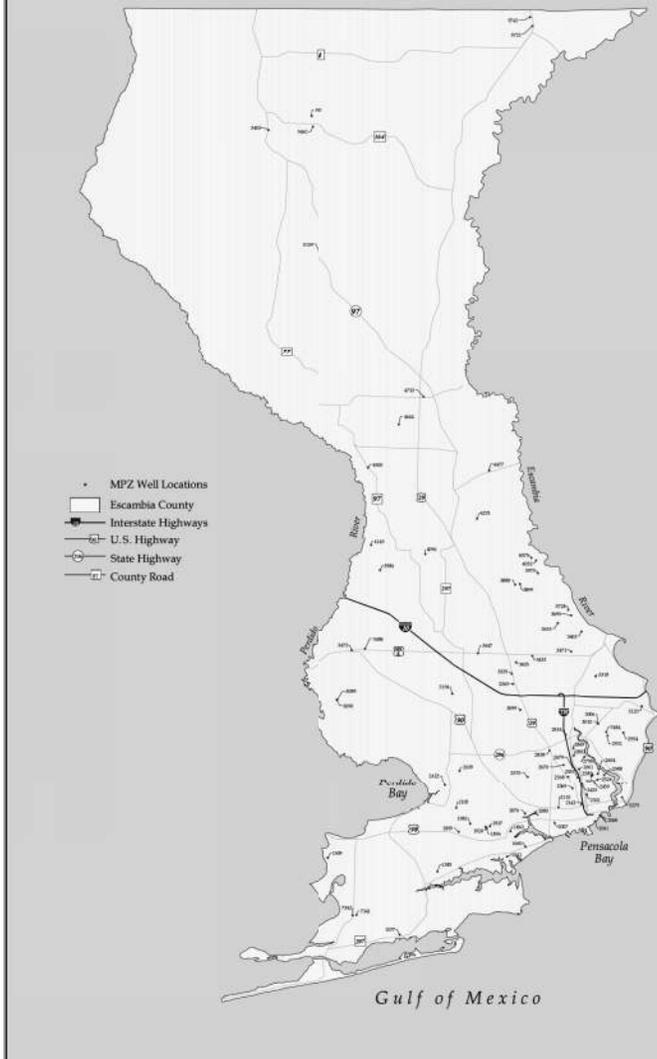
SHEET NO.

TABLE 1
SUMMARY OF USDA SOIL SURVEY
DETROIT BOULEVARD SAFETY UPGRADES
ESCAMBIA COUNTY, FLORIDA
TIERRA PROJECT NO. 4511-14-004

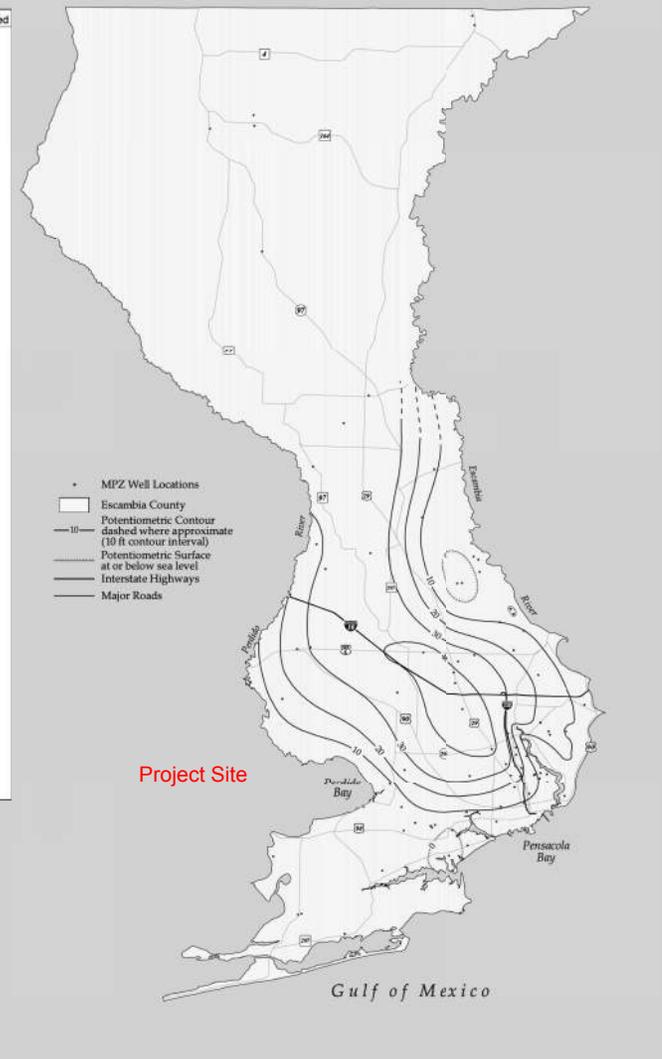
USDA Map Symbol and Soil Name	Depth (in)	Soil Classification		Permeability (in/hr)	pH	Seasonal High Water Table		Shrink/Swell Potential	Organic Matter, %	Risk of Corrosion							
		USCS	AASHTO			Depth (feet)	Months			Uncoated Steel	Concrete						
(12) Croatan	0-25	PT	A-8	0.1 - 6.0	1.8-4.4	0.0-0.5	Jan-July,Dec	Low	42	High	High						
	25-33	CL,CL-ML,SC,SC-SM	A-4,A-6	0.2 - 2.0	3.5-6.5												
	33-80	SC,SC-SM,SM	A-4	0.2 - 20.0	3.5-6.5												
(16) Arents - Urban land	0-80	SP,SP-SM	A-3	20.0 - 50.0	3.5-6.5	1.5-3.0	Jan-April,Dec	Low	1	High	Moderate						
	---	---	---	0.0 - 0.0	---	---	Jan-Dec	---	---	---	---						
(18) Pits	---	---	---	0.0 - 0.0	---	---	Jan-Dec	---	---	---	---						
(24) Poarch	0-5	SC-SM,SM	A-2-4,A-4	2.0 - 6.0	4.5-5.5	2.5-5.0	Jan-April,Dec	Low	1	Low	High						
	5-31	CL,CL-ML,ML	A-4	0.6 - 2.0	4.5-5.5												
	31-80	CL,CL-ML,ML	A-4	0.2 - 0.6	4.5-5.5												
(27) Escambia	0-5	SC-SM,SM	A-4	2.0 - 6.0	3.5-5.5	1.5-2.5	Jan-April,Dec	Low	1	Moderate	High						
	5-10	SC-SM,SM	A-4	2.0 - 6.0	3.5-5.5												
	10-24	CL,CL-ML,SC,SC-SM	A-4,A-6	0.6 - 2.0	3.5-5.5												
(38) Bonifay	0-3	SM	A-2-4	6.0 - 20.0	4.5-6.5	3.5-5.0	Jan-April,Dec	Low	2	Low	High						
	3-54	SM	A-2-4	6.0 - 20.0	4.5-6.0												
	54-80	SC,SC-SM,SM	A-2-4,A-2-6,A-4,A-6	0.2 - 0.6	4.5-5.5												
(43) Albany	0-7	SM,SP-SM	A-2	6.0 - 20.0	3.5-6.5	1.0-2.5	Jan-April,Dec	Low	2	High	High						
	7-57	SM,SP-SM	A-2	6.0 - 20.0	3.5-6.5												
	57-72	SM	A-2	2.0 - 6.0	4.5-6.0												
	72-80	SC,SC-SM,SM	A-2,A-4,A-6	0.2 - 2.0	4.5-6.0												
(49) Dorovan - Fluvaquents	0-8	PT	A-8	0.6 - 2.0	3.5-5.0	0.0-0.5	Jan-Sept,Dec	Low	50	High	High						
	8-80	PT	A-8	0.6 - 2.0	3.5-5.0												
	0-4	SM,SP,SP-SM	A-2-4,A-3	6.0 - 20.0	5.6-7.8												
	4-25	SM	A-2-4	0.6 - 2.0	6.6-7.8			Low	2	High	Moderate						
	25-40	SC,SC-SM,SM	A-2-4,A-2-6	0.6 - 20.0	5.6-7.8												
	40-80	SP,SP-SM	A-3	6.0 - 20.0	5.6-7.8												
(51) Pelham	0-5	SM	A-2	6.0 - 20.0	3.5-5.5	0.0-0.5	Jan-April,Dec	Low	2	High	High						
	5-35	SM,SP-SM	A-2-4,A-3	6.0 - 2.0	3.5-5.5												
	35-58	SC,SC-SM,SM	A-2,A-4,A-6	0.6 - 2.0	3.5-5.5												
	58-80	CL,ML,SC,SM	A-2,A-4,A-6,A-7	0.2 - 2.0	3.5-5.5												
(55) Troup - Poarch	0-5	SM,SP-SM	A-2	6.0 - 20.0	4.5-6.0	---	Jan-Dec	Low	1	Low	Moderate						
	5-58	SM,SP-SM	A-2	6.0 - 20.0	4.5-6.0												
	58-68	SC,SC-SM,SM	A-2,A-4,A-6	0.6 - 2.0	4.5-5.5												
	68-80	CL,CL-ML,SC,SC-SM	A-2,A-4,A-6	0.6 - 2.0	4.5-5.5												
	0-5	SC-SM,SM	A-2-4,A-4	2.0 - 6.0	4.5-5.5							2.5-5.0	Jan-April,Dec	Low	1	Low	High
	5-31	CL,CL-ML,ML	A-4	0.6 - 2.0	4.5-5.5												
31-80	CL,CL-ML,ML	A-4	0.2 - 0.6	4.5-5.5													
(56) Troup - Poarch	0-5	SM,SP-SM	A-2	6.0 - 20.0	4.5-6.0	---	Jan-Dec	Low	1	Low	Moderate						
	5-58	SM,SP-SM	A-2	6.0 - 20.0	4.5-6.0												
	58-68	SC,SC-SM,SM	A-2,A-4,A-6	0.6 - 2.0	4.5-5.5												
	68-80	CL,CL-ML,SC,SC-SM	A-2,A-4,A-6	0.6 - 2.0	4.5-5.5												
	0-5	SC-SM,SM	A-2-4,A-4	2.0 - 6.0	4.5-5.5							2.5-5.0	Jan-April,Dec	Low	1	Low	High
	5-31	CL,CL-ML,ML	A-4	0.6 - 2.0	4.5-5.5												
31-80	CL,CL-ML,ML	A-4	0.2 - 0.6	4.5-5.5													

⁽¹⁾ AASHTO and USCS do not provide classification for weathered/unweathered bedrock

POTENTIOMETRIC SURFACE OF THE MAIN PRODUCING ZONE OF THE SAND-AND-GRAVEL AQUIFER, ESCAMBIA COUNTY, FLORIDA
OCTOBER/NOVEMBER 2000



Nwl Id	Site Id	Well Name	Elev	Water Level (lcl)	Water Level (msl)	Date Measured
1077	30194608715302	SANCHEZ INT	17	-10.3	6.8	11-OCT-00
1883	30222087702026	WELLS AVE LPZ	86	-8.6	6.6	11-OCT-00
1509	30230208752501	USGS TH 107	11	-6.8	4.8	01-DEC-00
1641	30232087653001	ESCAMBIA 29 HANNA IB	10	-13.1	-0.2	11-OCT-00
1663	30232087641101	PEOPLES #4A	23	-11.0	0.9	07-NOV-00
1889	30241108790005	ECLA #3-VILLA DR	29	-26.7	3.3	26-OCT-00
1863	30241208765001	PEOPLES #5	30	-24.5	0.7	07-NOV-00
1896	3024208773701	CORRY #9	30	-26.1	3.4	14-NOV-00
1920	30242087745001	CORRY #15	30	-24.5	5.4	14-NOV-00
1937	30242087727001	CORRY #13	27	-26.8	0.7	14-NOV-00
1981	30242087659001	USGS TH 9	27	-26.8	16.3	07-NOV-00
2007	30243508741703	ROMANA ST LPZ	21	-16.7	6.1	11-OCT-00
2041	30244087215001	CENTRAL MAISON PARK	6	-2.0	3.3	08-NOV-00
2083	30245307200003	MUSCOGEE WHARF IRRIG	7	-3.8	6.6	08-NOV-00
2078	30245908754401	USGS TH 103	54	-44.6	9.2	14-NOV-00
2093	30250207613001	WINDHOLM #11	49	-19.4	6.6	14-NOV-00
2105	30251087900401	ECLA #10-DILLIAN	33	-17.9	5.1	25-OCT-00
2110	30251408740001	ECLA #9S-WEST PLANT	42	-20.8	26.0	26-OCT-00
2142	30252308725001	ECLA #11-WE	33	-23.7	9.3	25-OCT-00
2275	30254308755001	WINDHOLM PARK MPZ	42	-20.8	21.1	03-OCT-00
2301	30254908724001	MW-SD	70	-86.1	12.2	04-OCT-00
2369	30260508732301	MW-SD	46	-20.3	16.8	04-OCT-00
2412	302615087764101	AVONDALE STP LPZ	11	-4.3	6.7	25-OCT-00
2430	30261608725001	AC-8D	70	-82.4	17.3	12-OCT-00
2459	30262008721701	AC-10D	87	-86.3	10.8	12-OCT-00
2475	30262508714701	AC-11D	74	-88.2	6.4	12-OCT-00
2526	30263508720601	AC-18D	80	-71.1	26.9	04-OCT-00
2557	30284108730701	ANDERSON ST - MPZ	76	-55.1	20.6	11-OCT-00
2568	30284308739001	AC-5D	85	-68.5	27.4	01-NOV-00
2570	30284308753001	USGS TH 2	87	-27.3	32.7	07-NOV-00
2570	30284408722501	12TH AVE MPZ	79	-67.2	12.0	04-OCT-00
2588	30284708714801	AC-14D	89	-66.5	8.8	04-OCT-00
2589	30284708722701	AC-13D	79	-88.4	11.0	04-OCT-00
2604	30285008720701	AC-16D	78	-88.5	6.6	04-OCT-00
2639	30285708786701	ECLA #30-AVONDALE	80	-31.9	28.2	25-OCT-00
2661	3027508734701	PALM OX IND PK DEEP	88	-85.7	34.3	12-OCT-00
2679	30270708725101	AC-2	88	-82.7	27.0	11-OCT-00
2780	30273408724801	PINEGLADES MPZ	33	-22.3	10.0	03-OCT-00
2803	30273708731801	WOODLAND HEIGHTS MPZ	83	-86.6	23.7	18-OCT-00
2836	30275108743101	BRENTWOOD PARK MPZ	88	-46.8	41.3	03-OCT-00
2949	30275608730001	HEWITT ST - MPZ	84	-82.4	21.1	03-OCT-00
2931	30283008713801	PENSACOLA MUNIC APRT	110	-80.0	12.1	01-DEC-00
2933	30283108734301	BELAMY ST - MPZ	103	-72.1	26.8	12-OCT-00
2964	30283708703001	ECLA #18-IMPORT NOR	119	-113.5	6.7	25-OCT-00
3006	30290008720701	9TH AVE DEEP WELL	93	-76.0	16.6	15-NOV-00
3010	30290108720801	ECLA #17-TH AVENUE	92	-76.0	17.0	25-OCT-00
3089	302929087935001	HUNT CLUB DEEP S&G	9	9.8	18.8	10-OCT-00
3096	302930087955001	HUNT CLUB SHALLOW SD	9	9.6	14.6	10-OCT-00
3099	30293408756701	WEDGEWOOD DEEP	123	-74.4	48.6	24-OCT-00
3120	30294087789001	GULL POINT PARK	101	-83.9	17.1	20-NOV-00
3194	30301508792001	ECLA #25-DUNAWAY	76	-40.8	35.2	26-OCT-00
3265	30304108762001	ECLA #26-BROAD ST	128	-86.5	11.6	26-OCT-00
3319	30310208721401	ECLA #30K DEEP	87	-79.4	41.6	08-NOV-00
3336	30310808762301	USGS DETROIT BLVD	131	-88.9	42.2	08-NOV-00
3405	30314208761001	ECLA #28-EMLEY	134	-88.9	26.7	25-OCT-00
3433	30315408752201	ECLA #28-DLF 4A	130	-88.5	31.5	25-OCT-00
3447	30315808785001	9 MIL F ROAD DEEP	119	-72.4	45.6	19-OCT-00
3471	30320808732701	USGS TH 8	98	-38.5	17.7	15-NOV-00
3472	30320908741101	USGS OBS-724-1A	123	-86.1	18.9	19-OCT-00
3484	30321308734001	BEULAH FIRE DEPT	117	-86.8	30.2	10-OCT-00
3603	3032808725701	UWF #2	87	-76.5	10.5	08-NOV-00
3651	30331808741701	CHRIST PLANT #6	85	-76.0	10.0	16-NOV-00
3665	30333808723001	CHRIST PLANT #3	85	-85.8	-0.8	18-NOV-00
3728	30335008753001	CHRIST PLANT #4	100	-27.0	-2.0	15-NOV-00
3888	30345808781401	MONSANTO #D (#14)	100	-108.2	-8.2	21-NOV-00
3889	30350108755001	MONSANTO #C (#13)	105	-137.1	-12.1	21-NOV-00
3975	30352808754301	MONSANTO #5	40	-37.2	2.8	21-NOV-00
3989	30353008794001	MUSCOGEE INT	68	-22.8	26.5	19-OCT-00
4052	30356908751401	MONSANTO #2	38	-36.8	2.2	21-NOV-00
4076	30358108752601	MUSCOGEE INT	45	-40.3	4.7	05-OCT-00
4089	30358608704101	FARM HILL #1	157	-118.4	38.6	15-NOV-00
4145	30364208793201	USGS TH 12	108	-85.5	22.5	30-NOV-00
4273	30373408780001	COTTAGE HILL #2	80	-84.5	10.3	20-NOV-00
4477	30394608773001	QUINETTE INTER	147	-16.1	12.9	05-OCT-00
4500	303968087935001	ECLA TH 15	140	-84.0	45.7	18-OCT-00
4544	304155087920101	MOLINO #3	192	-145.9	46.1	18-NOV-00
4710	304304087904801	MOLINO #1	190	-142.1	47.9	16-NOV-00
5159	304822087878002	DOGWOOD PARK MPZ	232	-84.1	137.9	04-OCT-00
5240	30484087849001	WALNUT HILL #2	232	-82.2	187.6	16-NOV-00
5461	304850087854001	OAK GROVE DEEP USGS	259	-91.5	167.7	18-OCT-00
5480	305514087634001	OAK GROVE MEDIUM S&G	232	-88.7	163.3	04-OCT-00
5721	30590908762701	CENTURY #1	132	-132.0	6.4	16-NOV-00
5742	30593808783801	CENTURY #2	75	-17.9	57.2	15-NOV-00
7342	302534087252005	SERPENTO MW-2 WEST	25	-17.2	7.8	07-DEC-00
7484	30253808714201	PENSACOLA AIR #2N	188	-46.1	11.3	01-DEC-00



NORTHWEST FLORIDA
WATER MANAGEMENT DISTRICT
WATER RESOURCE MAP SERIES 01-2
GIS MAP ID 13-05-001

Roger A Countryman, Alan Baker, Tom Pratt and Gary Miller

REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO.	COUNTY	FINANCIAL PROJECT ID	POTENTIOMETRIC MAP	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION						
					ESCAMBIA				

APPENDIX B

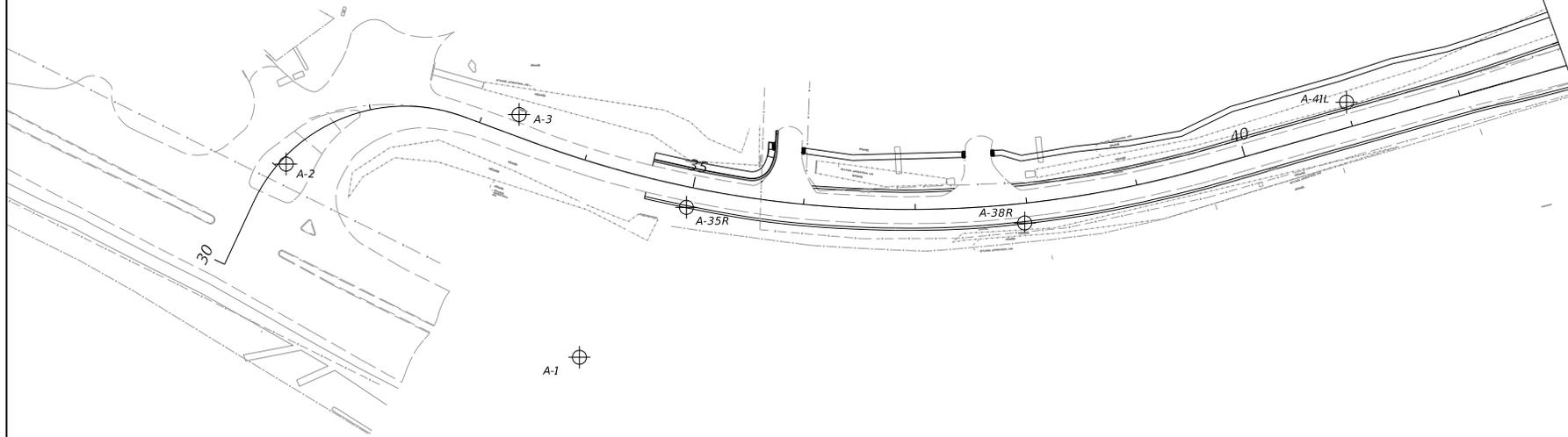
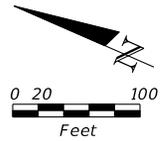
Roadway Boring Location Plan (10 Sheets)

Roadway Soil Profile Sheet

Table 2 – Summary of Laboratory Test Results

Table 3 – Summary of Seasonal High Water Table Estimates

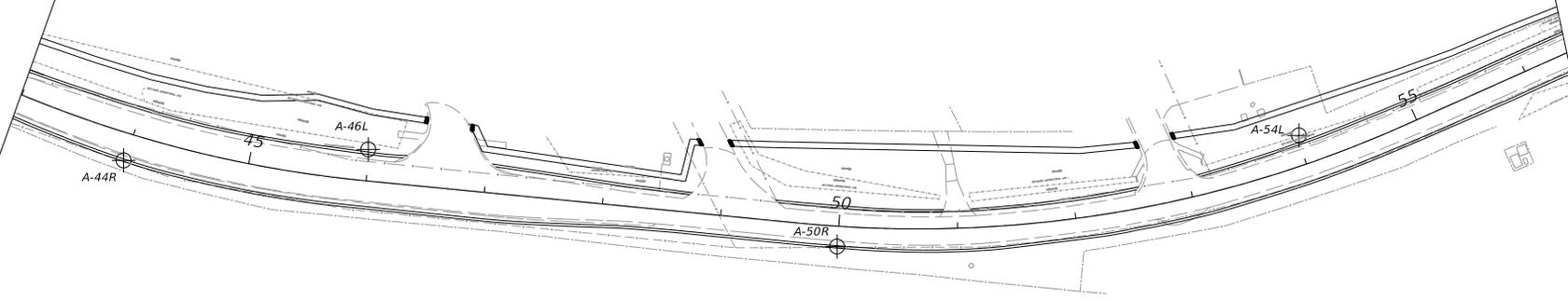
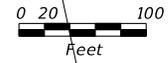
Preliminary Plastic Soil Delineation Plan Sheets (3 Sheets)



LEGEND


 APPROXIMATE AUGER BORING LOCATION

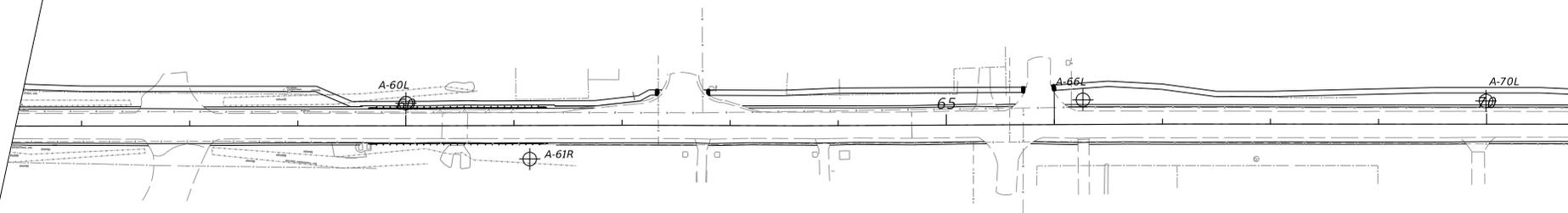
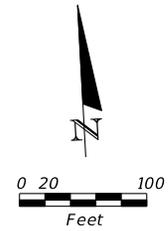
REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO. COUNTY FINANCIAL PROJECT ID			<i>BORING LOCATION PLAN (1)</i>	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ESCAMBIA				



LEGEND


 APPROXIMATE AUGER BORING LOCATION

REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO. COUNTY FINANCIAL PROJECT ID			<i>BORING LOCATION PLAN (2)</i>	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION						

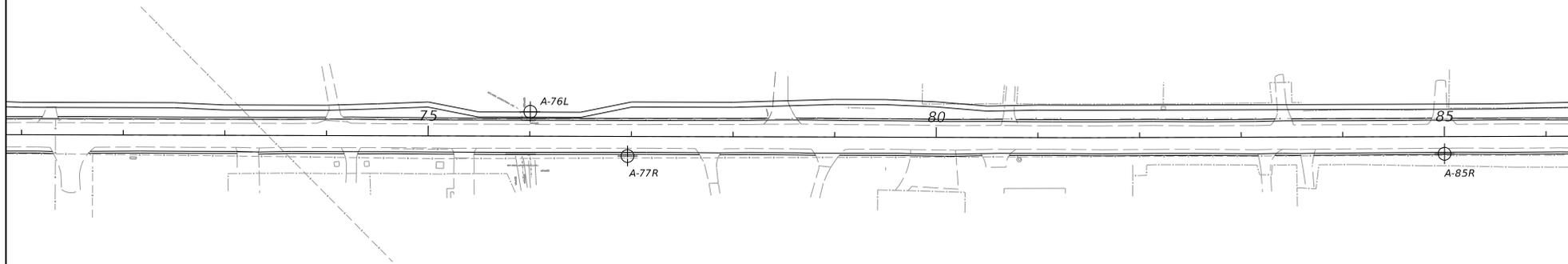
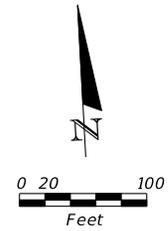


LEGEND

⊕ APPROXIMATE AUGER BORING LOCATION

REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	PROJECT INFORMATION			SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID	

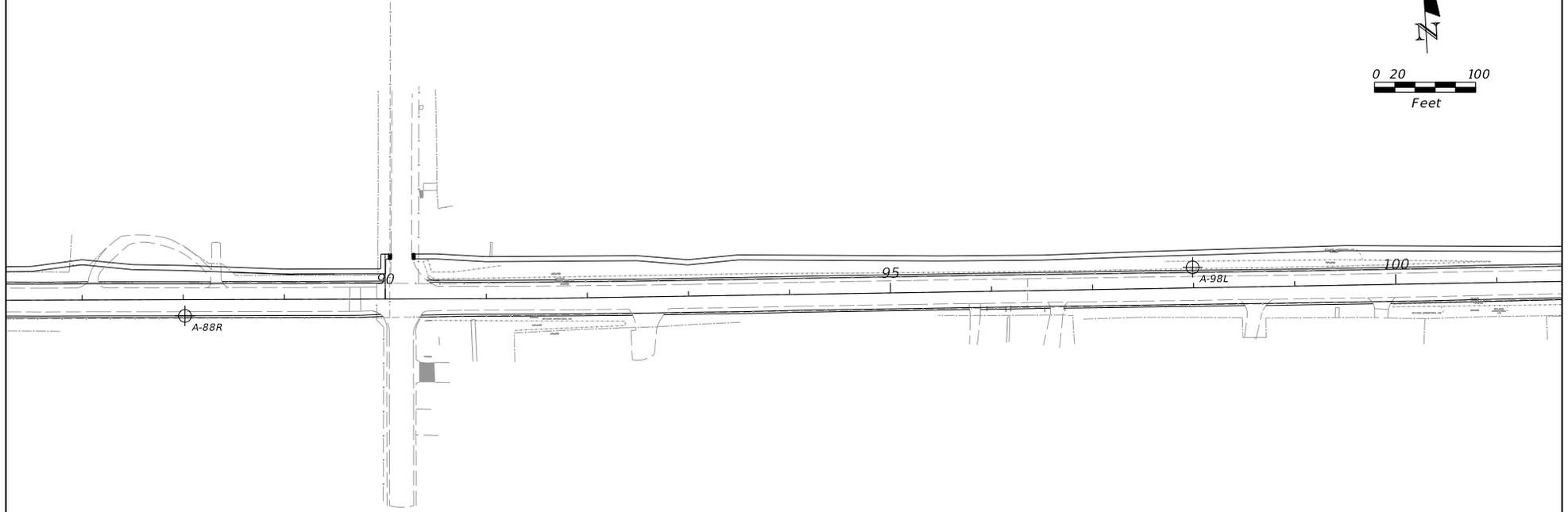
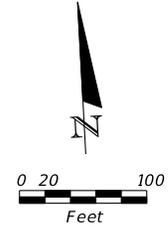
BORING LOCATION PLAN (3)



LEGEND


 APPROXIMATE AUGER BORING LOCATION

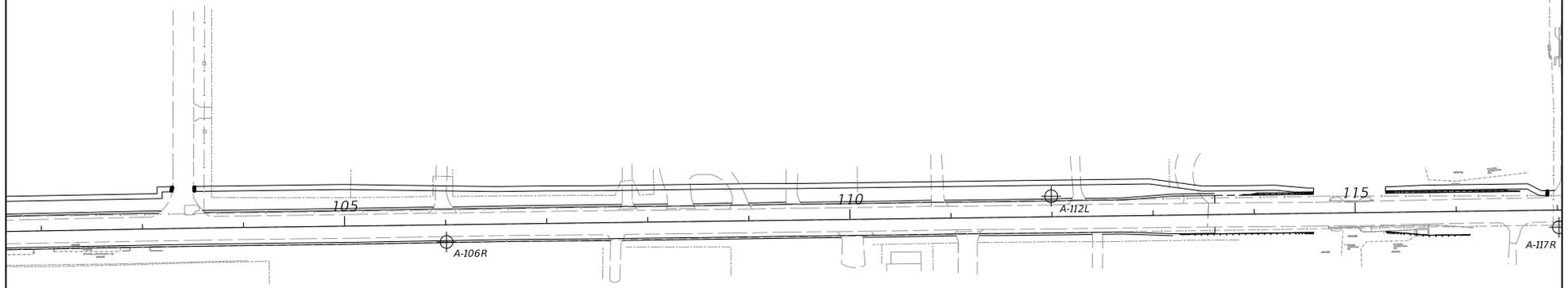
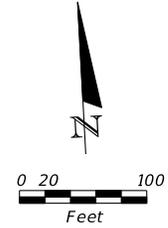
REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO. COUNTY FINANCIAL PROJECT ID			<i>BORING LOCATION PLAN (4)</i>	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ESCAMBIA				



LEGEND


 APPROXIMATE AUGER BORING LOCATION

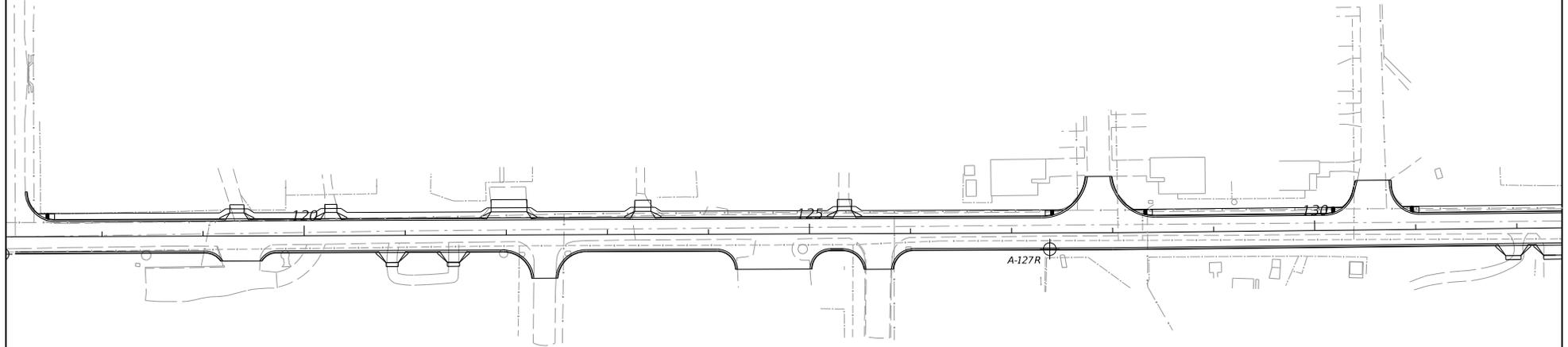
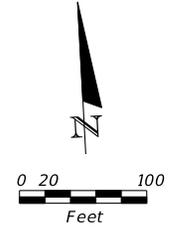
REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO. COUNTY FINANCIAL PROJECT ID			<i>BORING LOCATION PLAN (5)</i>	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ESCAMBIA				



LEGEND


 APPROXIMATE AUGER BORING LOCATION

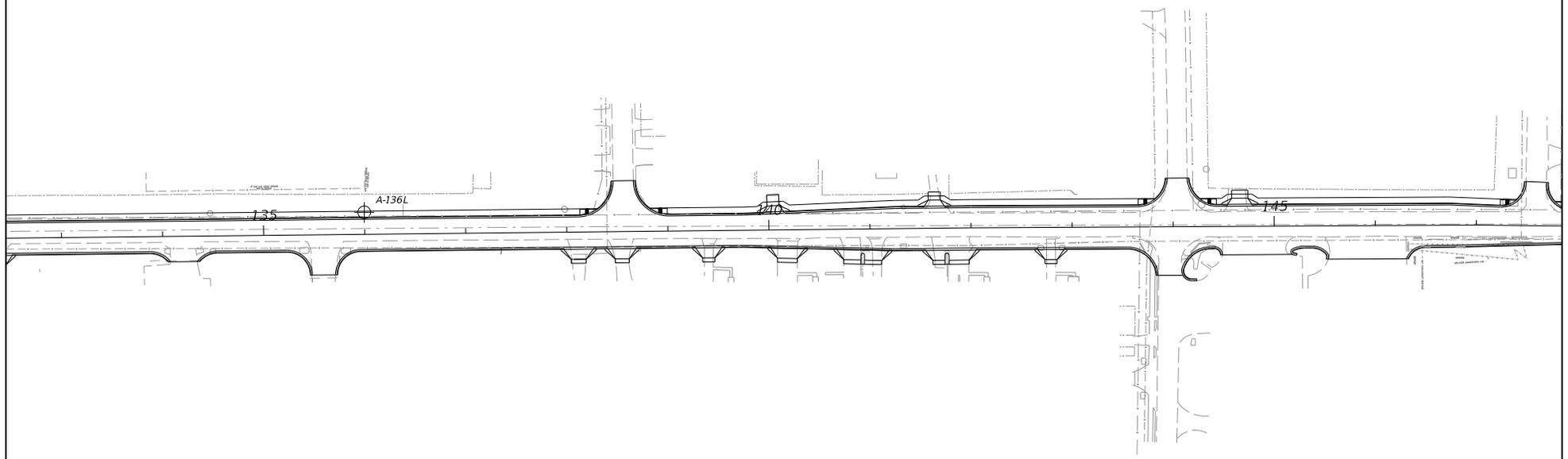
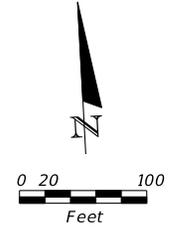
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DATE	DESCRIPTION	DATE	DESCRIPTION		ESCAMBIA				



LEGEND


 APPROXIMATE AUGER BORING LOCATION

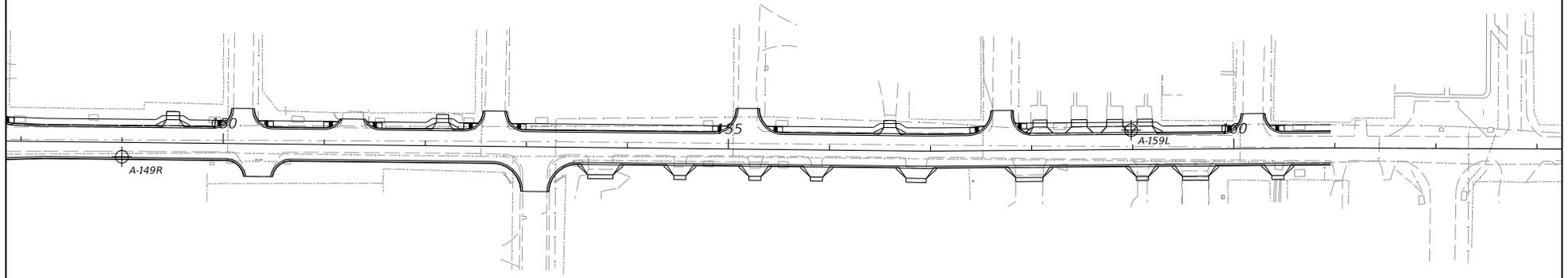
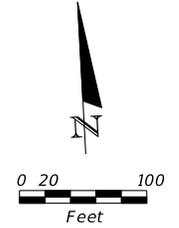
REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO. COUNTY FINANCIAL PROJECT ID			<i>BORING LOCATION PLAN (7)</i>	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ESCAMBIA				



LEGEND

⊕ APPROXIMATE AUGER BORING LOCATION

REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO. COUNTY FINANCIAL PROJECT ID			<i>BORING LOCATION PLAN (8)</i>	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION						



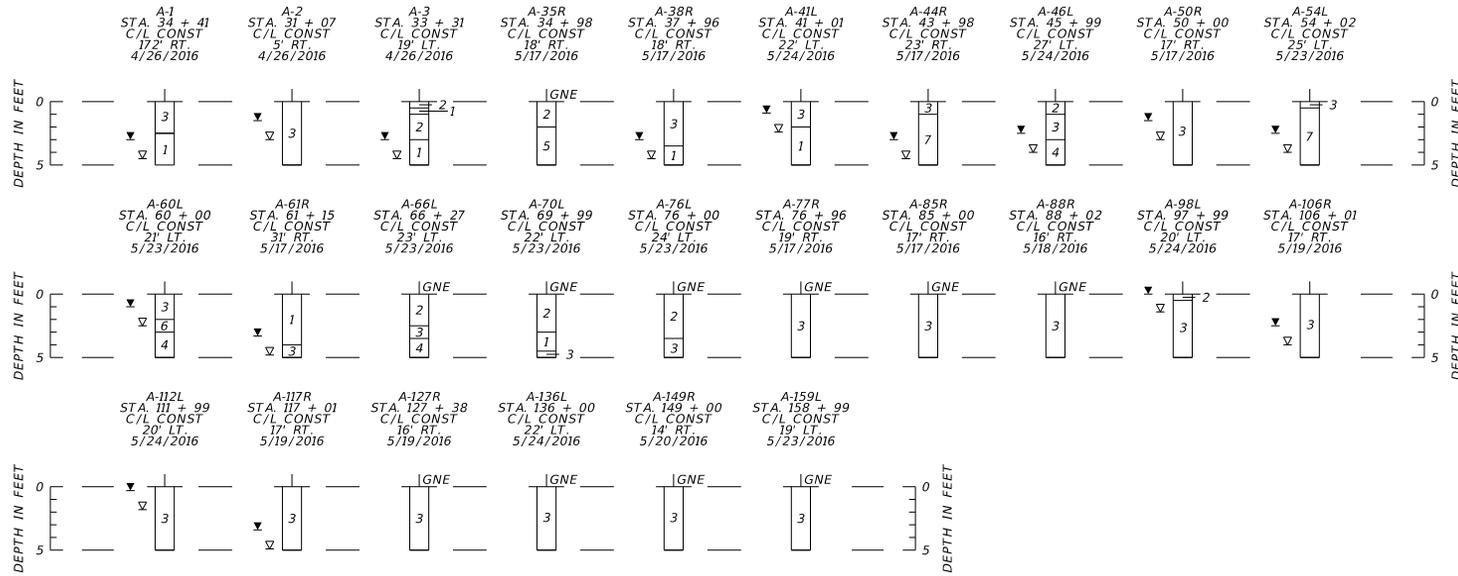
LEGEND


 APPROXIMATE AUGER BORING LOCATION

REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROAD NO. COUNTY FINANCIAL PROJECT ID			<i>BORING LOCATION PLAN (9)</i>	SHEET NO.
DATE	DESCRIPTION	DATE	DESCRIPTION		ESCAMBIA				

LEGEND

1. LIGHT GRAY, GRAY, TAN, BROWN, ORANGE SAND (A-3)
 2. LIGHT GRAY, GRAY, TAN, BROWN, ORANGE SILTY SAND, (A-2-4, 10%-15% FINES)
 3. GRAY, TAN, BROWN, ORANGE, RED SILTY SAND (A-2-4, >15% FINES)
 4. GRAY, TAN, BROWN, ORANGE, RED CLAYEY SAND (A-2-6)
 5. GRAY, TAN, ORANGE SILT (A-4)
 6. TAN, ORANGE, RED CLAY (A-6)
 7. BROWN, ORANGE, RED CLAY (A-7-6)
- A-3 AASHTO GROUP SYMBOL AS DETERMINED BY VISUAL REVIEW AND LABORATORY TESTING ON SELECTED SAMPLES FOR CONFIRMATION OF VISUAL REVIEW.
- ▽ GROUNDWATER LEVEL ENCOUNTERED DURING FIELD EXPLORATIONS
- ▼ ESTIMATED SEASONAL HIGH GROUNDWATER TABLE
- GNE GROUNDWATER NOT ENCOUNTERED



REVISIONS				MITCHELL L. SMITH, P.E. P.E. LICENSE NUMBER 43416 TIERRA, INC. 1300 WEST MAIN STREET PENSACOLA, FLORIDA 32502 CERTIFICATE OF AUTHORIZATION NO. 6486	ROADWAY SOIL PROFILES			SHEET NO.
DATE	DESCRIPTION	DATE	ROAD NO.		COUNTY	FINANCIAL PROJECT ID		

TABLE 2
SUMMARY OF LABORATORY TEST RESULTS
DETROIT BOULEVARD SAFETY UPGRADES
ESCAMBIA COUNTY, FLORIDA
TIERRA PROJECT NO. 4511-14-004

STRATUM	BORING	BORING LOCATION			SAMPLE DEPTH (ft)	ORGANIC CONTENT (%)	MOISTURE CONTENT (%)	SIEVE ANALYSES (% PASSING)					ATTERBERG LIMITS (%)		AASHTO		
		STA (feet)	OFFSET (feet)					#10	#40	#60	#100	#200	LL	PI			
1	A-1	34+41	172	RT	2.5	-	3	-	19	99	83	45	13	4	-	-	A-3
1	A-3	33+31	19	LT	0.5	-	1	-	7	99	92	65	19	9	-	-	A-3
1	A-3	33+31	19	LT	3	-	3.5	-	22	99	69	36	14	4	-	-	A-3
1	A-61R	61+15	31	RT	1	-	1.5	-	7	98	61	33	17	10	-	-	A-3
2	A-1	34+41	172	RT	1	-	2	-	15	99	79	50	24	12	-	-	A-2-4
2	A-3	33+31	19	LT	1	-	2	-	-	100	94	68	20	11	-	-	A-2-4
2	A-66L	66+27	23	LT	0.5	-	1	-	7	96	73	35	18	11	-	-	A-2-4
2	A-70L	69+99	22	LT	0.5	-	1	-	8	100	82	48	26	13	-	-	A-2-4
2	A-76L	76+00	24	LT	0.5	-	1	-	11	99	75	46	26	14	-	-	A-2-4
3	A-1	34+41	172	RT	2	-	2.5	-	26	99	85	58	32	21	-	-	A-2-4
3	A-106R	106+01	17	RT	1.5	-	2	-	11	100	88	65	36	17	-	-	A-2-4
3	A-117R	117+01	17	RT	1.5	-	2	-	12	-	-	-	-	24	22	4	A-2-4
3	A-127R	127+38	16	RT	1.5	-	2.5	-	1	100	83	57	34	22	-	-	A-2-4
3	A-149R	149+00	14	RT	0.5	-	1	-	9	-	-	-	-	20	20	1	A-2-4
3	A-149R	149+00	14	RT	2.5	-	3	-	12	-	-	-	-	27	18	1	A-2-4
3	A-159L	158+99	19	LT	1	-	1.5	-	11	97	80	58	34	19	-	-	A-2-4
3	A-159L	158+99	19	LT	1.5	-	2	-	10	-	-	-	-	21	18	1	A-2-4
3	A-2	31+07	5	RT	4	-	4.5	-	15	98	74	45	26	20	-	-	A-2-4
3	A-38R	37+96	18	RT	1.5	-	2	-	10	-	-	-	-	19	18	1	A-2-4
3	A-46L	45+99	27	LT	1.5	-	2	-	10	92	77	52	32	20	-	-	A-2-4
3	A-50R	50+00	17	RT	0.5	-	1	-	10	97	80	54	30	17	-	-	A-2-4
3	A-50R	50+00	17	RT	4	-	4.5	-	19	-	-	-	-	24	28	8	A-2-4
3	A-61R	61+15	31	RT	4	-	4.5	-	30	-	-	-	-	23	25	2	A-2-4
3	A-76L	76+00	24	LT	3.5	-	4	-	16	-	-	-	-	26	20	2	A-2-4
3	A-98L	97+99	20	LT	1	-	1.5	-	23	99	82	60	36	22	-	-	A-2-4
3	A-98L	97+99	20	LT	0.5	-	1	-	19	-	-	-	-	27	24	6	A-2-4
4	A-46L	45+99	27	LT	3	-	3.5	-	18	95	83	63	45	33	-	-	A-2-6

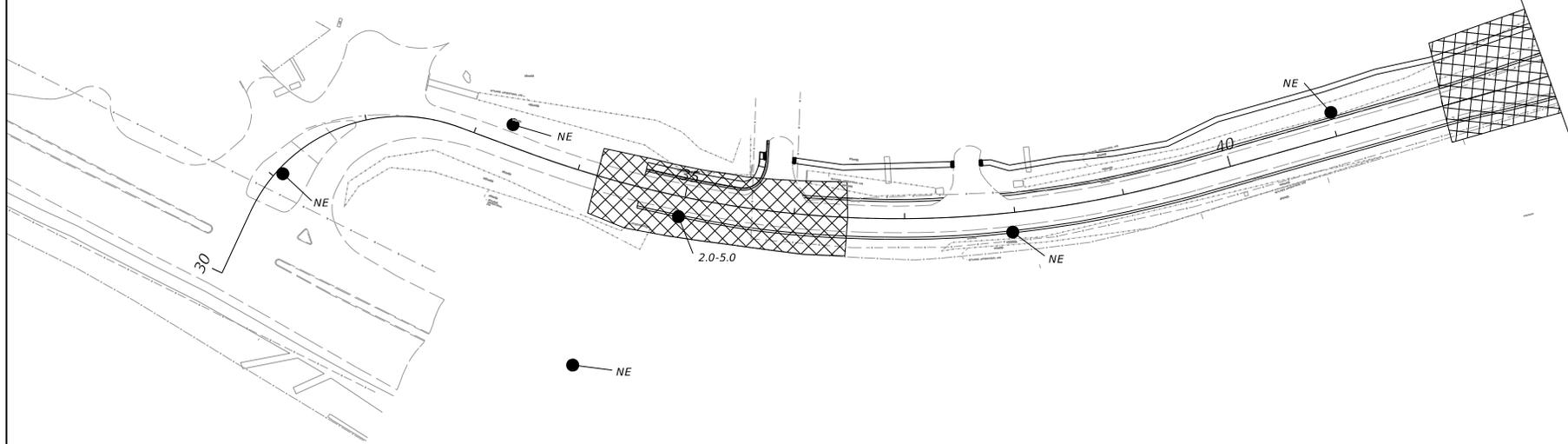
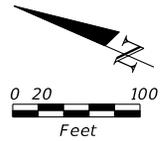
TABLE 3
GROUNDWATER ELEVATION DATA & ESTIMATED "NORMAL" SEASONAL HIGH WATER TABLE ELEVATION
DETROIT BOULEVARD SAFETY UPGRADES
ESCAMBIA COUNTY, FLORIDA
TIERRA PROJECT NO. 4511-14-004

BORING	STATION	OFFSET (ft) ⁽¹⁾		GROUND SURFACE ELEVATION (ft. NAVD88)	GROUNDWATER ELEVATION (ft. NAVD88)	
					MEASURED AT TIME OF BORING	ESTIMATED NORMAL SEASONAL HIGH ⁽²⁾
A-1	34+41	172	RT	102.1	97.6	99.1
A-2	31+07	5	RT	111.1	108.1	109.6
A-3	33+31	19	LT	103.2	98.7	100.2
A-35R	34+98	18	RT	98.7	GNE	N/A ⁽³⁾
A-38R	37+96	18	RT	92.7	88.2	89.7
A-41L	41+01	22	LT	87.0	84.6	86.1
A-44R	43+98	23	RT	92.0	87.5	89.0
A-46L	45+99	27	LT	89.7	85.7	87.2
A-50R	50+00	17	RT	93.1	90.1	91.6
A-54L	54+02	25	LT	86.5	82.5	84.0
A-60L	60+00	21	LT	76.1	73.6	75.1
A-61R	61+15	31	RT	76.4	71.6	73.1
A-66L	66+27	23	LT	95.3	GNE	N/A ⁽³⁾
A-70L	69+99	22	LT	93.3	GNE	N/A ⁽³⁾
A-76L	76+00	24	LT	72.9	GNE	N/A ⁽³⁾
A-77R	76+96	19	RT	77.9	GNE	N/A ⁽³⁾
A-85R	85+00	17	RT	77.3	GNE	N/A ⁽³⁾
A-88R	88+02	16	RT	76.0	GNE	N/A ⁽³⁾
A-98L	97+99	20	LT	84.2	82.8	84.2
A-106R	106+01	17	RT	99.5	95.5	97.0
A-112L	111+99	20	LT	95.8	94.0	95.5
A-117R	117+01	17	RT	97.8	92.9	94.4
A-127R	127+38	16	RT	102.9	GNE	N/A ⁽³⁾
A-136L	136+00	22	LT	111.5	GNE	N/A ⁽³⁾
A-149R	149+00	14	RT	114.1	GNE	N/A ⁽³⁾
A-159L	158+99	19	LT	122.5	GNE	N/A ⁽³⁾

Note: (1) Reference is CL of Construction

(2) Seasonal High Water Table may be controlled by drainage features where present/designed.

(3) Seasonal High Water Table can not be determined since groundwater was not encountered at the time of drilling.



LEGEND

- NE ● PLASTIC SOIL NOT ENCOUNTERED (NE) WITHIN BORING DEPTH
- 2.0-2.5 ● APPROXIMATE DEPTH RANGE OF PLASTIC SOIL ENCOUNTERED (FT.)
- ▨ APPROXIMATE LIMITS OF PLASTIC SOIL ENCOUNTERED

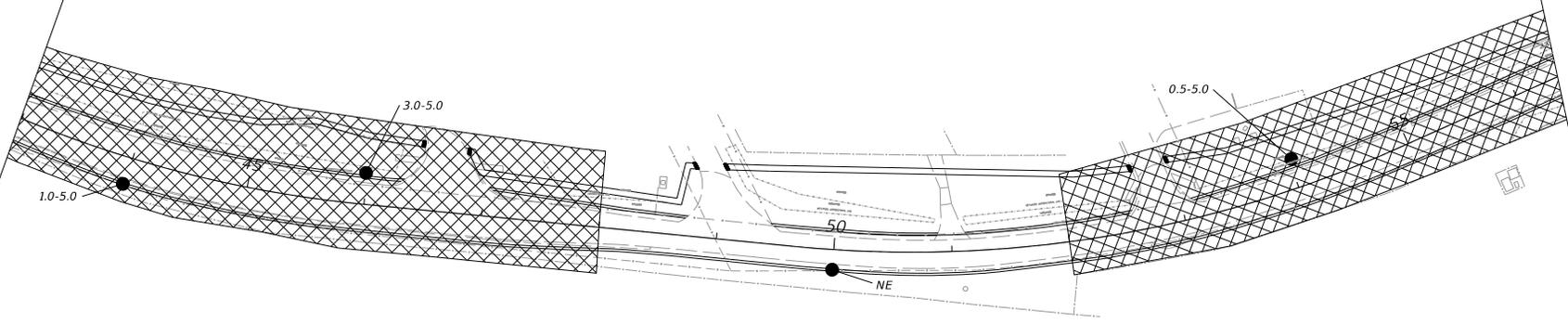
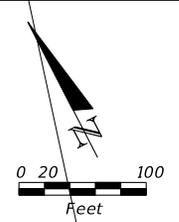
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

MITCHELL L. SMITH, P.E.
 P.E. LICENSE NUMBER 43416
 TIERRA, INC.
 1300 WEST MAIN STREET
 PENSACOLA, FLORIDA 32502
 CERTIFICATE OF AUTHORIZATION NO. 6486

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
	ESCAMBIA	

**PLASTIC SOIL
 DELINEATION PLAN (1)**

SHEET NO.



LEGEND

- NE ● PLASTIC SOIL NOT ENCOUNTERED (NE) WITHIN BORING DEPTH
- 2.0-2.5 ● APPROXIMATE DEPTH RANGE OF PLASTIC SOIL ENCOUNTERED (FT.)
- APPROXIMATE LIMITS OF PLASTIC SOIL ENCOUNTERED

REVISIONS

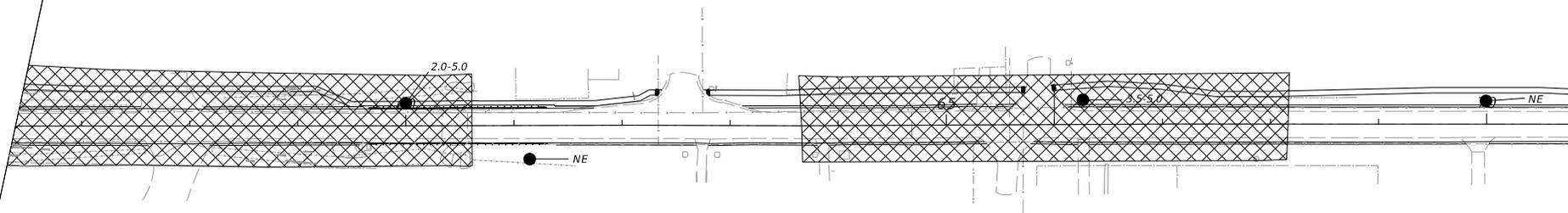
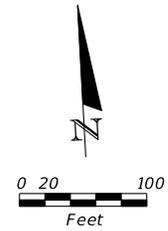
DATE	DESCRIPTION	DATE	DESCRIPTION

MITCHELL L. SMITH, P.E.
 P.E. LICENSE NUMBER 43416
 TIERRA, INC.
 1300 WEST MAIN STREET
 PENSACOLA, FLORIDA 32502
 CERTIFICATE OF AUTHORIZATION NO. 6486

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
	ESCAMBIA	

**PLASTIC SOIL
 DELINEATION PLAN (2)**

SHEET NO.



LEGEND

- NE PLASTIC SOIL NOT ENCOUNTERED (NE) WITHIN BORING DEPTH
- 2.0-2.5 APPROXIMATE DEPTH RANGE OF PLASTIC SOIL ENCOUNTERED (FT.)
- ▨ APPROXIMATE LIMITS OF PLASTIC SOIL ENCOUNTERED

REVISIONS

DATE	DESCRIPTION	DATE	DESCRIPTION

MITCHELL L. SMITH, P.E.
 P.E. LICENSE NUMBER 43416
 TIERRA, INC.
 1300 WEST MAIN STREET
 PENSACOLA, FLORIDA 32502
 CERTIFICATE OF AUTHORIZATION NO. 6486

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
	ESCAMBIA	

**PLASTIC SOIL
 DELINEATION PLAN (3)**

SHEET NO.

TIERRA

1300 W. Main Street Pensacola, Florida 32502
Phone: (850) 462-8774 • Fax: (850) 607-6953
FDOT Laboratory No. I03066

RESULTS OF LABORATORY TESTING

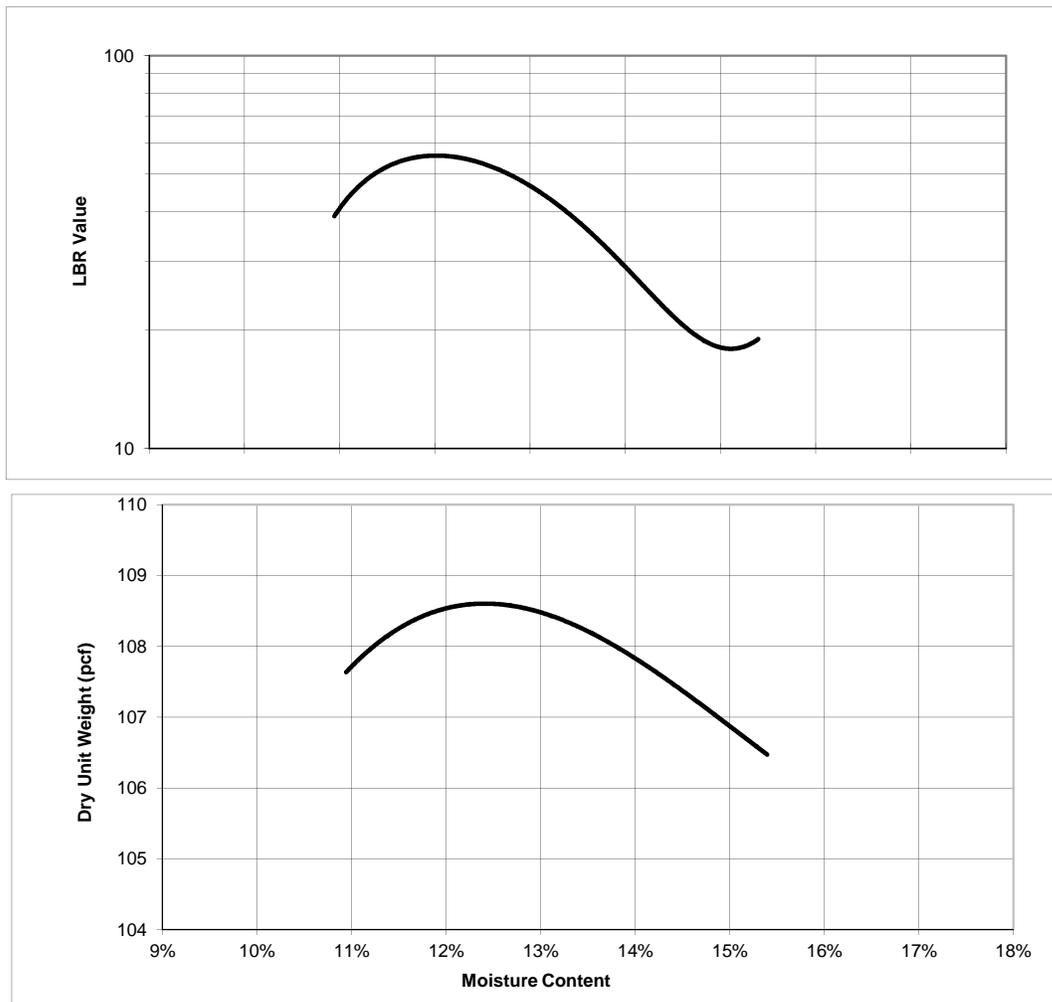
Tested For: Mr. Thomas McLendon, PE
Hatch Mott MacDonald of Florida, Inc.
5111 North 12th Avenue
Pensacola, Florida 32504

Project: Detroit Boulevard Safety Improvements
Escambia County, Florida

Tierra Project No: 4111-14-004
Intended Use: Design
Sample No: A-3, Station 33+31, Offset 19' LT

Report Date: 4/27/2016

LBR & MOISTURE-DENSITY RELATIONSHIP



LBR Value 54
Maximum Density 109 pcf
Optimum Moisture 12.5 %
Test Method: **FM 5-515**
(%) Passing #4 Sieve 100.0%
Sampled By: Tim Gerow
Tested By: David Hill

Description: Tan silty Sand (A-2-4), Stratum 2

Sample Location: Boring A-3, 1' to 2'

Date Sampled: 04/26/2016
Sampled in accordance with AASHTO T-2

Remarks:

Respectfully Submitted,
TIERRA INC.

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FDOT Laboratory No. I03066

RESULTS OF LABORATORY TESTING

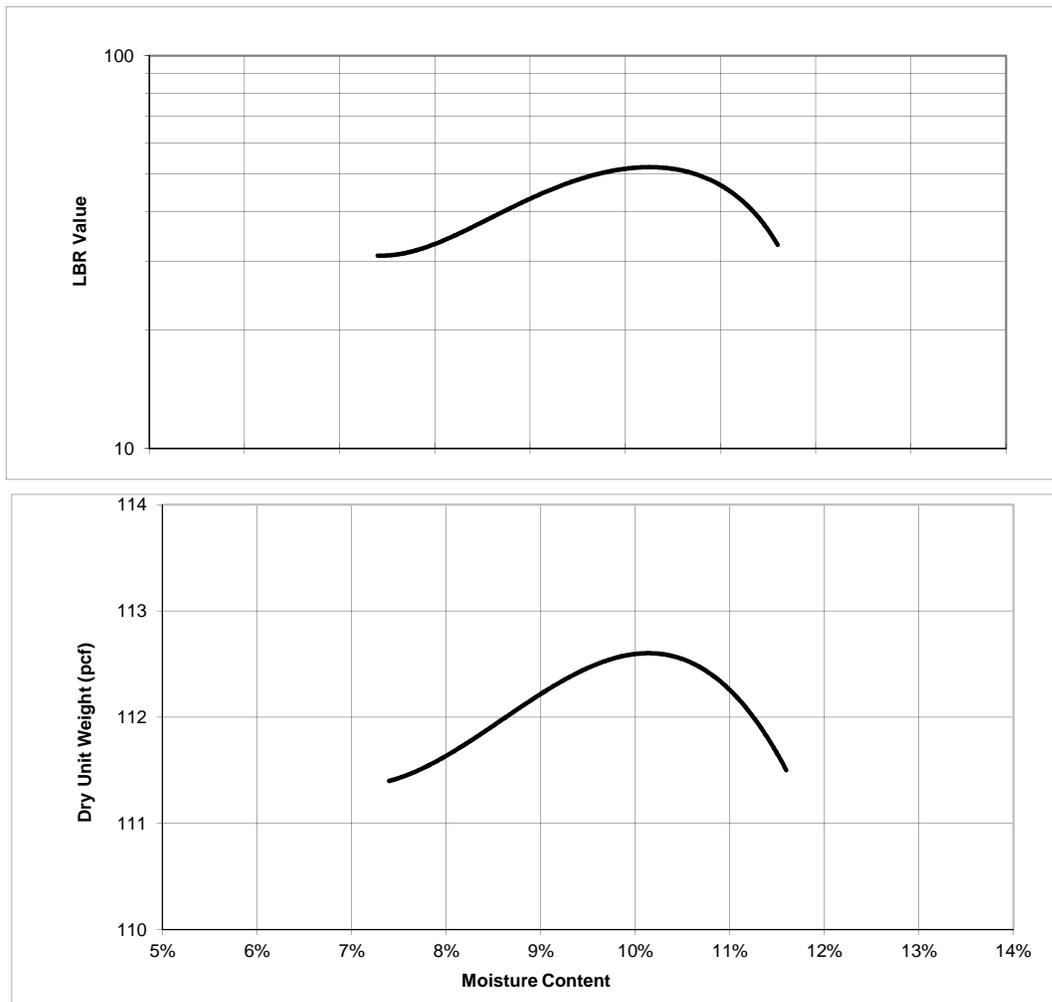
Tested For: Mr. Thomas McLendon, PE
Hatch Mott MacDonald of Florida, Inc.
5111 North 12th Avenue
Pensacola, Florida 32504

Project: Detroit Boulevard Safety Improvements
Escambia County, Florida

Tierra Project No: 4111-14-004
Intended Use: Design
Sample No: A-61R, Station 61+15, Offset 31' RT

Report Date: 6/21/2016

LBR & MOISTURE-DENSITY RELATIONSHIP



LBR Value 52
Maximum Density 113 pcf
Optimum Moisture 10.2 %
Test Method: **FM 5-515**
(%) Passing #4 Sieve 100.0%
Sampled By: Josh Fields
Tested By: David Hill

Description: Orange Sand (A-3), Stratum 1
Sample Location: Boring A-61R, 1' to 2'
Date Sampled: 05/25/2016
Sampled in accordance with AASHTO T-2

Remarks:

Respectfully Submitted,
TIERRA INC.

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Phone: (850) 462-8774 • Fax: (850) 607-6953
FDOT Laboratory No. I03066

RESULTS OF LABORATORY TESTING

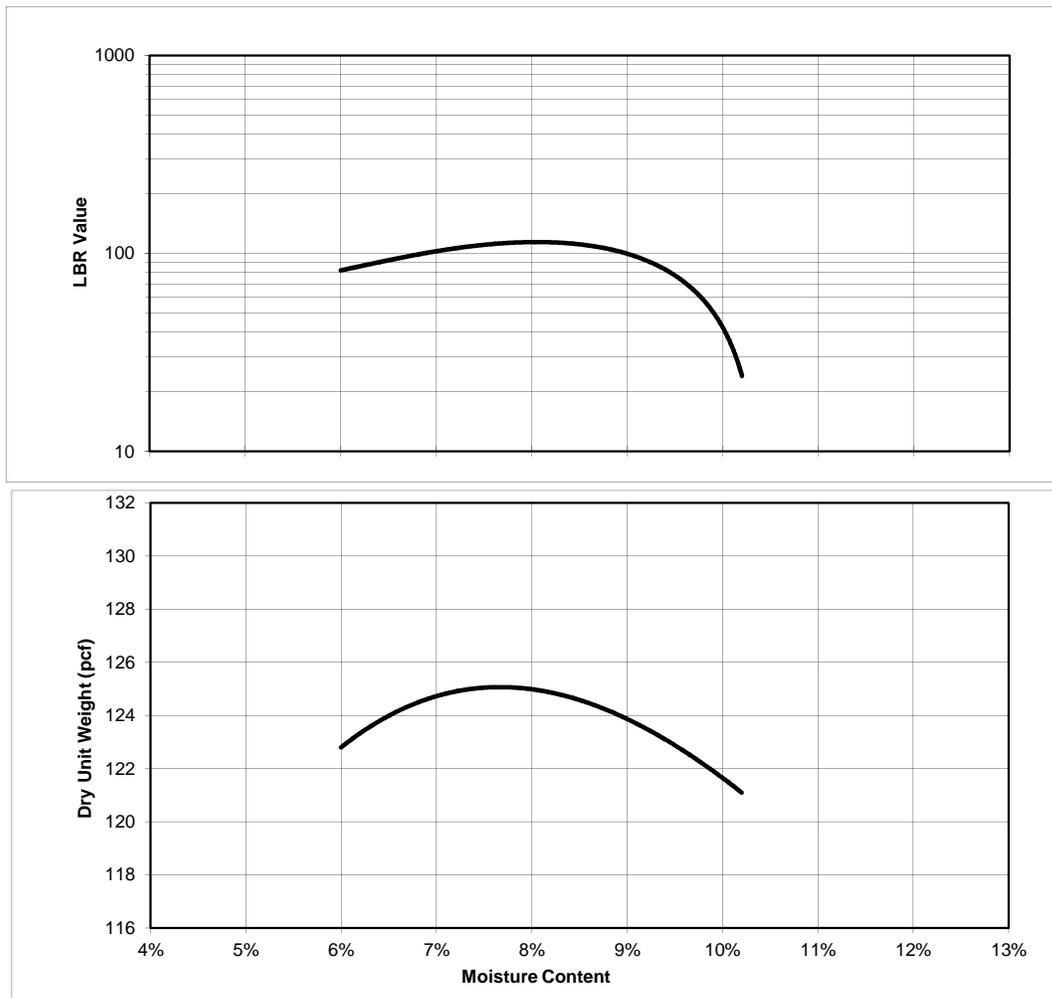
Tested For: Mr. Thomas McLendon, PE
Hatch Mott MacDonald of Florida, Inc.
5111 North 12th Avenue
Pensacola, Florida 32504

Project: Detroit Boulevard Safety Improvements
Escambia County, Florida

Tierra Project No: 4111-14-004
Intended Use: Design
Sample No: A-88R, Station 88+02, Offset 16' RT

Report Date: 6/6/2016

LBR & MOISTURE-DENSITY RELATIONSHIP



LBR Value 106
Maximum Density 125 pcf
Optimum Moisture 7.2 %
Test Method: **FM 5-515**
(%) Passing #4 Sieve 100.0%
Sampled By: Josh Fields
Tested By: David Hill

Description: Brown silty Sand (A-2-4), Stratum 3
Sample Location: Boring A88R, 1' - 2'
Date Sampled: 05/20/2016
Sampled in accordance with AASHTO T-2

Remarks:

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FDOT Laboratory No. I03066

RESULTS OF LABORATORY TESTING

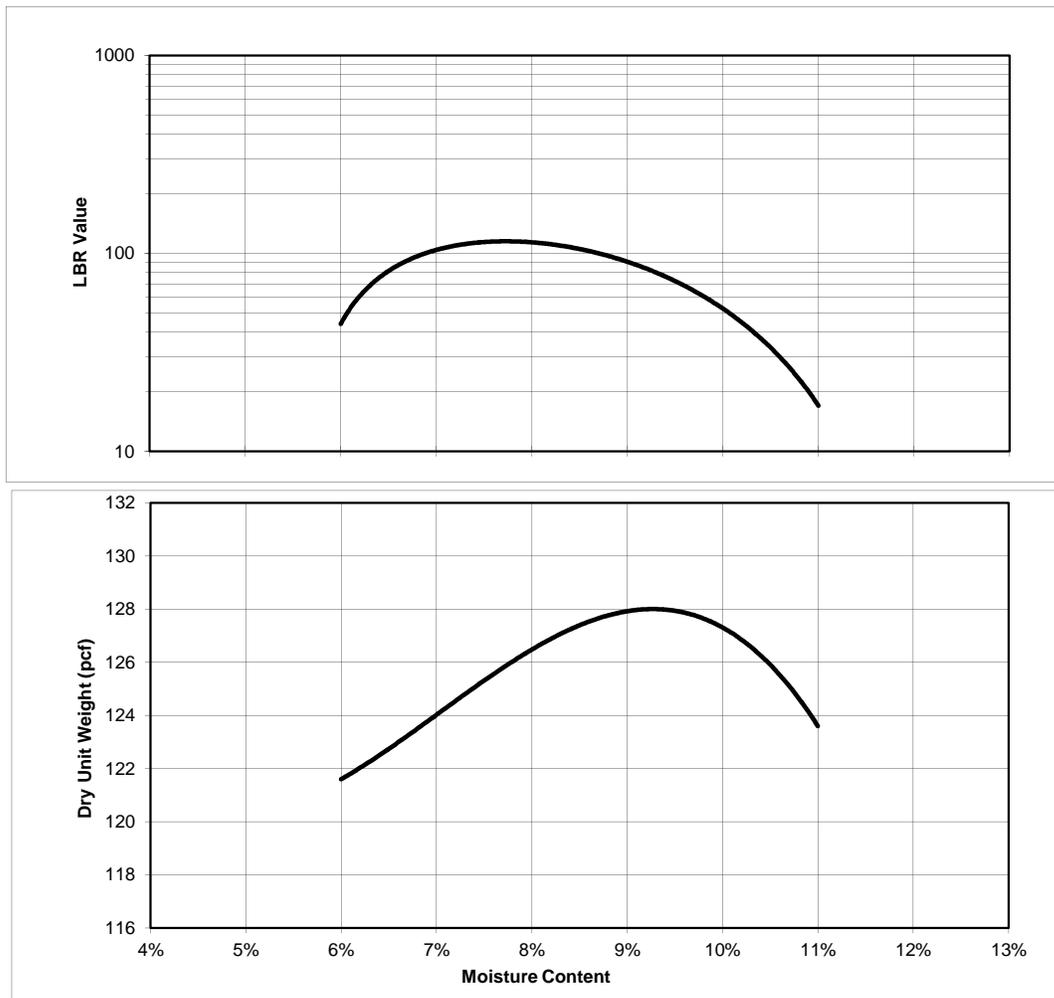
Tested For: Mr. Thomas McLendon, PE
Hatch Mott MacDonald of Florida, Inc.
5111 North 12th Avenue
Pensacola, Florida 32504

Project: Detroit Boulevard Safety Improvements
Escambia County, Florida

Tierra Project No: 4111-14-004
Intended Use: Design
Sample No: A-117R, Station 117+01, Offset 17' RT

Report Date: 6/1/2016

LBR & MOISTURE-DENSITY RELATIONSHIP



LBR Value 115
Maximum Density 128 pcf
Optimum Moisture 9.2 %
Test Method: **FM 5-515**
(%) Passing #4 Sieve 100.0%
Sampled By: Josh Fields
Tested By: David Hill

Description: Brown silty Sand (A-2-4), Stratum 3
Sample Location: Boring A117R, 1' - 2'
Date Sampled: 05/20/2016
Sampled in accordance with AASHTO T-2

Remarks:

Respectfully Submitted,
TIERRA INC.

TIERRA

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Phone: (850) 462-8774 • Fax: (850) 607-6953
FDOT Laboratory No. I03066

RESULTS OF LABORATORY TESTING

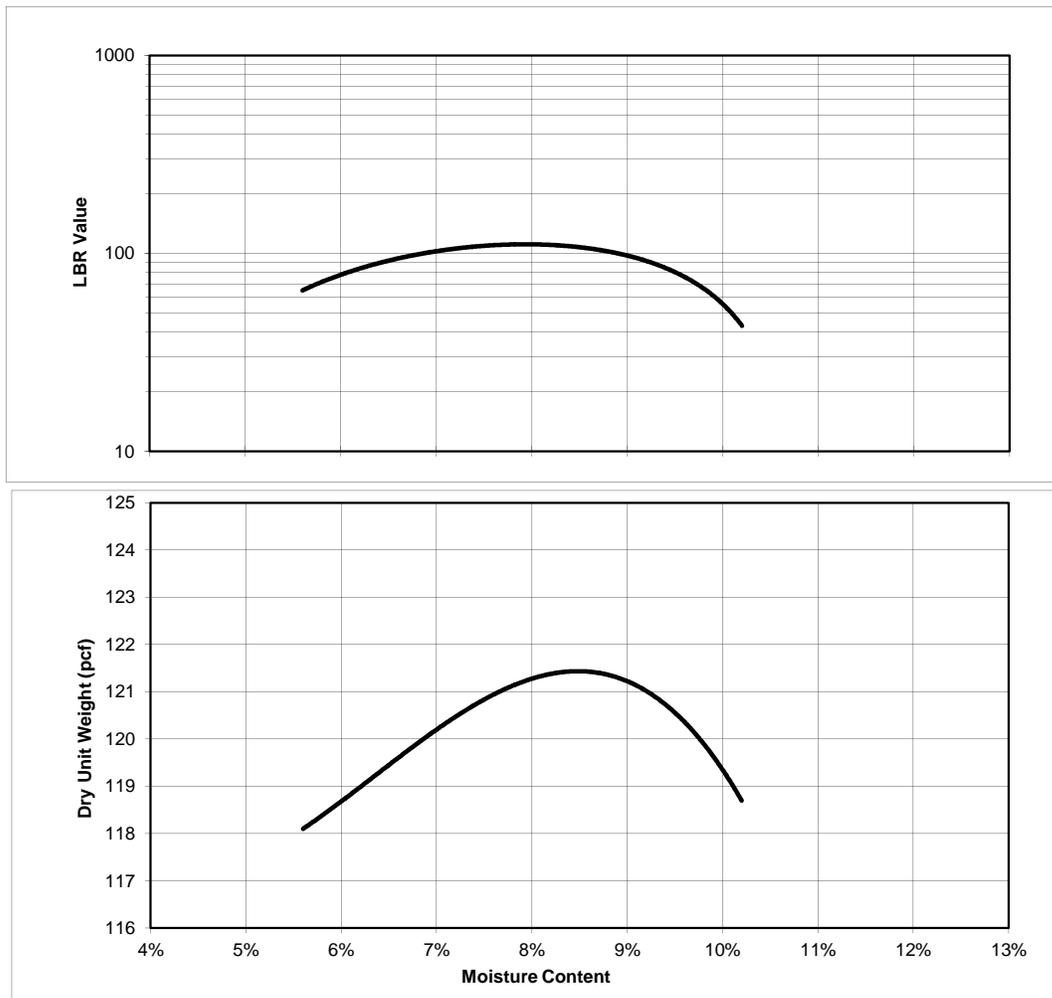
Tested For: Mr. Thomas McLendon, PE
Hatch Mott MacDonald of Florida, Inc.
5111 North 12th Avenue
Pensacola, Florida 32504

Project: Detroit Boulevard Safety Improvements
Escambia County, Florida

Tierra Project No: 4111-14-004
Intended Use: Design
Sample No: A-136L, Station 136+00, Offset 22' LT

Report Date: 6/21/2016

LBR & MOISTURE-DENSITY RELATIONSHIP



LBR Value 107
Maximum Density 121 pcf
Optimum Moisture 8.5 %
Test Method: **FM 5-515**
(%) Passing #4 Sieve 100.0%
Sampled By: Josh Fields
Tested By: David Hill

Description: Brown silty Sand (A-2-4), Stratum 3
Sample Location: Boring A-136L, 1' to 2'
Date Sampled: 05/25/2016
Sampled in accordance with AASHTO T-2

Remarks:

Respectfully Submitted,
TIERRA INC.